

Original

THE STATE OF NEW YORK
NYS CENTRAL PINE BARRENS COMMISSION

-----X

In the Matter of
Letter of Interpretation Appeal
Joseph Zachary Gazza
Suffolk County Tax Map Number
900-307-2-28

-----X

Town of Brookhaven
One Independence Hill
Farmingville, New York 11738

Wednesday, May 19, 2010

The above-entitled matter came on for hearing at 4:00 p.m.



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Telephone: 212.349.9692
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One Penn Plaza
Suite 4715
New York, NY 10119



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A P P E A R A N C E S:

CENTRAL PINE BARRENS JOINT PLANNING AND POLICY
COMMISSION:

PETER SCULLY, CHAIRMAN

MARTY SHEA, Representative of Town of
Southampton

ANNA E. THRONE-HOLST, Town of Southampton
Supervisor.

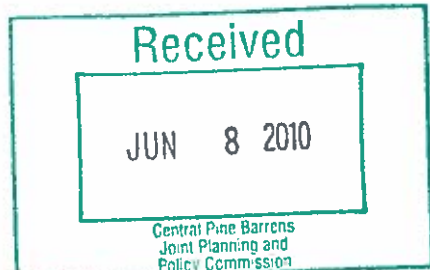
DANIEL McCORMICK, Representative of
Riverhead Town Supervisor.

CARRIE MEEK GALLAGHER, Representative of
Suffolk County

MARK LESKO, Town of Brookhaven Supervisor

JOHN C. MILAZZO, Pine Barrens Commission Staff
Counsel

JUDITH JAKOBSEN, Environmental Planner



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I N D E X

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P R O C E E D I N G S

1
2 CHAIRMAN SCULLY: I would like to
3 call the public hearing to order.

4 The Central Pine Barrens Joint
5 Planning and Policy Commission will hold a
6 public hearing on May 19, 2010 at 3:00 p.m. at
7 Brookhaven Town Hall, 1 Independence Hill,
8 Farmingville, New York on the Letter of
9 Interpretation Appeal for Joseph Zachary Gazza
10 for Suffolk County Tax Map parcel number
11 900 -- 307-2-28. Said appeal is made pursuant
12 to Section 6.7.3.3 of the Central Pine Barrens
13 Comprehensive Land Use Plan, dated 1995, (the
14 Plan). The Central Pine Barrens Joint
15 Planning and Policy Commission will be holding
16 the appeal hearing pursuant to Section 6.7.3.4
17 of the Plan. The appellant is appealing the
18 allocation of 0.34 Pine Barrens Credits stated
19 in the Letter of Interpretation dated March
20 12, 2010 for this parcel and is seeking
21 instead one Pine Barrens Credit. The parcel
22 is located at 120 Summit Boulevard,
23 Westhampton, in the Town of Southampton,

24 Will the Commission representatives
25 identify themselves?

1 MR. WALTER: Sean Walter, Town of
2 Riverhead.

3 MS. GALLAGHER: Carrie Gallagher,
4 Suffolk County.

5 MS. THRONE-HOLST: Anna
6 Throne-Holst, Town of Southampton.

7 CHAIRMAN SCULLY: Peter Scully
8 representing the Governor of the State of New
9 York.

10 We will hear from Ms. Jakobsen.

11 MS. JAKOBSEN: I have to admit as
12 staff exhibits A through L, which I have
13 given to the stenographer to include as part
14 of the hearing transcript. I will generally
15 refer to them as I go through a brief overview
16 of the project.

17 (Commission Exhibits A - L were
18 so marked for identification
19 and received in evidence.)

20 MS. JAKOBSEN: The credit appeal,
21 basically Mr. Gazza is appealing his credit
22 allocation of .34 Pine Barrens Credits granted
23 for his parcel located at 129 Summit
24 Boulevard. He is referring to the provision
25 which I have as Exhibit A as part of the Land

1 Use Plan, the Commission's Land Use Plan under
2 6.7.6.6, where it states issuance of a full
3 Pine Barrens Credit for Certain Roadfront
4 parcels where it states the Pine Barrens
5 Credit Clearinghouse may elect to allocate one
6 full Pine Barrens Credit for a parcel of land
7 consisting of at least 4,000 square feet with
8 frontage on an existing improved road.

9 I have some additional maps that I
10 will out. These are exhibits. The first one
11 is Exhibit B, that shows Summit Boulevard, the
12 location of the parcel, the location that is
13 the subject of the appeal is outlined in
14 purple. There it also indicates in the lime
15 green the credit appeal parcels south of the
16 subject parcel that were granted. Increase in
17 allocation were received and full Pine Barrens
18 Credits. One was subject -- one or two were
19 subject to appeal where they received a full
20 credit. You will see also on the west side of
21 Summit Boulevard, the original outline is the
22 development section map that is a compatible
23 growth area of Pine Barrens.

24 This will give you, I am handing out
25 Exhibit D, which will show you the core and

1 compatible growth area in relationship to
2 subject parcel, which again is the purple
3 outlined parcel, so north of the CGA. This is
4 a close-up of the aerial Exhibit C, of the
5 prior aerial showing the subject area and
6 surrounding undeveloped area.

7 And in addition, I have some
8 photographs which are Exhibit L, which show
9 the condition of the parcel and the
10 surrounding area along Summit Boulevard.
11 Summit Boulevard is a gravel road as you enter
12 from Summit Boulevard, from the south just
13 north of the railroad, and Old Country Road,
14 and you travel north on Summit Boulevard, the
15 road is approximately two car widths. When
16 you reach an area which you will see on the
17 aerial just below as you travel up Summit
18 Boulevard, you reach an area about 3/10ths of
19 a mile up on the west side is a horse corral
20 and residence. And just as you pass that
21 area, the road for Summit Boulevard narrows to
22 a single car width and the road as you
23 continue north is a single car width. As you
24 get up to the subject parcel, which is purple,
25 and Summit Boulevard continues north up to a



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1 larger, sandy area, and then from there it
2 continues as a very narrow dirt path.

3 Summit Boulevard itself is, like I
4 said, basically a gravel road from the
5 beginning where you enter just above the
6 railroad up until you get beyond the subject
7 parcel, so just to where you reach a wide
8 open, sandy area. From what I understand from
9 the Town, this portion of Summit Boulevard is
10 not maintained.

11 MR. MILAZZO: Judy, did you do a
12 staff inspection on April 6th and did you do a
13 second inspection on April 20th?

14 MS. JAKOBSEN: Yes.

15 MR. MILAZZO: That's when you
16 observed the change road condition?

17 MS. JAKOBSEN: Yes.

18 MR. MILAZZO: South of this parcel?

19 MS. JAKOBSEN: Yes.

20 MR. MILAZZO: This parcel north of
21 the property was the subject of the last
22 Commission meeting where we had a hearing?

23 MS. JAKOBSEN: Yes.

24 MR. MILAZZO: On the west side,
25 north of the development area designated by

1 the Town of Southampton?

2 MS. JAKOBSEN: Yes.

3 MR. MILAZZO: And the Town at the
4 last meeting testified that the Town neither
5 owned nor maintained Summit Boulevard north of
6 Old Country Road and the intersection with
7 Long Island Rail Road tracks.

8 Is that still true?

9 MR. SHEA: I also have had an
10 opportunity to visit the site since the last
11 meeting and the road conditions don't meet any
12 of our standards for improved roads.

13 MR. MILAZZO: And the Town has
14 specifications for highway conditions,
15 correct. It was admitted as an exhibit at the
16 last hearing, and I think the exhibit today is
17 Exhibit I. So this wouldn't meet the Town --
18 the condition of Summit Boulevard in front of
19 the subject lot of today's hearing, does that
20 meet the requirements set forth in the Town's
21 Road and Drainage Standards?

22 MR. SHEA: No, it does not.

23 MR. MILAZZO: Thank you. We don't
24 have anything else.

25 CHAIRMAN SCULLY: Mr. Gazza, please

1 state your name and address for the record.

2 MR. MILAZZO: He is an attorney, so
3 we don't have to swear him in.

4 MR. GAZZA: Sure. Joseph Zachary
5 Gazza, 37 B Gardeners Lane, Hampton Bays, New
6 York 11946.

7 Well, it feels like I was just here,
8 and after last month's meeting I knew that I
9 couldn't compete with Ms. Jakobsen's volume
10 for exhibits so I brought in size. This is
11 the big map. We are all familiar with this
12 map. This is Old Country Road. This is the
13 railroad tracks and this is of course Summit
14 Boulevard. We will start with blue square
15 that is the compatible growth area from 1993
16 when the Pine Barrens act came to fruition and
17 the line was drawn around this area. This was
18 designated as the compatible growth area which
19 is developable. It's a receiving area. The
20 only access to these parcels is Summit
21 Boulevard, which has been in existence for
22 quite a long time.

23 Moving up the map, I don't know if
24 it's visible back there, reflective tape marks
25 one of the three prior parcels on the east

1 side of Summit Boulevard that was allocated
2 one full Pine Barrens Credit. I know last
3 meeting there was a question as to if those
4 three parcels received their full credit
5 before or after the road was widened. Two of
6 the parcels received the credit after the road
7 was widened. This one received a full credit
8 before the road was widened. Before the road
9 was widened, it looked like this.

10 (Handing.)

11 MR. GAZZA: If I had a crystal ball
12 then, I would have taken more pictures of it
13 before we actually started the clearing. You
14 can see in the middle of the picture on the
15 left-hand side.

16 MR. MILAZZO: Who took the photos?

17 MR. GAZZA: Joseph Fredic Gazza, my
18 dad, with an old fashioned non-digital camera.

19 MR. MILAZZO: Mark this in as Gazza
20 Exhibit 1.

21 (Gazza Exhibit 1 was so marked
22 for identification and received
23 in evidence.)

24 MR. GAZZA: Sure. The width of the
25 road.

1 MR. MILAZZO: Were you there when he
2 took those photos?

3 MR. GAZZA: Yes, I helped him. The
4 important part is the width of road.

5 As I think is fairly visible, it is
6 not a two-car width road. It's gravel. It
7 has a base. This was the condition of Summit
8 Boulevard before the Town required that we
9 widen it to build a home across the street
10 from this lot. This is also the approximate
11 width and the same composition of the road in
12 front of the parcel I was here on last month
13 and the parcel for today. It's approximately
14 a one-plus car width gravel. It has a base.
15 It's used, it's maintained by the people who
16 use it. It may not be maintained by the Town.
17 It is cleared away. Somebody put gravel down.
18 Someone, you know, grades it back and forth to
19 keep it level, accessible by car and truck,
20 and it is used on a daily basis. It is used
21 up until this clearing, famous clearing that I
22 have brought up numerous times now.

23 MR. MILAZZO: Will you stipulate
24 that nothing north of the road is an existing
25 improved road?

1 MR. GAZZA: You will not see me here
2 with an appeal.

3 MR. MILAZZO: Will you stipulate
4 that nothing north of the road is an existing
5 improvement?

6 MR. GAZZA: As far as I am
7 concerned, anything north of this red line,
8 which is why I put the tape there, is not
9 paved. It is not gravel. It is not
10 maintained as far as I have observed. I
11 wouldn't consider it existing or improved.

12 MR. MILAZZO: Okay.

13 MR. GAZZA: However, up until this
14 clearing, it is gravel, it is maintained, it
15 is cleared, it is improved, it is used and
16 there is plenty of dumping up here, which was
17 the subject of my application that people use
18 it. So the parcel I am here on today is this
19 yellow block.

20 I believe that it is on an existing
21 improved road. I know the Commission would
22 like to make decisions that are consistent
23 with prior decisions as far as what an
24 existing improved road is. I am not sure if
25 the Commission is leaning towards deciding

1 that an existing improved road is that to the
2 Town of Southampton standards?

3 I did do a lot of homework on other
4 parcels that have received a full credit. Now
5 the Commission members will undoubtedly know
6 much more about these parcels than I will. I
7 was able to obtain aerial pictures. I only
8 have one copy because it's expensive.

9 MR. MILAZZO: Mark this Gazza 2.

10 (Gazza Exhibit 2 was so marked
11 for identification and received
12 in evidence.)

13 MR. GAZZA: The first property on
14 the list is 25062311. This is outlined in
15 red, the parcels that received a full credit.

16 Down here is definitely an improved
17 existing road. It's paved. However this
18 parcel is not on this road, it is on what
19 looks like a dirt driveway. It doesn't look
20 like it has gravel to me. I am sure it is
21 used, it looks like there is tire tracks but
22 this road was considered existing and improved
23 as far as the Commission was concerned. It's
24 a 6,000 square foot parcel and I don't think
25 the Town of Brookhaven has 6,000 square foot

1 zoning.

2 The second parcel is a 4,000 square
3 foot lot. It is behind a home. It is on a
4 road which is not improved at all road, might
5 be improved with the home. Once again there
6 might be something that I am missing. This is
7 a lot that received a full credit, 40 by 100.
8 This is a road. I will leave these over here.

9 Now, the third one I must be missing
10 something, because this is an island. I am
11 sure you are all familiar with this because
12 there must have been a lot of talk about
13 giving a full credit to an island in the
14 Potomac River, unless water is a considered a
15 road, it is navigable by boat.

16 MR. MILAZZO: Is that the result of
17 an appeal?

18 MR. GAZZA: I am not sure. But I
19 know these are all indicated as full credits
20 and I don't believe they are up to zoning.

21 The fourth one which is 200-269.13 is
22 closer to the road than the other ones that I
23 have shown you. It looks like broken pieces
24 of asphalt, overgrown, certainly not passable,
25 certainly not maintained by any town or

1 anybody for that matter. And this has also
2 received a full credit.

3 The last parcel I believe is on a
4 road. This road is paved. There is a car on
5 it, it's passable as existing, it's improved.

6 MR. MILAZZO: Did you skip one, land
7 number 141?

8 MR. GAZZA: This is the last one.
9 So this lot is on a road, a good example on an
10 existing improved road. However, not even
11 half the required size as far as the section
12 of the code that I am here on today, which is
13 4,000 feet.

14 MR. WALTER: Mr. Gazza, what the
15 Commission needs to know is were those done as
16 part of the Clearinghouse initially?

17 MR. MILAZZO: I don't know. We will
18 allow him to give his presentation and we will
19 do the research and give a recommendation at
20 the next meeting. These are arguments Mr.
21 Gazza did not make last time.

22 MR. GAZZA: I had to bring something
23 to keep everyone entertained.

24 So that's that. If I could go back,
25 to use Ms. Jakobsen's example, her Exhibit L

1 which shows Summit Boulevard in front of the
2 subject parcel. I know everyone has it. We
3 will go through it.

4 And this is my exhibit of Summit
5 Boulevard in front of the parcel before you
6 today, and I think that Summit Boulevard, as
7 having gravel and being used and there is a
8 base and it is maintained and it is cleared,
9 is certainly more than an existing improved
10 road than some of the examples that I have
11 shown you.

12 In the interest of the Commission
13 being consistent and the Clearinghouse being
14 consistent, this parcel should receive full
15 credit. This is the best I can do for this
16 meeting.

17 Finally, this is the improved
18 development section map that matches up with
19 this blue box, Summit Boulevard is a 50-foot
20 wide road. It is cleared more in some areas
21 than others. I actually am in the process of
22 preparing two new road review applications for
23 the Town for Summit Boulevard where they have
24 informed me that they will be having me clear
25 Summit Boulevard further, more widening and

1 more gravel.

2 And as far as the Town of Southampton
3 is concerned, I do know as there is more
4 development on an old file map of the road,
5 the Town does require more and more
6 improvement to be done to roads. They start
7 with the base gravel, clearing shoulders, and
8 eventually move on to blacktop, drainage and
9 curbing. And so, there are all different
10 stages of a road. And I believe that I have
11 shown you that Summit Boulevard is existed and
12 improved and to the standards of prior Pine
13 Barrens Clearinghouse Commission decisions on
14 a full credit allocation.

15 I am available for any questions.

16 CHAIRMAN SCULLY: Thank you.

17 MR. SHEA: Just a few comments. The
18 subject parcel is outside of the open old file
19 map developed area. The area that you are
20 describing now already has approval, road
21 improvement is south of the subject parcel.
22 Those roads improvements have not been
23 completed. Those road improvements would not
24 be completed until the building permit
25 applications are made and the area has been

1 assessed by the Road Review Committee that
2 would set the standards for improvement. But
3 even when that section of Summit Boulevard is
4 improved, that improved section will not
5 extend up to the subject parcel because this
6 is outside of that approved old file map
7 development area.

8 MR. GAZZA: If you are talking about
9 the double car-width road improved as per the
10 Town of Southampton Road Review Committee and
11 up to the standards that they require, as you
12 build on a map, you are right, they are not
13 done yet. And if they are done, they may
14 expand further. I don't know. I have had to
15 do road improvement on roads that do not even
16 come close to the property that I am building
17 on. I don't know what Road Review will have
18 me do.

19 MR. SHEA: I checked with the
20 Planning Commission and that section of Summit
21 Boulevard is not going to open up and the
22 improved section adjacent to the subject
23 parcel and making a determination as to
24 whether or not the road is improved, it is not
25 just the road width, it is based on whether or

1 not the surface provides satisfactory
2 vehicular access. The current base, the site
3 of the inspection of Summit Boulevard is
4 rutted with bumps. It really does not provide
5 safe vehicular access, even though vehicles
6 drive through that area.

7 MR. GAZZA: It was safe for my car
8 and it was safe for Ms. Jakobsen's car. We
9 have pictures of her at the area up at the
10 clearing. This is what the road looks like.
11 Once again, this is 129 Summit Boulevard in
12 front of the proposed lot which I am applying
13 for a credit for. It is gravel, it is stone,
14 it is maintained. Somebody put this down,
15 this you can drive on. It is not soft. It's
16 hard. It's stable. I have driven on it in
17 the rain. And it looks like just like this
18 Summit Boulevard. This is the same road where
19 credit was approved, full credit. Summit
20 Boulevard is like this all the way up to the
21 clearing.

22 Yes, the Town has to do more
23 improvements on the road and, yes, two of the
24 credits were issued after the improvements
25 were done. One was issued before. I maintain

1 this width was enough to get the earlier
2 credit. I am just trying to be consistent.

3 MR. SHEA: Just for comparison
4 purposes, the section of Summit Boulevard that
5 has been improved has good visibility, it is
6 wide, it has a safe, level surface. That's in
7 contrast to the section of Summit Boulevard
8 adjacent to the subject property. There is
9 some gravel there and you may be able get a
10 vehicle through there, but it is still not
11 improved.

12 MR. GAZZA: You can see a mile down
13 the road as far as visibility. This is Summit
14 Boulevard in front of the property
15 (indicating).

16 MR. SHEA: It still doesn't have a
17 stable surface.

18 MR. GAZZA: It has a base that has
19 been gone over by trucks. There are stumps in
20 the clearing here that are this big
21 (indicating). Large trucks put them in there,
22 the old stumps. The base has been developed
23 by constant use.

24 CHAIRMAN SCULLY: Other questions?

25 MR. WALTER: Two quick questions.

1 Mr. Gazza, where is the area of the
2 base in relation to this?

3 MR. GAZZA: The area of the base is
4 here (Indicating).

5 MR. WALTER: Where are the railroad
6 tracks?

7 MR. GAZZA: The railroad tracks,
8 this is the quarry down here and this is the
9 Town transfer station. This is the old drag
10 strip. Actually, the transfer station
11 encroaches.

12 MR. WALTER: This is not dispositive,
13 I am just curious, how long have you owned
14 this parcel?

15 MR. GAZZA: This parcel I acquired
16 in the last year.

17 CHAIRMAN SCULLY: Other questions?

18 MR. WALTER: When did you acquire
19 the other parcels.

20 MR. GAZZA: Within the last three or
21 four years. I acquired these parcels because
22 I am an owner of multiple lots on this map in
23 need of development rights. I am trying to
24 work within the program of Pine Barrens.

25 MR. WALTER: You don't need to

1 explain.

2 MR. GAZZA: I am explaining for the
3 benefit of everyone to have the right idea, I
4 am trying to move them onto these parcels. I
5 am trying to keep everything in the same
6 school district work, within the plan. And I
7 purchased the properties knowing that Summit
8 Boulevard was there and that these three
9 parcels had been approved for full credit and
10 knowing the existence of the condition of the
11 road when this one was approved.

12 CHAIRMAN SCULLY: Other questions
13 for Mr. Gazza?

14 Any questions for the staff?

15 Anyone else wish to be heard?

16 If not, we will close the hearing for
17 further deliberation of the Commission.

18 Thank you, Mr. Gazza.

19 MR. GAZZA: Thank you.

20 (Time Noted: 4:25 p.m.)
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C E R T I F I C A T I O N

I, Margaret Eustace, a Shorthand Reporter and Notary Public, within and for the State of New York, do hereby certify that I reported the proceedings in the within-entitled matter, on May 19, 2010, at 1 Independence Hill, Farmingville, New York, and that this is an accurate transcription of these proceedings.

IN WITNESS WHEREOF, I have hereunto set my hand this 4th day of June, 2010.

Margaret Eustace
MARGARET EUSTACE 





**CENTRAL
PINE
BARRENS**
JOINT
PLANNING
&
POLICY
COMMISSION

Peter A. Scully
Chair

Mark Lesko
Member

Steve A. Levy
Member

Anna E. Throne-Holst
Member

Sean M. Walter
Member

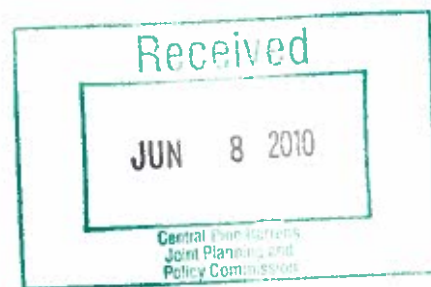
Notice of Public Hearing
Letter of Interpretation Appeal
Joseph Zachary Gazza SCTM No. 900-307-2-28

The Central Pine Barrens Joint Planning and Policy Commission will hold a public hearing on May 19, 2010 at 3:00 pm at Brookhaven Town Hall, 1 Independence Hill, Farmingville, N.Y., on the Letter of Interpretation Appeal for Joseph Zachary Gazza for Suffolk County Tax Map parcel Number 900-307-2-28. Said appeal is made pursuant to Section 6.7.3.3 of the Central Pine Barrens Comprehensive Land Use Plan, dated 1995 (the Plan). The Central Pine Barrens Joint Planning and Policy Commission will be holding the appeal hearing pursuant to Section 6.7.3.4 of the Plan. The appellant is appealing the allocation of 0.34 Pine Barrens Credits stated in the Letter of Interpretation dated March 12, 2010 for this parcel and is seeking instead one (1) Pine Barrens Credit. The parcel is located at 129 Summit Blvd, Westhampton, in the Town of Southampton.

Any persons who wish to comment on the appeal are invited to attend the hearing. The file for this matter is available for review by appointment at the Commission's Office located at 3525 Sunrise Highway, 2nd Floor, Great River, NY 11739. If you should have any questions, please contact the Commission's office at 631-224-2604.

P.O. Box 587
3525 Sunrise Highway
2nd Floor
Great River, NY
11739-0587

Phone (631) 224-2604
Fax (631) 224-7653
www.pb.state.ny.us



Central Pine Barrens Joint Planning and Policy Commission
Public Hearing on 129 Summit Blvd
Westhampton Beach, Town of Southampton
SCTM No. 900-307-2-28
May 19, 2010

Staff Exhibits

- A. Central Pine Barrens Comprehensive Land Use Plan Section 6.7.6.6 Issuance of a full Pine Barrens Credit for Certain Roadfront Parcels.
- B. 2007 Aerial map depicting general area in proximity to Summit Blvd. Subject parcel is outlined in purple. Parcels that received one Pine Barrens Credit are outlined in light green)
- C. Close-up of 2007 Aerial map depicting general area of subject parcel on Summit Blvd. Prepared by Commission staff, May 18, 2010.
- D. Map depicting
 - a. Core (indicated by small tree symbol on map) and Compatible Growth Area (indicated by light tan leaf symbol) in relation to Summit Blvd,
 - b. prior Pine Barrens Credit allocations to parcels along Summit Blvd, and
 - c. location of development Section Map for Vanderbilt Park Inc, Section 5 and P/O Westhampton Beach Park Section 4 that was approved by TOS in July 22, 2004.
- E. Letter of Interpretation for 900-307-2-28 dated March 12, 2010, Letters of Interpretation for 900-331-3-29 and 900-331-3-32.
- F. Commission Meeting Summary (Final) for March 17, 2004 – excerpt referencing project decisions - Appeals Gazza/Westhampton (900-331-3-28) and portions of Public Hearing Transcript for Letter of Interpretation Appeal February 18, 2004.
- G. Commission Meeting Summary (Final) for April 19, 2006 – excerpt on motion to deny Joseph Gazza /Westhampton 900-280-2-82. Basis: road is not maintained in this area.
- H. Development Section Map of Vanderbilt Park Section 5 and P/O Westhampton Beach Park Section 4, Situate at Westhampton, prepared by Department of Land Management and Planning Division, July 22, 2004
- I. Road and Drainage Standards for the Town of Southampton, Suffolk County, New York, Town of Southampton Highway Department, prepared by L.K. McLean Associates, PC, July 2002
- J. Town of Southampton Highway Department, Table “A”.
- K. Town of Southampton, NY Chapter 292 Subdivision of Land Article X Design Standards.
- L. Photos taken by Commission staff during site visit on April 16, 2010 depicting subject site and adjacent area.



6.7.6.6 Issuance of a full Pine Barrens Credit for certain roadfront parcels

The Pine Barrens Credit Clearinghouse may elect to allocate one (1) full Pine Barrens Credit for a parcel of land consisting of at least 4,000 square feet with frontage on an existing improved road.



Subject Parcel
(outlined in purple)
900-307-2-28
129 Summit Blvd

Development Section Map Vanderbilt Park, Inc. Section 5
& P/O Westhampton Beach Park Section 4
Approved July 22, 2004 by TOS

Zach Gazza
900-331-2-21.1
118 Summit Blvd.

Joseph Gazza
900-331-1-3-29
(allocated 1 PBC based on
appeal on Lot 28)

Zach Gazza
900-331-3-28
(appeal granted increase from
0.37 PBC to 1 PBC)

Zach-Gazza
900-331-1-3-32
Credit Cert# 900-412
1PBC 5/6/2008

Note: This map is intended for general reference only and is not to be used for property legal descriptions or parcel boundary boundaries, or other precise purposes. Information shown on this map has not been certified and cannot be used for address, survey, or other similar use.

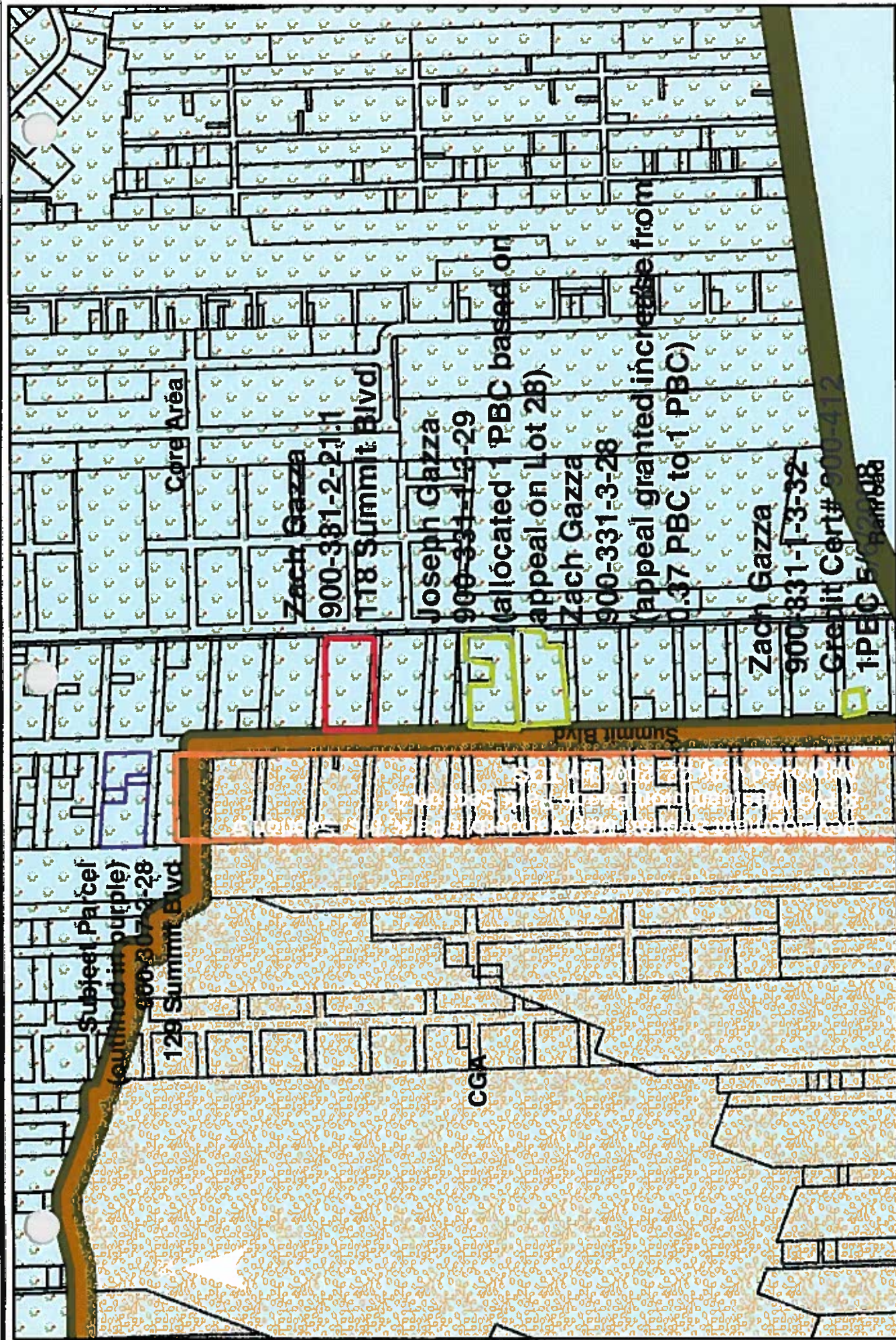
Spring 2017 Digital Orthomosaic, NYS Digital Ortho company Program, NYS/USACE
Suffolk County Real Property Tax Service Agency GIS Bureau/CITY/VILLAGE/County of
Suffolk, N.Y.

Prepared by Commission Staff (B) on April 23, 2016.

Joseph Zachery Gazza
Credit Appeal
May 19, 2010
2007 Aerial







Joseph Zachary Gazza
 Credit Appeal
 May 19, 2010
 2007 Aerial

Note: This map is intended for general reference only and is not to be used for surveying, legal, engineering or professional purposes. It is not a representation of any information shown on the map but may have overlaid and contains defects, errors, and omissions.

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 Albany, N.Y.

Prepared by Chasman S&E, Inc. on April 23, 2010



PINE BARRENS CREDIT CLEARINGHOUSE

FILE COPY

JAMES T. B. TRIPP, ESQ., CHAIRMAN
ANDREW P. FRELENG, AICP, VICE CHAIRMAN
RICHARD W. HANLEY, MEMBER
ITCHHELL H. PALLY, ESQ., MEMBER
ROBERT ANRIG, MEMBER

LETTER OF INTERPRETATION

Re: Suffolk County Tax Map Number: 900-307-2-28
Applicant: Joseph Z. Gazza

Date: March 12, 2010

Findings of Fact

The applicant applied for a Letter of Interpretation for the above-referenced 1.7 acre parcel. The parcel is in the Town of Southampton. It was in the CR-200 District at the adoption of the *Central Pine Barrens Comprehensive Land Use Plan* (the "Plan") on June 28, 1995.

Conclusions

The *Plan* grants to every parcel of land in a sending area a use right, known as Pine Barrens Credits, that may be used to seek development density or intensity increases on lands identified as receiving areas within the same township.

The *Plan* establishes the formula for allocating Pine Barrens Credits. In sending areas within Old Filed (OFM) areas of the Town of Southampton as per Section 6.4.4.1 of the *Plan* that are within the CR-200 District of the Town of Southampton, the number of Pine Barrens Credits allocated is equal to the parcel's size in acres multiplied by .20. Based upon this allocation formula, .34 Pine Barrens Credits may be allocated to this parcel. There are no known conditions on the parcel which reduce the allocation of Pine Barrens Credits pursuant to Section 6.3.3 of the *Plan*. The allocation is rounded to the nearest hundredth of a credit.

The total number of Pine Barrens Credits allocated for this parcel is .34

This Letter of Interpretation expires in one year from the above date. In order to obtain a Pine Barrens Credit Certificate you must complete the Pine Barrens Credit Certificate Application and follow the instructions contained in the Pine Barrens Credit Handbook.

If there is a mortgage or other lien on this parcel, the applicant will have to make an arrangement with the lender or other party holding the lien before the Clearinghouse can issue a Pine Barrens Credit Certificate for this tax map parcel.

Appealing your Allocation

Any person who is aggrieved by this determination may appeal the allocation within thirty (30) days of the date of this letter by giving notice, in writing, to the Central Pine Barrens Joint Planning and Policy Commission. The Commission address is P.O. Box 587, 3525 Sunrise Highway, 2nd Floor, Great River, New York 11739-0587. Included with this notice shall be the name and address of the person requesting reconsideration and the reasons supporting the appeal as well as the number of Pine Barrens Credits requested. The Commission shall consider and decide the appeal within sixty (60) days of receipt of an appeal and will schedule a public hearing on the appeal.

P.O. BOX 587, 3525 SUNRISE HIGHWAY, 2ND FLOOR, GREAT RIVER, NEW YORK 11739-0587
631-224-2604 / FAX 631-224-7653
<http://pb.state.ny.us>

PINE BARRENS CREDIT CLEARINGHOUSE

James T.B. Tripp, Esq., Chairman
Andrew P. Freleng, AICP, Vice Chairman
Richard W. Hanley, Member
Mitchell H. Pally, Esq., Member
Robert Anrig, Member

900-331-3-29 1.0 PBC

LETTER OF INTERPRETATION

Re: Suffolk County Tax Map Number: 900-331-3-29

Applicant: Joseph F. Gazza

Date: July 11, 2005

Findings of Fact

The applicant applied for a Letter of Interpretation for the above-referenced 1.79-acre parcel. The parcel is in the Town of Southampton. It was in the CR200 zoning district at the adoption of the *Central Pine Barrens Comprehensive Land Use Plan* (the "Plan") on June 28, 1995.

Conclusions

The *Plan* grants to every parcel of land in a sending area a use right, known as Pine Barrens Credits, that may be used to seek development density or intensity increases on lands identified as receiving areas within the same township.

The *Plan* establishes the formula for allocating Pine Barrens Credits. In sending areas within the CR200 zoning district of the Town of Southampton, the number of Pine Barrens Credits allocated is equal to the parcel's size in acres multiplied by 0.16. Based upon this allocation formula, 0.2864 Pine Barrens Credits may be allocated to this parcel. There are no known conditions on the parcel which reduce the allocation of Pine Barrens Credits pursuant to Section 6.3.3 of the *Plan*. This allocation qualifies for the application of Section 6.7.6.6 of the *Plan* permitting the allocation of one (1.00) full Pine Barrens Credit due to its frontage on an existing improved road.

The total number of Pine Barrens Credits allocated for this parcel is 1.00.

PINE BARRENS CREDIT CLEARINGHOUSE

JAMES T.B. TRIPP, ESQ., CHAIRMAN
ANDREW P. FRELENG, AICP, VICE CHAIRMAN
RICHARD W. HANLEY, MEMBER
MITCHELL H. PALLY, ESQ., MEMBER
ROBERT ANRIG, MEMBER

900-331-3-32 1.0 PBC

LETTER OF INTERPRETATION

Re: Suffolk County Tax Map Number: 900-331-3-32

Applicant: Joseph Zachery Gazza

Date: August 1, 2006

FILE COPY

Findings of Fact

The applicant applied for a Letter of Interpretation for the above-referenced .230 acre parcel. The parcel is in the Town of Southampton. It was in the CR-200 District at the adoption of the *Central Pine Barrens Comprehensive Land Use Plan* (the "Plan") on June 28, 1995.

Conclusions

The *Plan* grants to every parcel of land in a sending area a use right, known as Pine Barrens Credits, that may be used to seek development density or intensity increases on lands identified as receiving areas within the same township.

The *Plan* establishes the formula for allocating Pine Barrens Credits. In sending areas within Old Filed (OFM) areas of the Town of Southampton as per Section 6.4.4.1 of the *Plan* that are within the CR-200 District of the Town of Southampton, the number of Pine Barrens Credits allocated is equal to the parcel's size in acres multiplied by .20. Based upon this allocation formula, .046 Pine Barrens Credits may be allocated to this parcel. There are no known conditions on the parcel which reduce the allocation of Pine Barrens Credits pursuant to Section 6.3.3 of the *Plan*. The allocation is rounded to the nearest hundredth of a credit. This allocation qualifies for the application of Section 6.7.6.6 of the *Plan* permitting the allocation of one (1.00) full Pine Barrens Credit due to its frontage on an existing improved road.

The total number of Pine Barrens Credits allocated for this parcel is 1.0.

This Letter of Interpretation expires in one year from the above date. In order to obtain a Pine Barrens Credit Certificate you must complete the Pine Barrens Credit Certificate Application and follow the instructions contained in the Pine Barrens Credit Handbook.

If there is a mortgage or other lien on this parcel, the applicant will have to make an arrangement with the lender or other party holding the lien before the Clearinghouse can issue a Pine Barrens Credit Certificate for this tax map parcel.

Appealing your Allocation

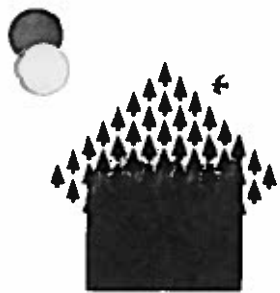
Any person who is aggrieved by this determination may appeal the allocation within thirty (30) days of the date of this letter by giving notice, in writing, to the Central Pine Barrens Joint Planning and Policy Commission. The Commission address is P.O. Box 587, 3525 Sunrise Highway, 2nd Floor, Great River, New York 11739-0587. Included with this notice shall be the name and address of the person requesting reconsideration and the reasons supporting the appeal as well as the number of Pine Barrens Credits requested. The Commission shall consider and decide the appeal within sixty (60) days of receipt of an appeal and will schedule a public hearing on the appeal.

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631-224-2604 / FAX 631-224-7653

<http://pb.state.ny.us>



900-331-3-28 granted 1.0 PBC



CENTRAL
PINE
BARRENS

JOINT
PLANNING
&
POLICY
COMMISSION

Peter A. Scully
Chair

Philip J. Cardinale
Member

Patrick A. Heaney
Member

John Jay LaValle
Member

Steven A. Levy
Member

Commission Meeting Summary (FINAL)
for Wednesday, March 17, 2004 (Approved 4/21/04; Corrected 7/21/04)
at Southaven County Park, Victory Avenue, Yaphank
2:00 pm

Commission members present: Mr. Deering (for Suffolk County), Mr. Scully (for New York State), Mr. Hoffman and Ms. Compitello (for Brookhaven), Mr. Hanley (for Riverhead), and Mr. Murphree (for Southampton).

Project Decisions - Pine Barrens Credit Appeals

Gazza / Westhampton (900-331-3-28) (*hearing held 2/18/04; decision due today*)

Summary: Mr. Randolph summarized this previously discussed appeal for a parcel on the Core side of Summit Boulevard, south of Sunrise Highway. He recommended granting the appeal for one full Credit based upon the parcel's location on Summit Boulevard and the physical status of that road as both passable and accessible. A motion was made by Mr. Murphree and seconded by Mr. Hoffman to grant the appeal on this parcel for 1.0 Credit. The motion was approved by a 5-0 vote.

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Highway
2nd Floor
Great River, NY
11739-0587

Phone (631) 224-2604
Fax (631) 224-7653
www.pb.state.ny.us

900-331-3-28

1

1 CENTRAL PINE BARRENS JOINT PLANNING
2 AND POLICY COMMISSION

3 -----X

4 In the Matter of the,

5

6 LETTER OF INTERPRETATION APPEAL

7

8 RE: JOSEPH F. GAZZA

9

10 -----X

11 South Haven Park
12 Yaphank, New York

13

14 February 18, 2007^H

15 3:15 P.M.

16

17

18

19 Taken by: Donna L. Spratt,

20 Court Reporter

21

22

23

24

25

COPY

1 opposed to a drivable, navigable road.

2 MR. SCULLY: The improved road
3 criteria threshold was not applied.

4 Any further discussion? If not --

5 MS. PRUSINOWSKI: How far off of
6 South Country?

7 MR. GAZZA: North of Old Country.

8 MS. PRUSINOWSKI: By approximately
9 how far?

10 MR. GAZZA: I'll tell you in a
11 moment. This is the permit application
12 for my lot across the street. There is
13 an indication on that.

14 MR. SCULLY: Any further comments
15 from the public? Questions?

16 MS. PRUSINOWSKI: I'm just trying
17 to locate it.

18 MR. SCULLY: If there are no
19 comments or questions, is there a
20 motion?

21 MR. DEERING: I make a motion to
22 grant the one Pine Barrens credit.

23 MR. MILAZZO: Based on an existing
24 improved road.

25 MR. DEERING: Based on the

1 criteria within the plan.

2 MR. MURPHREE: As outlined by
3 staff.

4 MR. MILAZZO: Not that it appears
5 on the receiving list.

6 MR. RANDOLPH: One condition, that
7 he will close the fire break.

8 MR. CORWIN: Can I ask a
9 clarification question? For the future,
10 are we deeming that road, fire break,
11 semi-improved path through the woods as
12 a road for purposes of any future
13 applications that come up that might be
14 on or adjacent to that?

15 MR. SCULLY: I think we would have
16 to.

17 MR. CORWIN: We're setting a
18 precedent?

19 MR. DEERING: Maybe not for the
20 part that the fire break --

21 MR. CORWIN: We need to know that
22 if the next owner says --

23 MR. MURPHREE: For this portion of
24 it, we don't know for further north how
25 far it goes.

1 MR. CORWIN: We're saying from
2 this property south is considered an
3 improved road, not opining about the
4 northerly portion.

5 MR. MURPHREE: Because the north
6 we don't know.

7 MR. RANDOLPH: I think it has to
8 be a case by case basis because you
9 don't really know.

10 MS. CARTER: Does this require a
11 280 A approval from the Town?

12 MR. GAZZA: It goes under old
13 filed map regulations of the Town of
14 Southampton, and they have their own
15 road review commission. In connection
16 with the parcel cross the street that we
17 have an application to develop right
18 now, there will be some improvements
19 necessary to Summit Boulevard.

20 MR. MURPHREE: Have you been to
21 the road review committee?

22 MR. GAZZA: No. I'm at the town
23 planning board level right now. I've
24 had the public hearing.

25 MR. SCULLY: Is that pertinent to

1 MR. GAZZA: Everyone thinks it is
2 an error except me.

3 MR. RANDOLPH: You can't be
4 sending and receiving simultaneously.

5 MR. CORWIN: The Commission
6 established a principle that you don't
7 receive in the core. We've been over
8 this.

9 MR. FREELENG: That is what I
10 didn't understand. This was a receiving
11 site, he had an application before the
12 Commission.

13 MR. SCULLY: Any further
14 questions?

15 (No response).

16 There is a motion on the table.

17 MR. HOFFMAN: Seconded.

18 MR. SCULLY: Anything further? If
19 not, all in favor?

20 (All voted in the affirmative).

21 Opposed?

22 (No response).

23 None.

24 MR. GAZZA: Thank you for your
25 consideration. Have a good day.



900-280-2-82

Denial - Not a maintained road

**Commission Meeting Summary (FINAL)
for Wednesday, April 19, 2006 (Approved 5/17/06)
at Brookhaven Town Hall,
1 Independence Hill, Farmingville, 2:00 pm**

Commission members present: Mr. Scully (for New York State), Ms. Longo (for Suffolk County), Ms. Prusinowski and Mr. Turner (for Brookhaven), Mr. Kent (for Riverhead), and Mr. Shea (for Southampton).

Others present: Staff counsel was Mr. Milazzo. Staff members included Mr. Corwin, Ms. Jakobsen, Ms. Mills, Ms. Carter and Mr. Limoli (from the Commission), Mr. Pavacic (from the NYS Department of Environmental Conservation (NYS DEC)), and Mr. Freleng (from the Suffolk County Planning Department and Vice Chair of the Pine Barrens Credit Clearinghouse Board). Additional attendees are shown on the attached sign-in sheet.

(Excerpt from Minutes:)

Ms. Prusinowski arrived during the following item, and a five member quorum remained.

Peter A. Scully
Chair

Pine Barrens Credit Program

Philip J. Cardinale
Member

Joseph Gazza / Westhampton (900-280-2-82) / Credit allocation appeal (*appeal letter previously distributed*)

Brian X. Foley
Member

Summary: Mr. Scully read the attached hearing notice. Ms. Jakobsen explained that a Letter of Interpretation (LOI) for 1.00 Credit had originally been issued for this parcel, and was later rescinded by the staff when it was determined that the parcel is on an unimproved dirt road, rather than an improved road. A new LOI for 0.36 Credit was then issued. The parcel contains 1.79 acres, and is zoned CR200 Residence. Mr. Gazza spoke about this parcel, the road access to it, and answered questions from the Commissioners regarding access, utilities, the Town status of the road, and other topics. Mr. Shea observed that Southampton Town does not maintain the road in this area.

Patrick A. Heaney
Member

Steve A. Levy
Member

A motion was then made by Mr. Shea and seconded by Mr. Kent to deny the appeal on the basis that the road in question is not a maintained road. The motion was approved by a 5-0 vote, and the hearing was concluded.

Attachments (In order of discussion):

12. Gazza Credit appeal hearing notice and photographs (undated; 3 pages)

P.O. Box 587
3525 Sunrise Highway
2nd Floor
Great River, NY 11739-
0587

Phone (631) 224-2604
Fax (631) 224-7653
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ROAD AND DRAINAGE STANDARDS

FOR THE

**TOWN OF SOUTHAMPTON
SUFFOLK COUNTY, NEW YORK**



TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT

WILLIAM H. MASTERSON SUPERINTENDENT OF HIGHWAYS

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

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TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

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TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

A. DRAINAGE DESIGN CRITERIA

All storm water collection systems shall be designed in accordance with the following minimum standards and shall be submitted on design sheets to be reviewed by the Town Engineer.

1) Rational Method:

- Q = Aci**
Q = Discharge in cubic feet per second (c.f.s.)
A = Tributary drainage area in acres within the subdivision and shall include areas outside the subdivision
c = Coefficient of runoff of drainage area

The following values of "c" shall be used:

<u>Type of Surface</u>	<u>Flat</u>	<u>Rolling</u>
Roofs	1.00	---
Concrete or Asphalt Pavement	0.90	0.90
Bituminous Macadam	0.70	0.90
Gravel	0.25	0.70
<u>Composite Areas</u>		
Business and Dense Residential	0.60	0.95
Suburban Residential	0.30	0.60
Rural Districts	0.10	0.25
Parks, Golf Courses, etc.	0.10	0.35
Agricultural Areas	0.20	0.60

Where:

"i" is the rainfall intensity in inches per hour, shall be determined by the following formula:

$$i = \frac{120}{t+20}$$

Where:

"t" is the time of concentration in minutes at the point of design.

Time of Concentration can be calculated by using Table 31.2 Equations for Overland Flow Travel Time (The Civil Engineering Handbook, W.F. Chen, 1995).

Maximum time of concentration (t) shall be 28 minutes.

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

DRAINAGE DESIGN CRITERIA (Continued)

2) **Manning's Formula:**

Closed conduits and open channels shall be designed using Manning's Formula:

a)
$$V = \frac{1.486 R^{2/3} S^{1/2}}{n}$$

Where: V = velocity in feet per second
R = hydraulic radius in feet
S = Slope in ft./ft. The slope (S) shall generally be considered to be the slope of the pipe invert except that such slope shall be checked against the available hydraulic gradient wherever the system discharges against an existing hydraulic head.

Where: n = 0.015 for reinforced concrete pipe 18" or less
0.013 for reinforced concrete pipe 24" or larger
0.013 for smooth plastic pipe
0.021 for corrugated metal pipe
0.025 for earth ditches
0.013 for paved ditches

- b) Design velocities to be limited to 2 ft./sec. minimum and 10 ft./sec. maximum, unless special approval for unusual conditions is granted by the Town Engineer.
- c) Minimum pipe diameter to be 15".
- d) Minimum pipe cover from finished pavement or ground surface to be two (2) feet.

3) **Manhole Installation:**

Maximum spacing distance between manholes shall be 350 feet. Manholes shall be provided where there are changes in pipe alignment or gradients.

4) **Catch Basin Installation:**

Spacing of catch basins shall be governed by the following: The maximum overland flow rate to a single catch basin shall not exceed three (3) c.f.s. The maximum distance of flow in the roadway gutter, before a catch basin is required shall not exceed 350 feet.

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

DRAINAGE DESIGN CRITERIA (Continued)

5) Recharge Basins:

Recharge basins shall be provided where there is no available outlet for storm water or where in the opinion of the Town Engineer, a potential drainage problem exists. In general, a tributary area of eight (8) acres or more shall be deemed to necessitate a storm water recharge basin. In areas less than eight (8) acres drainage facilities shall be installed as directed by the Town Engineer.

Storm water recharge basins shall be designed in accordance with the drawing details and the following:

- a) All storm water recharge basins shall provide storage capacity for 5" of rainfall on the total tributary area multiplied by a weighted runoff coefficient (minimum $c = 0.25$). Where it is evident that possible overflow of the recharge basins will cause property damage, the storage capacity shall be increased using 8" of rainfall.
- b) Maximum depth of storage capacity shall be ten (10) feet. This measurement is to be two (2) feet below the elevation of the berm, or elevation of grate in the inlet basin at the low point in system, whichever is the lowest.
- c) If feasible, the area of the recharge basin shall be cleared of trees, only where excavation is required and that a natural tree screening remain along the perimeter of the basin.
- d) Asphalt gutters will be required around the perimeter of the recharge basin where possible erosion from upland runoff may occur.
- e) Maximum discharge velocity of the outfall pipe shall not exceed 10 f.p.s.
- f) Excavation shall be carried down through good leaching material. A test hole shall be required at the bottom elevation of the recharge basin and shall indicate at least 5' of good leaching material, below the basin bottom. Should any doubt exist as to the adequacy of the leaching material, percolation tests will be required.
- g) Fencing complete with gate shall be constructed before starting excavation.

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

DRAINAGE DESIGN CRITERIA (Continued)

6) **Landscaping of Recharge Basins:**

Topsoil shall be placed at least 6" deep on the surface of the bank around the recharge basin extending from the inside of the fence to a line 4' down from the top of the slope. All topsoil areas shall be limed, fertilized and seeded. The area around the fence shall have screen planting parallel to, inside and outside the fence around the entire recharge basins, except at the gates.

a) **Topsoil**

The topsoil shall consist of natural loam, horticulture soil, free of refuse, clods and stones larger than 1" in diameter, weeds or any other objectionable loam material. It shall contain not less than 5% and not more than 20% organic matter as determined by loss on ignition of moisture free samples dried at 100 degrees. The pH shall be between 5.5 and 7.6. Topsoil shall be placed 6" deep when compacted. The surface shall be smoothly graded to meet established elevations and adjacent ground levels.

b) **Liming**

Limestone shall be agricultural ground limestone with a total carbonate content of not less than 80% or 44.8% calcium oxide equivalent, for the purposes of calculations. Total carbonates shall be considered as calcium carbonate. Limestone shall be evenly distributed at the rate of 50 lbs. per 1,000 square feet and worked into the top 3" of the soil.

c) **Fertilizing**

Not less than 5 days after the application of limestone, commercial fertilizer organic type (10-6-4) shall be evenly distributed at the rate of 20 lbs. per 1,000 square feet and worked into the top 3" of the soil.

d) **Hydroseeding** - Grass seed shall be applied as follows:

Per Acre: 12 pounds Crownvetch
 10 pounds Birdfoot Trefoil
 30 pounds Tall Fescue
 45 pounds Annual Rye

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

DRAINAGE DESIGN CRITERIA (Continued)

The seed producer's certificate of analysis shall be available for inspection.

A 100% wood fiber mulch binder should be incorporated into the seed mixture at a rate of 1,200 pounds per 150 pounds of seed. The 100% wood cellulose fiber mulch binder should meet the following requirements:

Organic Matter	98% \pm 2%
Ash Content	1.4%
pH	6 \pm 2
Water Holding Capacity	90% minimum

Grass seeding shall be done between April 15th and June 15th or between August 15th and October 15th and shall not be accepted unless there is a uniform growth evident over all seeded areas.

e) **Manual Seeding** - Grass seed shall be applied as follows:

Per Acre:	12 pounds Crownvetch
	10 pounds Birdfoot Trefoil
	30 pounds Tall Fescue
	45 pounds Annual Rye

Minimum seed germination shall be 80%. Inert matter and weed seeds shall not exceed 8%. The seed producer's certificate of analysis shall be available for inspection. Grass seed shall be sown evenly at the rate of 3 lbs. per 1,000 square feet. The seed shall be covered to a proper depth by raking or other suitable means. After seeding and raking, the surface shall be rolled with an approved roller weight of at least 100 lbs.

Grass seeding shall be done between April 15th and June 15th or between August 15th and October 15th and shall not be accepted unless there is a uniform growth evident over all seeded areas.

f) **Screen Planting**

Plants shall be placed 8 feet apart and 7.5 feet from and parallel to the fence on both sides of the fence. The minimum height shall be 6' and the minimum spread shall be 2 feet. All trees shall be nursery grown and shall be normal columnar or narrowly pyramid habit of growth typically characteristic of the particular variety. Only the following species and varieties are acceptable:

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DRAINAGE DESIGN CRITERIA (Continued)

- 1) Bay Berry (Myrica pensylvanica)
- 2) White Pine (Pinus strobus)
- 4) Red Twig Dogwood (Corinus stolonifera)
- 5) Eastern Red Cedar (Juniperus virginiana)
- 6) Highbush Cranberry (Viburnum dentatum)
- 7) Arrowwood Viburnum (Viburnum dentatum)
- 8) Inkberry (Ilex glabra)
- 9) Witchazel (Hamamelis virginiana)

All evergreen trees shall be moved with a burlap and tied with a good grade of haylopp or heavy twine. The size of ball for all specified evergreen trees shall be a minimum of 12" in diameter and deep enough to include all lateral roots. All evergreen trees shall be set plumb at such a level that after settlement they bear the same relationship to the elevation of the surrounding ground as they bore to the ground from which they were dug. All trees shall be planted and the holes backfilled and tamped with topsoil. The screen planting shall be maintained and all dead or dying trees replaced by the developer until such time as the recharge basin has been accepted for dedication by the Town.

7) **Roadside Ditches:**

Under special conditions, roadside ditches may be utilized with prior approval of the Town. The following design criteria for roadside ditches shall be used:

Slopes 4% or less – sod

Slopes greater than 4% - asphalt

8) **Leaching Basins:**

Under special conditions, leaching basins may be used in addition to a recharge basin. The following design criteria for leaching basins shall be used:

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DRAINAGE DESIGN CRITERIA (Continued)

a) **$V_c = A R C$**

Where: V_c = volumetric capacity in cubic feet
 A = tributary drainage area in square feet within the subdivision and shall include areas outside the subdivision.
 R = 2"
 C = weighted coefficient for runoff of the drainage area (minimum $c= 0.25$)

9) **Valley Gutters:**

Valley Gutters will be permitted at "T" intersections where they will be parallel to the center line of the through road and shall be installed only after design approval by the Town Engineer. In general, the crown of the intersecting road shall be gradually eliminated starting from a point about thirty (30) feet back from the flow line of the through street. At other than "T" intersections, valley gutters will be permitted only when warranted by limited traffic use as approved by the Town Engineer.

10) **Drainage Structures on State or County Land:**

Drainage structures which are located on state highway rights-of-way shall be approved by the appropriate highway engineer's office and a letter of approval from that office shall be filed with the Town Planning Board. Where County highways are involved, the Planning Board will obtain the approval from the Suffolk County Department of Public Works.

11) **Dry Wells on Individual Plots:**

Individual plot grading studies shall be made from map data submitted and where required, in the opinion of the Town Engineer, dry wells for house leaders, driveways, or yard drains shall be installed as directed.

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B. HORIZONTAL ALIGNMENT CRITERIA

- 1) Horizontal alignment curves: The recommended minimum centerline radius curvature shall be 200 feet for local street and 400 feet for collector streets.
- 2) A tangent distance of at least 50 feet shall be provided between reverse curves.
- 3) Minimum radius at a corner shall be 25 feet at the property line except that a larger radius shall be provided at major intersections.
- 4) Adequate sight distance must be provided at all intersections in accordance with the New York State Manual of Uniform Traffic Control Devices (MUTCD), 2001 edition.

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C. VERTICAL ALIGNMENT CRITERIA

- 1) All street gradients shall conform as much as possible to the natural terrain, minimizing excessive cuts and fills.
- 2) Minimum road gradients shall be 0.5%.
- 3) Maximum road gradients shall be 6%.
- 4) Gradients approaching intersections shall not exceed 2.5%, commencing at a point at least fifty (50) feet from the nearest intersecting right-of-way line measured along the centerline of the road. Intersections of roads and curbs having a minimum gradient shall be detailed sufficiently to insure proper surface drainage.
- 5) Gutter line gradients of cul-de-sacs shall be a minimum of 0.5%.
- 6) All changes in grade of 1.0% or more shall be connected with a vertical curve.
- 7) Sag and Crest Vertical Curves:

The following formula shall be used in the design of street profiles:

a) $L = K A$

Where:

L	=	length in feet of a vertical curve in
K	=	constant equaling 28 for minor streets and 50 for collector streets.
A	=	algebraic difference in percent of grade.

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D. SPECIAL DETAILS

1) Retaining Walls:

- a) Where retaining walls are required by reason of plot grading or terrain they shall be designed by a Licensed Professional Engineer and approved by the Town Engineer prior to construction.
- b) Retaining walls shall be constructed of structural concrete, reinforced as required unless special written permission to substitute other materials is granted.
- c) Retaining walls constructed of railroad ties or similar treated timbers may be substituted when the Town Engineer determines that such construction is adequate provided that any such wall shall have a maximum reveal of four (4) feet. There shall be no more than two (2) such walls on the same slope, and the minimum horizontal distance between shall not be less than three (3) feet. Adequate tie-back or cribbing shall be provided, and no wall shall be backfilled until inspected.

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E. GENERAL INSTRUCTIONS

1) Schedule of Operations:

The Town Engineer, Superintendent of Highways and the Developer shall together review the Schedule of Operations prior to the start of construction to insure the orderly procedure of the work.

2) Traffic Maintenance:

The flow of traffic or the safety thereof on any public street or highway within the Town shall not be restricted or endangered in any way by construction operations, equipment, vehicles or materials connected with the construction or sales operations of a subdivision. The Developer shall provide for off-street parking space for all vehicles used in construction or sales operations. Equipment or materials shall not be stored within the right-of-way lines of any public street or highway.

3) Temporary Roads:

All roads within a development which are used by the contractors or others and which have not been surfaced shall be suitably treated with calcium chloride or other material which have dust-laying qualities.

4) Temporary Drainage:

Rainfall run-off from development during construction operations must be confined to the site unless an adequate stormwater drainage system exists. Methods of providing for temporary drainage shall be approved by the Town Engineer. Stormwater run-off disposal onto existing roads, streets or private property is prohibited.

5) Protection of New Drainage Facilities:

All drainage facilities during construction operations shall be cleared of lumber, debris, dirt and other objectionable material after completion and shall be maintained in a clean condition until time for use as directed by the Town Engineer.

6) Developer's Responsibility:

The Developer is responsible for keeping Town roads, streets and private property free of debris and damage caused by construction operations or by stormwater run-off from the Development site. Any damage so caused shall be immediately repaired by the Developer at his own expense. If, after due notice by the Town Engineer the Developer does not proceed

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GENERAL INSTRUCTIONS (Continued)

within 72 hours to make the necessary repairs or to remove the debris caused by his operations, the Town Engineer is empowered to take necessary corrective measures and the costs shall be paid by the Developer.

7) Construction Emergencies:

In any emergency arising during the construction period of a Development where the Developer or his representatives is not immediately available to take responsible charge, the Town Engineer shall take such measures and render such decisions as may be necessary to control the situation. Any resultant costs shall be borne by the Developer at no expense to the Town.

8) Decision of Town Engineer Final:

It shall be mandatory upon the Developer that the work proceed in accordance with the best construction practice and that all necessary measures be taken for the protection of the Public Health, Safety and Welfare. The decision of the Town Engineer shall be final and binding in these matters.

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F. ROADWAY CONSTRUCTION SPECIFICATIONS

1) Stabilized Soil Base:

Depending on the character of the soils encountered all or part of the soil base areas where directed by the Engineer shall be improved with added material from the excavation. The Contractor shall stockpile, during the excavation process as directed by the Engineer, sufficient selected materials for this purpose. Suitable loamy or sandy soils shall be added and incorporated to form a stable base, a minimum of six (6) inches in compacted thickness. The areas to be treated in cuts shall be excavated to a depth of three (3) inches below subgrade, a layer of suitable material between three (3) and six (6) inches loose measure shall be spread over the roadway area. The added material shall be thoroughly mixed to a depth of six (6) inches by scarifying or by other methods approved by the Engineer. This operation shall continue until the soil is thoroughly mixed. If, in the opinion of the Engineer, soft, yielding or unsuitable material is encountered at subgrade level, it shall be removed to a depth of eighteen (18") inches or as directed by the Engineer and backfilled with suitable materials compacted in layers of six (6) inches.

The soil base shall then be shaped and compacted to the finished line and grade with a roller weighing a minimum of ten (10) tons. Shaping, filling and rolling shall continue until the soil base is thoroughly compacted to a depth of six (6) inches. When required, water shall be added to the soil mixture to obtain the optimum moisture content for compacting to the maximum density. The prepared finished soil base shall be kept free from ruts, depressions and properly drained. Upon completion of the soil base, the contractor shall request an inspection by the Engineer and shall not proceed with further roadway work until the inspection has been made and the work approved.

2) Dense Graded Aggregate Base Course:

a) Description:

Upon the stabilized soil base a dense graded aggregate base course shall be uniformly placed so that, after thorough rolling and compacting, a minimum depth of 6" for residential roads and a minimum depth of 8" for commercial roads are obtained.

b) Materials:

The base course blend shall consist of well graded crushed stone or recycled concrete aggregate (RCA). The base course blend shall have the following mechanical gradation:

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ROADWAY CONSTRUCTION SPECIFICATIONS (Continued)

<u>Screen Size</u>	<u>% Passing</u>
1-1/2"	100%
1"	90-100%
1/2"	65-85%
3/8"	55-75%
#4	40-55%
#8	30-45%
#30	16-27%
#200	0-10%

The material shall also achieve a maximum dry density of not less than 145 pounds per cubic foot at optimum moisture content when tested in accordance with ASTM Designation D1557 – Standard Test Methods for Laboratory Compaction Characteristics of Soil Using Modified Effort, Latest.

c) Method of Construction:

The stabilized soil base shall be smooth parallel to and at the required depth below the dense graded base surface. The soil base shall not be in a muddy or frozen condition. The dense graded base material shall be deposited on the stabilized base by means of a dump truck spreader tail-gate or any other approved method of depositing.

The spreading of the material shall be by means of approved self-propelled spreader equipment. No segregation of large or fine particles will be allowed, and the material as spread shall be well graded. After the base course has been laid loose, it shall be thoroughly rolled with an approved roller weighing not less than ten tons. Rolling must begin at the sides and continue toward the center, and shall continue until there is no movement of the course ahead of the roller.

The base course shall be kept in a moist condition to allow compaction to maximum density. Six (6) percent moisture shall be a guide line for the optimum moisture content.

In lieu of the above method of finishing rolling, the contractor may, at his option, use a vibratory method as follows: After the material is spread evenly, so that it will have the required thickness after compaction, the entire area shall be compacted by an approved vibratory compactor. Vibration shall continue until the material is keyed sufficiently to permit rolling with an approved roller without displacement of the material. For breakdown rolling, a pneumatic roller must be used.

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ROADWAY CONSTRUCTION SPECIFICATION (Continued)

Care shall be exercised to see that the voids in the base course are completely filled, but the operation of vibratory compaction shall not be such as to cause floating of the coarse aggregate.

The entire area shall then be rolled with an approved roller weighing not less than ten tons. Rolling shall begin at the sides and continue toward the center, and shall continue until there is no movement of the course ahead of the roller.

d) Testing:

The thickness of the base course shall be determined by the method directed by the Engineer, and a sieve analysis shall be performed as directed by the Engineer.

Upon completion of the aggregate base course the contractor shall request an inspection by the Engineer and shall not proceed with further roadway work until such inspection has been made and the work approved.

3. Asphalt Binder Course

The asphalt binder course shall meet the requirements of the New York State Department of Transportation Standard Specification Section 403 – Hot Mix Asphalt (HMA) Pavements for Municipalities – Type 3 Binder Course and shall be a minimum thickness of 2-1/2 inches for commercial roads and 1-1/2 inches for residential roads.

4. Asphalt Top Wearing Course

The asphalt top course shall meet the requirements of the New York State Department of Transportation Standard Specification Section 403 – Hot Mix Asphalt (HMA) Pavements for Municipalities – Type 6 Top Course and shall be a minimum thickness of 1-1/2 inches for residential roadways and commercial roads.

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G. TESTING OF PAVEMENT MATERIALS

The following tests shall be performed by a reliable testing laboratory, approval of the laboratory and additional test required shall be approved by the Town Engineer, at the developer's expense.

1) Dense Graded Aggregate Base Course:

The base course material shall be subject to laboratory test for particle size analysis for conformance with specifications.

2) Asphaltic Concrete Paving:

One (1) core from the finished pavement will be taken for each 1,000 feet of road, or if the road is less than 1,000 feet, one (1) core per road. There shall be a minimum of two (2) cores per project.

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H. MATERIALS OF CONSTRUCTION

1) Portland Cement Concrete

a) Proportioning:

Concrete shall consist of one (1) part Portland cement, two (2) parts of clean washed sand and four (4) parts of ¾-inch broken stone or cleaned washed gravel.

b) Strength:

All concrete shall have a minimum compressive strength of 3500 psi when tested 28 days after pouring.

c) Temperature:

Concrete shall be poured at a minimum temperature of forty (40) degrees Fahrenheit and rising.

d) Curing:

Concrete shall be maintained in a moist condition for at least five (5) days after placement.

e) Rejection:

All concrete shall be deposited with a maximum slump of 4 inches. Any concrete not acceptable to the Town Engineer shall be rejected and immediately removed from the job site.

2) Reinforcing Steel

a) Material:

All reinforcing steel shall conform to the American Society of Testing Materials ASTM A615 specification for deformed and plain billet steel bars for concrete reinforcement.

b) Placing:

Reinforcement shall be accurately placed in accordance with the approved plan and shall be held securely in place during the pouring of concrete.

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MATERIALS OF CONSTRUCTION (Continued)

3) Piping

a) Smooth Wall Plastic Pipe:

Unless otherwise specified, all drainage piping shall be smooth wall corrugated plastic in accordance with the requirements of the New York State Department of Transportation Standard Specification Section 706-12 – Smooth Interior Corrugated Polyethylene Pipe.

1. Pipe and fittings shall meet the requirements of A.A.S.H.T.O. M294 for Type S and Type SP.
2. The smoothness of the interior liner shall not deviate more than 1/4" per foot when checked with a straight edge.

b) Reinforced Concrete Pipe:

Under certain conditions, reinforced concrete pipe may be used. It shall be installed in accordance with the requirements of the New York State Department of Transportation Standard Specification Section 706-02 – Reinforced Concrete Pipe. Design considerations shall be approved by the Town Engineer prior to installation.

1. Pipe shall meet the requirements of A.A.S.H.T.O. M 170M.
2. Elastomeric gaskets shall be used at pipe joints.

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I. FENCING - 6' HIGH CHAIN LINK FENCE AND GATES

1) Description

The Contractor shall furnish and erect chain link fencing including posts, fittings and gates, of the type and size indicated in the plans and specifications and in locations shown on the plans and/or as directed by the Engineer.

2) Materials and Methods

a) FABRIC

Fence Fabric shall conform to the specifications for "Zinc-Coated Steel Chain Link Fence Fabric", ASTM Designation: A-392 as currently revised or herein modified. Galvanizing shall be done by the hot dip process and only after weaving. The fabric shall be woven to a height of 6 feet as indicated on the drawings with 2 " diamond mesh of 9 gauge wire with diameter of 0.148 inch and breakload strength of 1290 lb.

b) STEEL FENCE FRAMING

Steel Pipe: ASTM F 1083, standard weight schedule 40; minimum yield strength of 25,000 psi (170Mpa), sizes as indicated. Hot-dipped galvanized with minimum average of 1.8 oz/ft² (550 g/m²) of coated steel surface area.

c) CORNER AND END POSTS

Corner and End Posts shall be hot dipped galvanized 3" (2.875"o.d.) standard weight, weighing 5.70 lbs/ft. All terminal posts shall be set 42" into 12" diameter concrete footings set with a 2" slope to shed water and shall be completed with all necessary fittings including 1-5/8"o.d. pipe braces, according to specifications outlined in the top rail section. Each brace shall be securely held in compression by a 3/8" diameter galvanized truss rod and turnbuckle assembly, using two braces and truss assemblies for each corner post, one in each direction. All end connections to have bevel edged brace bands with galvanized rail end cups, 3/16" x 3/4" galvanized tension bars, and bevel edged tension bands spaced approximately 14" on center. All bolts to be galvanized 5/16"x1-1/4" carriage bolts with hex nuts.

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FENCING - 6' HIGH CHAIN LINK FENCE AND GATES (Continued)

d) **GATE POSTS**

Gate posts: Steel pipe ASTM F 1083 standard weight schedule 40; minimum yield strength of 25,000 psi. Size as indicated. Hot-dipped galvanized with a minimum 1.8 oz/ft² (550 kg/m²) of coated steel surface area.

<u>Gate Leaf Single Width</u>	<u>Post Size (Round)</u>	<u>Weight</u>
6 feet	4.00 inches	9.11 lb/ft
15 feet	6.625 inches	18.97 lb/ft

e) **LINE POSTS**

Line Posts shall be hot dipped galvanized 2-1/2" (2.375"o.d.) and shall be 2'-9" longer than the width of the fabric. Line posts shall be standard weight, weighing 3.65 lbs/ft. All line posts shall be set 36" into 10" diameter concrete footings crowned with a 2" slope to shed water. All posts to be completed with line post caps using 6 gauge (.192") aluminum ties to attach chain link fabric to posts with ties on approximately 14" centers. All posts to be spaced as ordered by the Engineer but not further than 10' apart.

f) **TOP AND BOTTOM RAILS**

Top and Bottom Rails shall be hot-dipped galvanized pipe 1-5/8" (1.660"o.d.) to be furnished in random lengths not less than 20 feet for top rail, joined with galvanized steel rail couplings for a rigid connection but allowing for expansion and contraction. Rail shall be standard weight, weighing 2.27 lbs/ft. Fabric to be tied to the rail and braces with 6 gauge (.192") aluminum ties with maximum spacings of 24". Middle and bottom rails shall be attached to posts as approved by the Engineer with galvanized steel couplings bolted through rail.

3) **CHAIN LINK FENCE ACCESSORIES**

Accessories shall meet the requirements of ASTM F 626. Provide all items required to complete fence system. Galvanize each ferrous metal item in accordance with ASTM A 153 and finish to match framing.

- a) Post caps: Formed steel, weathertight closure cap for tubular posts. Provide one cap for each post.

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FENCING - 6' HIGH CHAIN LINK FENCE AND GATES (Continued)

- b) Wire ties: 9 gauge (0.148") galvanized steel wire for attachment of fabric to line posts. Hog ring ties of 12-1/2 gauge (0.0985") for attachment of fabric to tension wire. Double wrap 13 gauge (0.092") for braces.
- c) Brace and tension (stretcher bar) bands: Pressed steel.
- d) Tension (stretcher) bars: One piece lengths equal to 2 inches less than full height of fabric with a minimum cross-section of 3/16"x 3/4" or equivalent fiber glass rod. Provide tension (stretcher) bars where chain link fabric meets terminal posts.
- e) Truss rods: Steel rods with minimum diameter of 5/16"
- f) Concrete: Minimum 28-day compressive strength of 3000 psi.

4) CHAIN LINK SWING GATES

- a) Gate frames: Fabricate chain link swing gates in accordance with ASTM F 900 using galvanized steel members, 2" square, weighing 2.60 lb/ft. Fusion or stainless steel welded connections forming rigid one-piece unit.
- b) Chain link fence fabric: Mesh and gauge to match fence. Install fabric with hook bolts and tension bars at all 4 sides. Attach to gate frame at not more than 15" on center.
- c) Hardware material: Hot dipped galvanized steel or malleable iron shapes to suit gate size.
- d) Hinges: Structurally capable of supporting gate leaf and allow opening and closing without binding. Non-lift-off hinge design shall permit gate to swing 180° inward.
- e) Latch: Forked type capable of retaining gate in closed position and have provision for padlock. Latch shall permit operation from either side of gate.
- f) Keeper: Provide keeper for each gate leaf over 5' wide. Gate keeper shall consist of mechanical device for securing free end of gate when in full open position.
- g) Double gates: Provide drop rod to hold inactive leaf. Provide gate stop pipe to engage center drop rod. Provide locking device and

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FENCING - 6' HIGH CHAIN LINK FENCE AND GATES (Continued)

padlock eyes as an integral part of the latch, requiring one padlock for locking both gate leaves.

- h) Gate posts: Steel pipe ASTM F 1083 standard weight schedule 40; minimum yield strength of 25,000 psi. Size as indicated. Hot-dipped galvanized with a minimum 1.8 oz/ft² (550 kg/m²) of coated steel surface area.

<u>Gate Leaf Single Width</u>	<u>Post Size (Round)</u>	<u>Weight</u>
6 feet	4.00 inches	9.11 lb/ft
15 feet	6.625 inches	18.97 lb/ft

- i) Concrete: Minimum 28-day compressive strength of 3,000 psi

7) **SUBMITTALS**

Submit manufacturer's shop drawings and data for all fencing material to the Engineer for approval prior to ordering.

8) **CONSTRUCTION DETAILS**

The Contractor shall verify areas to receive fencing are completed to final grades and elevations prior to installation. The Contractor shall ensure property lines and legal boundaries of work are clearly established. The Owner will provide horizontal control for the Contractor's use to establish property line.

9) **CHAIN LINK FENCE FRAMING INSTALLATION**

- a) Install chain link fence in accordance with ASTM F 567 and manufacturer's instructions.
- b) Locate terminal post at each fence termination and change in horizontal or vertical direction of 30° or more.
- c) Space line posts uniformly at 10' on center.
- d) Concrete set terminal, line and gate posts: Auger holes in firm, undisturbed or compacted soil. Holes shall have diameter 4 times greater than outside dimension of post, and depths approximately 6" deeper than post bottom. Excavate deeper as required for adequate support in soft and loose soils. Set post bottom 36" below

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FENCING - 6' HIGH CHAIN LINK FENCE AND GATES (Continued)

surface when in firm, undisturbed soil. Place concrete around posts in a continuous pour. Trowel finish around post. Slope to direct water away from posts.

- e) Check each post for vertical and top alignment, and maintain in position during placement and finishing operations.
- f) Bracing: Install horizontal pipe brace at mid-height, on each side of terminal posts. Firmly attach with fittings. Install diagonal truss rods at these points. Adjust truss rod, ensuring posts remain plumb.

10) CHAIN LINK FABRIC INSTALLATION

- a) Fabric: Install fabric on security side and attach so that fabric remains in tension after pulling force is released. Leave approximately 2" between finish grade and bottom selvage. Attach fabric with wire ties to line posts at 15" on center and to braces and rails at 24" on center.
- b) Tension (stretcher bars): Pull fabric taut; thread tension bar through fabric and attach to terminal posts with bands or clips spaced maximum of 15" on center.

11) GATE INSTALLATION

- a) Install gates plumb level, and secure for full opening without interference.
- b) Attach hardware by means that will prevent unauthorized removal.
- c) Adjust hardware for smooth operation.
- d) Set keeper, stops, sleeves into concrete.

12) ACCESSORIES

- a) Tie wires: Bend end of wire to minimize hazard to persons and clothing.
- b) Fasteners: Install nuts on side of fence opposite fabric side for added security.

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J. **ROAD OPENING STANDARDS**

1) **Introduction**

All work performed in the Town of Southampton Highway Right-of-Way shall adhere to the standards specified herein. There shall be no deviation from the approved Road Opening Permit and these Standards without the written approval of the Superintendent of Highways. The Applicant shall notify the Superintendent of Highways in writing no later than 5 working days prior to commencement of work. Absolutely no work shall be performed without written notification and 24 hours confirmation. The Applicant shall submit Emergency Phone Numbers where they can be contacted 24 hours a day in the event of any emergency.

2) **Pre-construction Meeting**

After review of Road Opening Permit Application and proposed scope of work, the Superintendent of Highways will determine if the project warrants a Pre-construction meeting. If a Pre-construction Meeting is required, the Applicant shall submit detailed plans for the project to the Superintendent of Highways. After review of the plans the Applicant will be notified of the time and date for the on-site Pre-construction meeting. Trenching, backfilling, right-of-way restoration, maintenance and protection of traffic and other relevant issues will be discussed. At the meeting the Applicant will be issued a *Road Opening Special Exception Listing* detailing the Applicant's restoration responsibilities.

3) **Trenching and Backfill**

a) **Description**

The work consists of excavating, backfilling and compacting trench excavation necessary to install underground pipe, duct, cable, etc. Trenching and backfilling shall also include those excavations necessary to set manholes, splice and pull vaults, bore pits, and other miscellaneous excavations required throughout the performance of the work.

b) **Trench excavation**

- 1) The Applicant shall comply with all current and applicable Occupational Safety and Health Administration (OSHA), federal, state and local rules and regulations governing the safety of workmen and the general public during excavation, installation, and backfilling operations.

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ROAD OPENING STANDARDS (Continued)

- 2) The Applicant shall be solely responsible for locating and identifying all existing utilities or structures within the right-of-way. This shall be done prior to the performance of the work. All information relative to the above shall be recorded and incorporated into the records in a manner accepted by the Superintendent of Highways.
 - 3) Trench width and depth shall be as indicated on the approved project plans and details. The minimum width for all trenches shall be two (2) feet. Requested deviation from the location indicated on the approved drawings shall be subject to the acceptance of the Superintendent of Highways. Trench excavation shall be accomplished by hand digging, mechanical trencher or backhoe at the discretion of the Applicant.
 - 4) Trenching shall be excavated in a straight line as practical between structures. The trench line shall not meander.
 - 5) Damage to adjacent works or property caused by surface runoff or dewatering, and as a result of construction operations, shall be the Applicant's responsibility and shall be corrected and restored immediately by the Applicant.
- c) **Backfill and Compaction**
- 1) Backfilling and compaction shall begin as soon as the corresponding trenching work is complete. All excavation shall be backfilled at the end of each working day unless otherwise approved by the Superintendent of Highways or his authorized representative. Backfill shall be compacted and shaped to the original contour and drainage.
 - 2) Backfill material shall be placed and thoroughly compacted in 6-inch lifts. Moisture content shall be adjusted as required to obtain the specified density with the compaction equipment used.
 - 3) Each lift shall be mechanically compacted using a vibratory plate compactor or other equipment subject to the approval of the Superintendent of Highways.

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ROAD OPENING STANDARDS (Continued)

- 4) Backfill shall be compacted to a minimum 95 percent of maximum density within 5 percent of optimum moisture content in accordance with ASTM D698.
- 5) All trenches that have not acceptably backfilled and compacted, or which settle after backfilling, shall be reopened to the depth of satisfactory compaction and refilled and recompactd as specified to the satisfaction of the Superintendent of Highways.
- 6) Trenches that require temporary asphalt patch shall be backfilled with N.Y.S.D.O.T. Asphalt Concrete Type 3 Dense Binder to a minimum compacted thickness of 4 inches. No cold patch will be permitted.

4) **Right-Of-Way Restoration**

a) **Description**

This section covers the work associated with the restoration of asphalt pavement, concrete sidewalk, concrete driveways, concrete curbs, shoulder areas, or the restoration of any areas disturbed during trenching operations.

b) **Concrete Sidewalk, Driveway and Curb Restoration**

- 1) When a trench is located parallel to a length of concrete sidewalk or curb and the limits of the trench are under or near enough to undermine the sidewalk or curb, the entire sidewalk or curb shall be removed.
- 2) When construction requires the removal and replacement of sidewalk or curb, removal shall extend to the nearest joint. Cutting of sidewalk or curb between joints will not be permitted.
- 3) Concrete sidewalk and curb replacement shall be in accordance with the Town of Southampton Sidewalk Contract specifications. Concrete sidewalks, driveways and aprons shall be replaced to the original lines and grades and match adjacent existing sections. In general, concrete sidewalks shall be a minimum of 4 inches thick with welded wire fabric reinforcement. Handicapped sidewalk ramps shall be constructed at all intersections and at locations designa-

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

ROAD OPENING STANDARDS (Continued)

ted by the Superintendent of Highways. Concrete driveways and aprons shall be a minimum of 6 inches thick with welded wire reinforcement. Concrete curbs shall be replaced in kind to the original lines and grades. All concrete shall have a minimum compressive strength of 3,500 psi at 28 days.

C) Asphalt Pavement Restoration

- 1) The existing asphalt pavement adjacent to the trench area shall be saw cut full depth prior to restoration. Cut edges of the asphalt pavement shall be thoroughly cleaned and a tack coat shall be uniformly applied to the cut edges prior to paving.
- 2) The Applicant shall backfill and compact the trench to an elevation 10 inches below the finished pavement grade.
- 3) The Applicant shall then place and compact 6 inches of stabilized soil aggregate subbase course in the trench. The stabilized soil aggregate subbase material shall meet the material requirements for the Town of Southampton Specification for Subbase Course.
- 4) The stabilized soil aggregate subbase material shall be compacted using a vibratory plate compactor or other equipment subject to the approval of the Superintendent of Highways.
- 5) The stabilizes soil aggregate subbase material shall be compacted to a minimum 95 percent of maximum density within 5 percent of optimum moisture content in accordance with ASTM D698.
- 6) The Applicant shall then place and compact a 2-1/2 inch lift of Asphalt Concrete Type 3 Dense Binder Course over the compacted stabilized soil aggregate subbase course.
- 7) After completing the placement and compaction of the Dense Binder Course, the Applicant shall place and compact a 1-1/2 inch lift of Asphalt Concrete Type 6 Top Course.
- 8) Joint sealant shall be applied on the pavement surface where new pavement meets existing pavement.

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
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ROAD OPENING STANDARDS (Continued)

- 9) All pavement markings shall be replaced in kind.
 - 10) During the site investigation at the Pre-construction meeting, the Superintendent of Highways may direct the Applicant to perform additional asphalt pavement restoration operations such as truing and leveling, key cuts/v-notch, crack filling, tack coat, and/or other operations in order to restore the asphalt pavement to a first class manner to the satisfaction of the Superintendent of Highways.
- D) Shoulder Restoration
- 1) Shoulder area disturbed by construction shall be restored within 2 weeks of construction completion. This includes, but is not limited to:
 - a) Removal of excess excavation spoils from the construction area.
 - b) Removal of trash and construction debris.
 - c) Filling, repairing, and stabilizing ground surfaces disturbed by construction.
 - d) Regrading, hand raking, or manipulating finished ground surface to the level of smoothness necessary for topsoil and seeding.
 - e) Topsoil and Seeding.
 - f) Treating, repairing, or replacing trees and shrubs damaged by construction activities.
 - 2) All earth surfaces disturbed by the Applicant's construction activities shall be revegetated by topsoil and seeding.
 - 3) The Applicant shall furnish, spread, and grade 4 inches of topsoil over the disturbed areas. Natural grades shall be re-established to the extent practical. Care shall be taken so as not to disturb natural surface drainage patterns.
 - 4) The topsoil area shall be fertilized and seeded as specified to establish a satisfactory stand of grass acceptable to the Superintendent of Highways. Seeding operations shall take

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
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ROAD OPENING STANDARDS (Continued)

place only during seasons when satisfactory results can be expected. The Applicant may be required to return after completion of construction to meet seasonal limits.

- 5) If in the opinion of the Superintendent of Highways an acceptable stand of grass has not been established, the Applicant will be directed to return to reseed the area as many times as necessary in order to establish an acceptable stand of grass.

5) **Maintenance of Traffic**

- a) The Applicant shall conduct the work to interfere as little as possible with public travel, whether vehicular or pedestrian. Whenever it is necessary to cross, obstruct, or close roads, driveways, or walks, whether public or private, the Applicant shall provide and maintain suitable and safe passages, detours, or other temporary expedients for the accommodation of public and private travel, and shall give reasonable notice to owners of private drives before interfering with them.
- b) In making open-cut street crossings, the Contractor shall not block more than of the street at a time. When required by the Superintendent of Highways, the Applicant shall widen the shoulder on the opposite side to facilitate traffic flow. Temporary asphalt surfacing shall be provided as necessary on shoulders as directed by the Superintendent of Highways.
- c) Materials stored upon or alongside public streets shall be so placed, and the work at all times shall be conducted, as to cause minimum obstruction and inconvenience to the traveling public.
- d) At times it will be necessary to divert vehicular or pedestrian traffic around construction areas, the Applicant shall furnish all signs, barricades, cones, drums, warning lights, flag persons, or other devices required in the New York State Manual of Uniform Traffic Control Devices (MUTCD), 2001 edition or as required by the Superintendent of Highways.

6) **Barricades and Lights**

- a) All roads, which are closed to traffic, shall be protected by effective barricades on which shall be placed acceptable warning signs. Barricades shall be located at the nearest intersecting road on each

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

ROAD OPENING STANDARDS (Continued)

side of the blocked section in accordance with the guidelines of the New York State Manual of Uniform Traffic Control Devices (MUTCD), 2001 edition.

- b) All open trenches and other excavations shall be provided with suitable barriers, signs, and lights to the extent that adequate protection is provided to the public. Obstructions, such as material piles and equipment shall be provided with similar warning signs and lights.
- c) All barricades and obstructions shall be illuminated by means of warning lights from sunrise to sunset or in cases of low visibility.

7) Protection of Public and Private Property

- a) The Applicant shall protect, shore, brace, support, and maintain all underground pipes, conduits, drains, and other underground construction uncovered or otherwise affected by the construction work. All pavement, surfacing, driveways, curbs, walks, buildings, utility poles, guy wires, fences, and other surface structures affected by construction together with all sod and shrubs, shall be restored to their original condition whether inside or outside of the right-of way. All replacements shall be made with new materials.
- b) The Applicant shall be responsible for all damages to roads, shoulders, ditches, embankments, culverts, bridges, and other public or private property, regardless of location or character, which may be caused by transporting equipment, materials, or men to or from the work or any part or site thereof. The Applicant shall make satisfactory and acceptable arrangements with the Superintendent of Highways over the damaged property concerning its repair or replacement, or payment of costs incurred in connection with the damage.
- c) All fire hydrants and water control valves shall be kept free from obstruction and available for use at all times.
- d) Street signs, mailboxes, and other items which conflict with construction shall be removed, stored, and reinstalled in a condition comparable to the condition prior to removal.

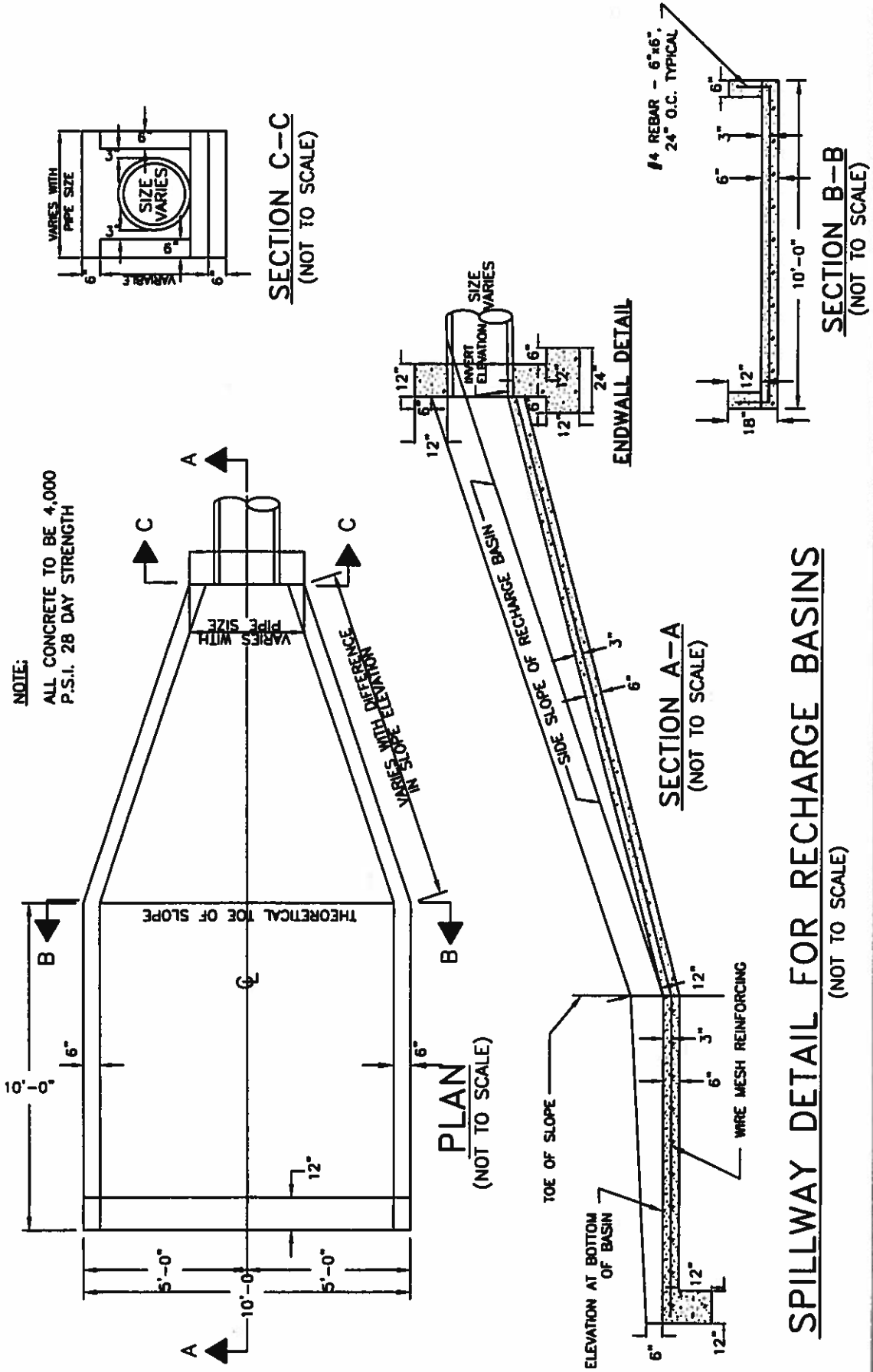
TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT
WILLIAM H. MASTERSON – SUPERINTENDENT OF HIGHWAYS

ROAD OPENING STANDARDS (Continued)

8. Emergency Protection

- a) Whenever, in the opinion of the Superintendent of Highways, the Applicant has not taken sufficient precautions for the safety of the public or adjacent structures or property, and whenever in the opinion of the Superintendent of Highways, an emergency has arisen and immediate action is considered necessary, the Southampton Town Highway Department, with or without notice to the Applicant, may provide suitable protection by causing work to be done and materials to be furnished and placed. The cost of such work and materials shall be charged back to the Applicant. The performance of such emergency work shall not relieve the Applicant of responsibility for damage, which may occur.

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT



SPILLWAY DETAIL FOR RECHARGE BASINS
(NOT TO SCALE)

TOWN OF SOUTHAMPTON
SUFFOLK COUNTY, NEW YORK

REVISED
JULY, 2002

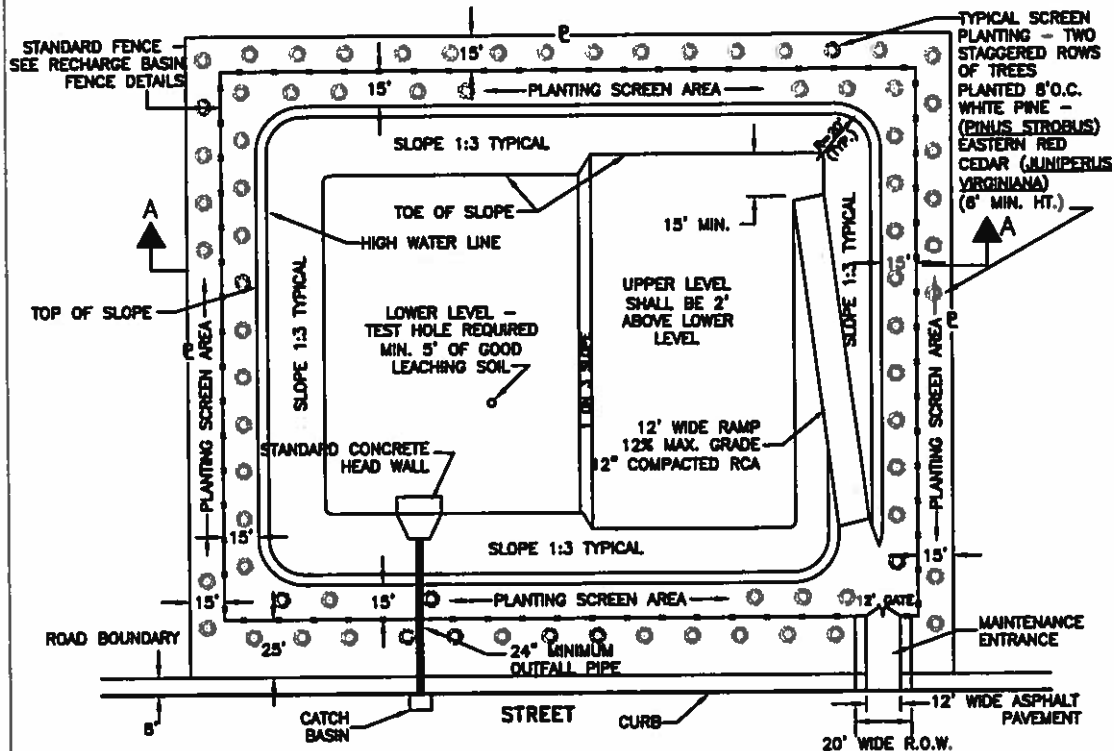
ROAD & DRAINAGE STANDARDS

TABLE "A"

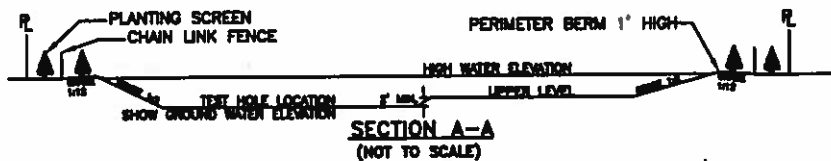
LOCATION OF ROAD BY ZONING DISTRICT	RIGHT-OF-WAY WIDTH	PAVEMENT WIDTH	CURB REQUIRED
R-10, R-15, R-20 & MULTI-FAMILY DISTRICTS			
HIGHWAYS	84-120 FEET	64 FEET	(b)
COLLECTOR STREET	70 FEET	40-50 FEET(a)	(b)
LOCAL STREET "A"	50-60 FEET	30-34 FEET	(b)
LOCAL STREET "B"	50 FEET	24-30 FEET	(b)
MARGINAL ROAD	50 FEET	30-34 FEET	(b)
COMMON DRIVEWAY	N/A	12-16 FEET	NO
OTHER RESIDENTIAL DISTRICTS			
HIGHWAYS	84-120 FEET	64 FEET	(b)
COLLECTOR STREET	70 FEET	40-50 FEET(a)	(b)
LOCAL STREET "A"	50-60 FEET	26-30 FEET	(b)
LOCAL STREET "B"	50 FEET	20-24 FEET	(b)
LANE	50 FEET	18 FEET	NO
MARGINAL ROAD	50 FEET	26-30 FEET	(b)
COMMON DRIVEWAY	N/A	12-16 FEET	NO
BUSINESS & INDUSTRIAL DISTRICTS			
HIGHWAYS	84-120 FEET	64 FEET	YES
COLLECTOR STREET	70 FEET	50 FEET	YES
LOCAL STREET "A"	60 FEET	34-40 FEET	(b)
LOCAL STREET "B"	-----	-----	---
MARGINAL ROAD	5 FEET	34-40 FEET	(b)
COMMON DRIVEWAY	N/A	24-34 FEET	(b)

(a) INCLUDES STABILIZED SHOULDERS

(b) AT THE DISCRETION OF THE PLANNING BOARD & SUPERINTENDENT OF HIGHWAYS



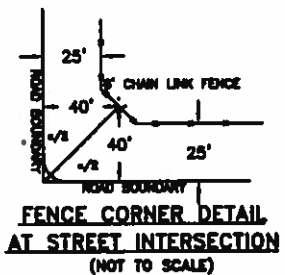
RECHARGE BASIN DETAIL
(NOT TO SCALE)



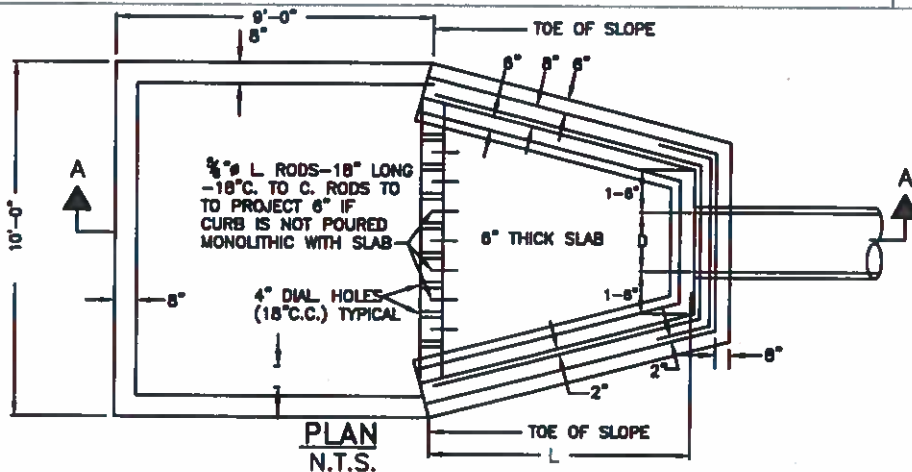
SECTION A-A
(NOT TO SCALE)

NOTES:

1. VEGETATION (TREES) SURROUNDING THE BASIN SITE SHALL BE KEPT IN NATURAL CONDITION.
2. TOPSOIL AND SEED FROM FENCE TO A LINE 4' DOWN FROM TOP OF SLOPE.
3. FENCING COMPLETE WITH GATE & LOCK SHALL BE CONSTRUCTED BEFORE STARTING EXCAVATION.
4. DIMENSIONS OF RECHARGE BASIN VARY IN ACCORDANCE WITH REQUIRED STORAGE CAPACITY & SHAPE OF PROPERTY.
5. DATA TO BE SHOWN SHALL INCLUDE LOWER LEVEL ELEVATION, HIGH WATER ELEVATION, AND ELEVATIONS ALONG SIDE SLOPES.



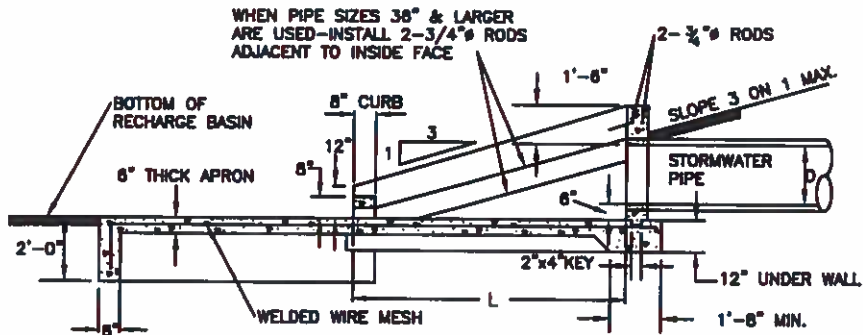
FENCE CORNER DETAIL AT STREET INTERSECTION
(NOT TO SCALE)



DIMENSIONS	
D	L
24"	9'-0"
30"	10'-6"
36"	12'-0"
42"	13'-6"

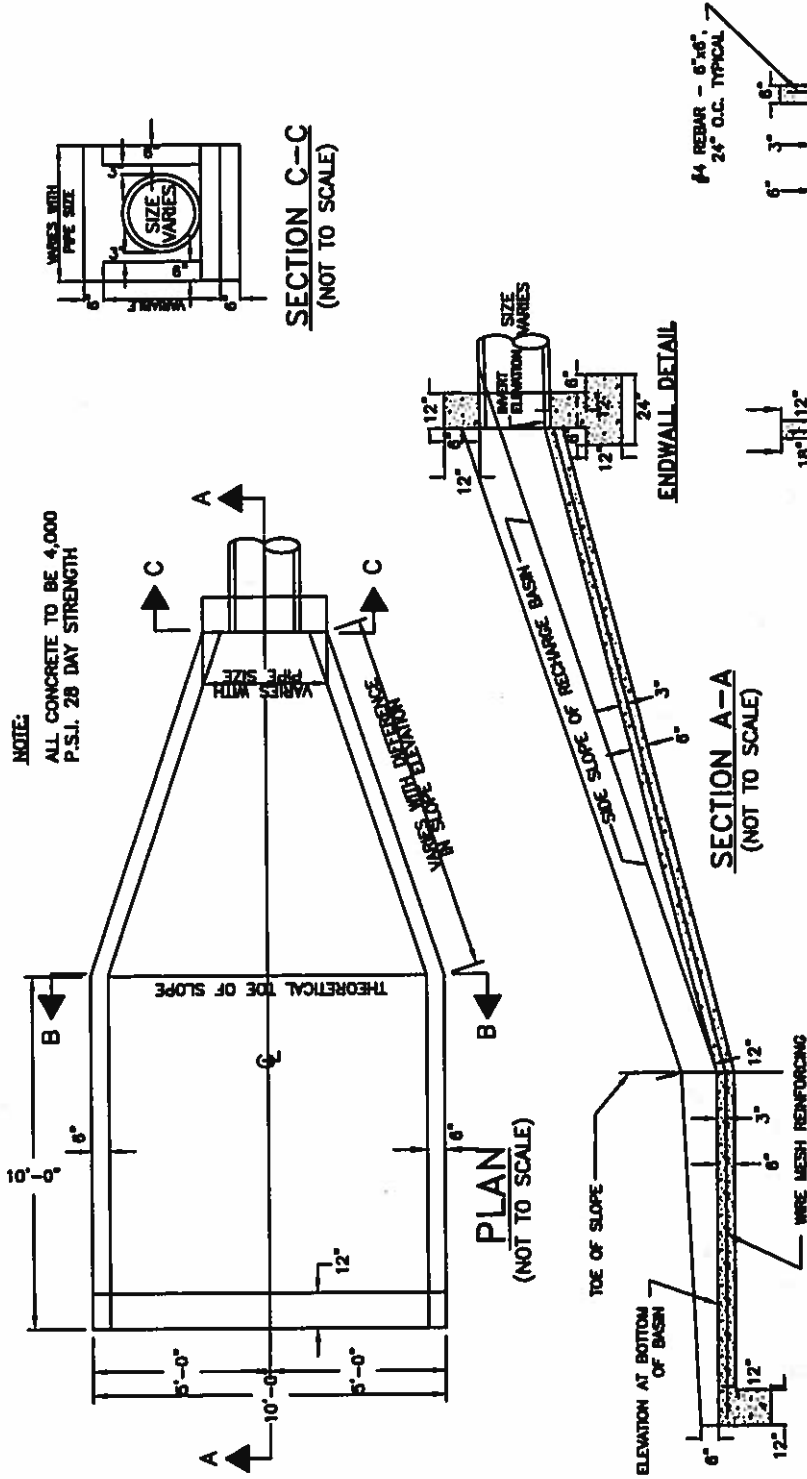
NOTE:

1. ALL CONCRETE TO BE 1,000 P.S.I. 28 DAY STRENGTH.
2. LENGTH "L" VARIES WITH PIPE DIAMETER.



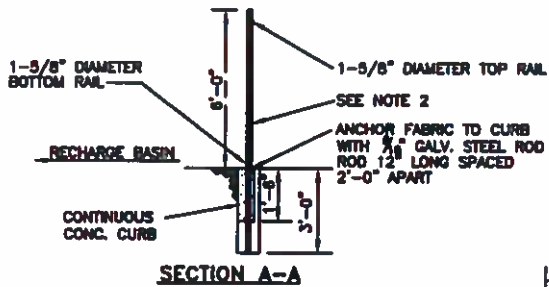
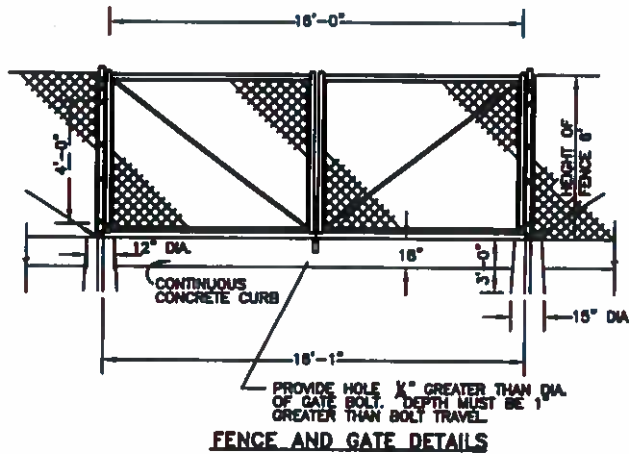
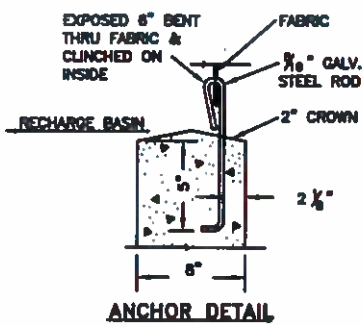
HEADWALL DETAILS FOR RECHARGE BASINS
(NOT TO SCALE)

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT



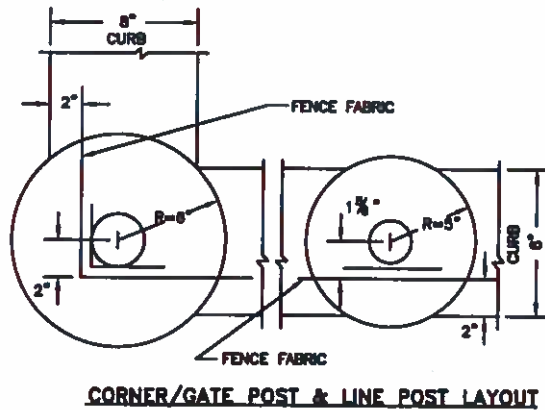
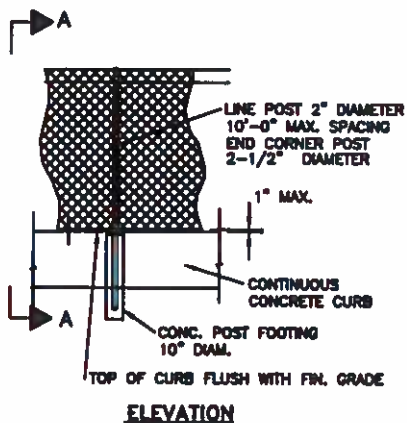
SPILLWAY DETAIL FOR RECHARGE BASINS
(NOT TO SCALE)

TOWN OF SOUTHAMPTON SUFFOLK COUNTY, NEW YORK	REVISED JULY, 2002	ROAD & DRAINAGE STANDARDS	5
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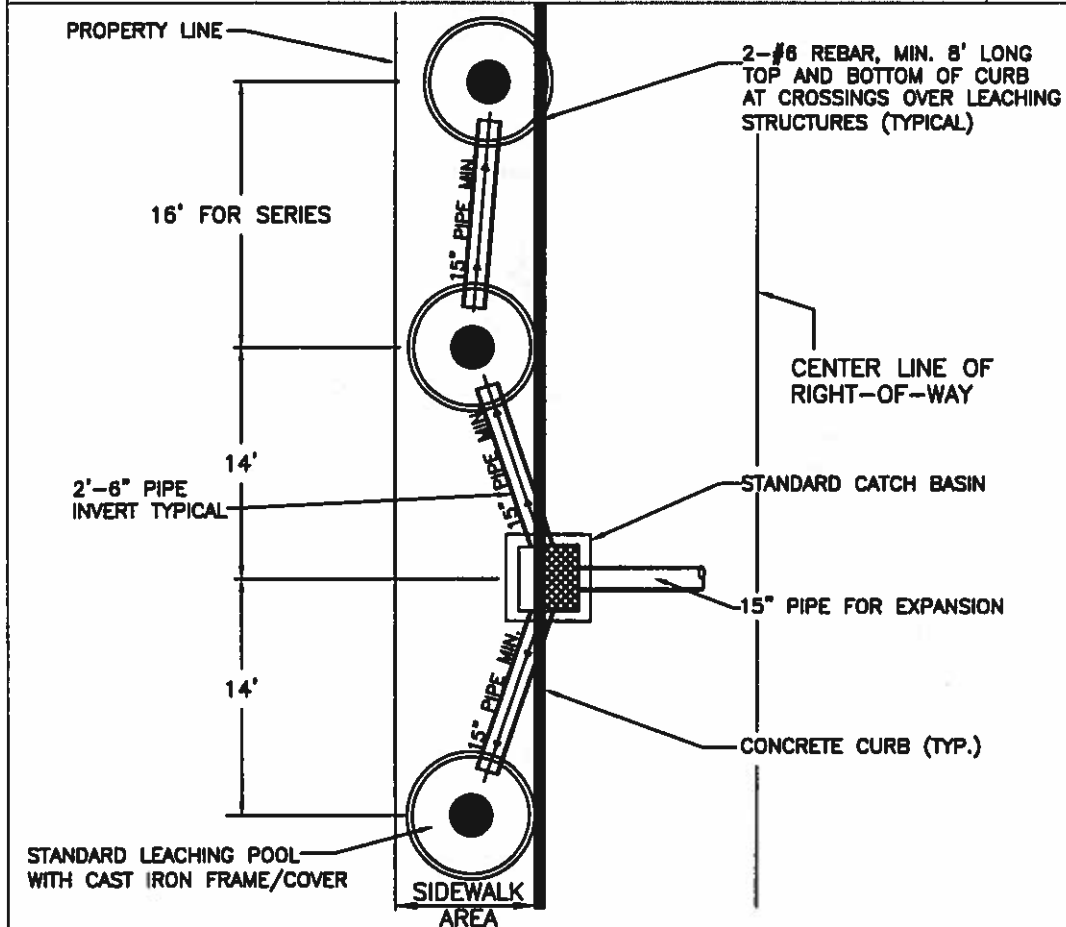
NOTES:

1. CONCRETE FOR CURB AND POST FOUNDATIONS SHALL HAVE A 3,500 P.S.I., 28 DAY COMPRESSIVE STRENGTH.
2. WHERE "NO CURB" IS SPECIFIED PROVIDE COIL SPRING WIRE 6" ABOVE BOTTOM OF WIRE.



RECHARGE BASIN FENCE DETAILS

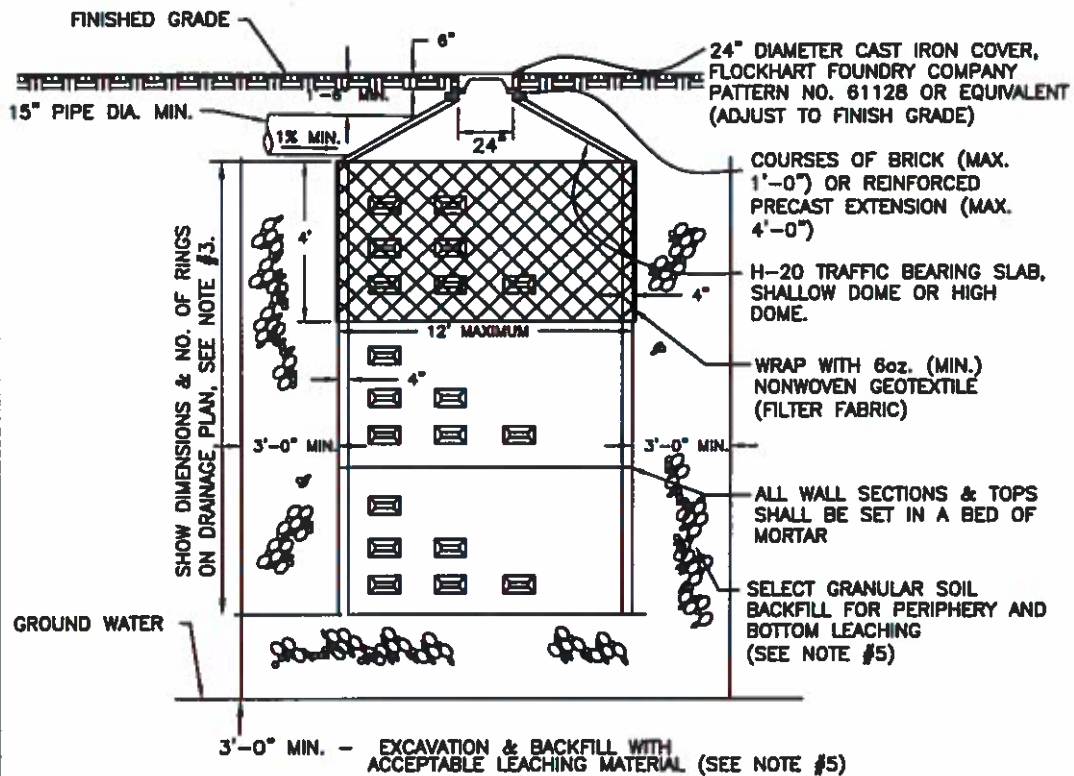
(NOT TO SCALE)



TYPICAL LEACHING BASIN PLAN
(NOT TO SCALE)

NOTE:

1. FOR SECTION, SEE TYPICAL LEACHING POOL DETAIL.
2. PIPE MAY EXIT FROM ANY SIDE.
3. BACK FILL SHALL BE COMPACTED IN 8" LIFTS.
4. THE PERIPHERAL JOINT BETWEEN PIPE AND LEACHING POOL SHALL BE GROUTED WITH A MIXTURE OF 1 PART CEMENT MORTAR & 2 PARTS SAND. WHERE THE OPENING IS TOO LARGE, AGGREGATE OR BRICK SHALL BE USED.
5. WHERE DEPTH IS LIMITED BY GROUND WATER, THE EQUIVALENT CAPACITY SHALL BE ACHIEVED BY ADDITIONAL LEACHING BASINS CONNECTED IN SERIES.

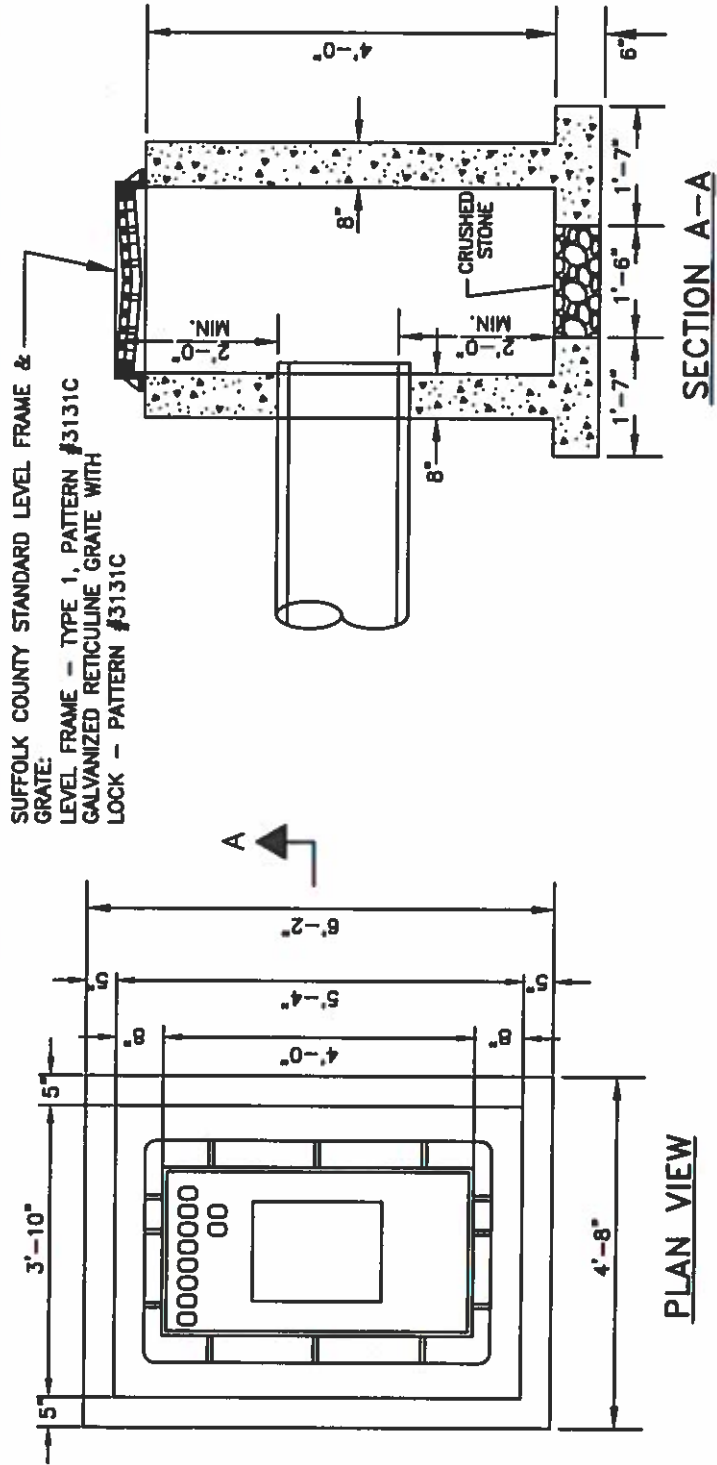


TYPICAL LEACHING POOL DETAIL
(NOT TO SCALE)

NOTES:

1. LEACHING POOLS SHALL BE PRECAST CONCRETE CEMENT STRUCTURES AS MANUFACTURED BY SUFFOLK PRECAST INC. OR EQUIVALENT. SHOP DRAWINGS SHALL BE SUBMITTED TO THE TOWN ENGINEER FOR APPROVAL.
2. ALL CONCRETE CEMENT USED IN THE MANUFACTURING OF PRECAST STRUCTURES SHALL DEVELOP A MINIMUM COMPRESSIVE STRENGTH OF 4000 P.S.I. IN 28 DAYS.
3. SHOP DRAWINGS OF PRECAST MANHOLES & CATCH BASINS MUST BE SUBMITTED TO THE TOWN ENGINEER FOR APPROVAL.
4. REQUESTS FOR DEVIATIONS FROM STANDARD DRAINAGE INSTALLATIONS SHOWN MUST BE MADE IN WRITING & MUST INCLUDE DRAINAGE CALCULATIONS.
5. ALL DRAINAGE STRUCTURES SHALL BE PLACED IN ACCEPTABLE LEACHING SOILS AS APPROVED BY THE TOWN ENGINEER. IN THE EVENT POOR LEACHING MATERIAL IS ENCOUNTERED, EXCAVATION IS TO BE CONTINUED TO SUITABLE MATERIAL & BACK FILLED AS DETAILED.

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT



SUFFOLK COUNTY STANDARD LEVEL FRAME & GRATE:
 LEVEL FRAME - TYPE 1, PATTERN #3131C
 GALVANIZED RETICULINE GRATE WITH LOCK - PATTERN #3131C

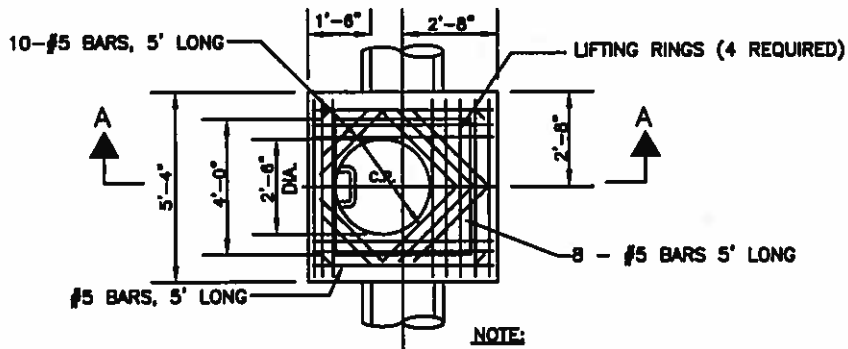
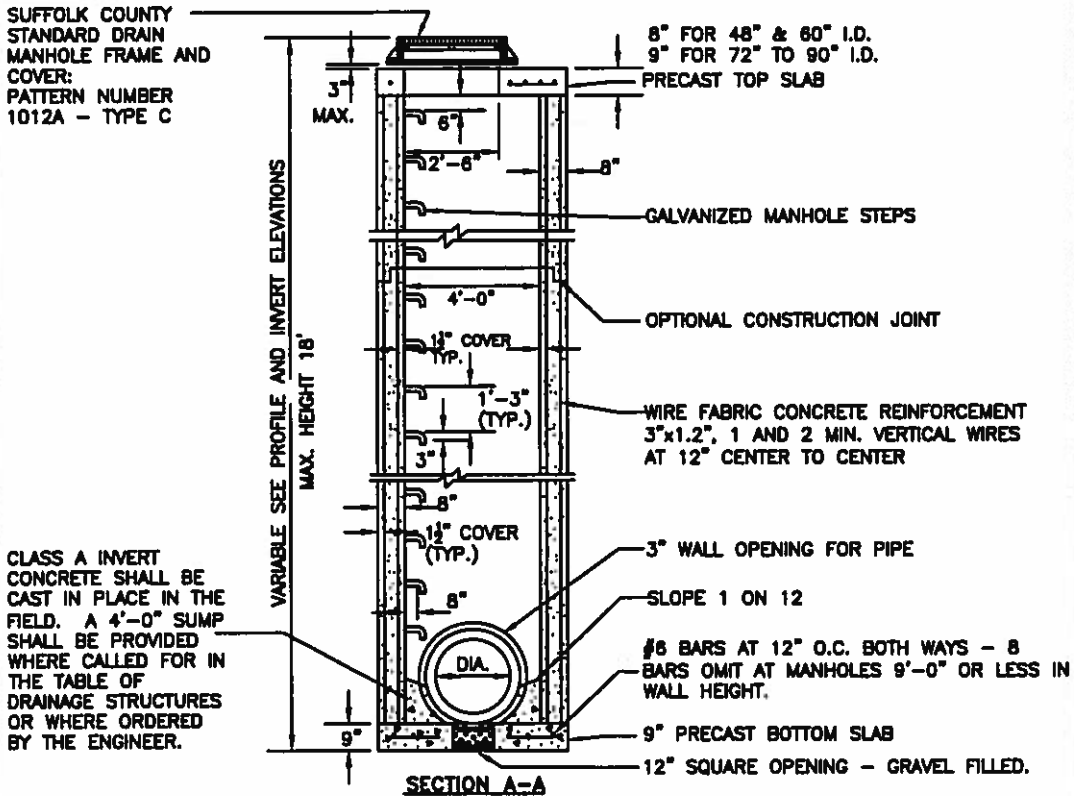
STANDARD DROP INLET STRUCTURE

(NOT TO SCALE)

1. CONCRETE TO BE 4,000 P.S.I. AT 28 DAYS.
 RIGID PIPE OPENINGS AS REQUIRED.

TOWN OF SOUTHAMPTON SUFFOLK COUNTY, NEW YORK	REVISED JULY, 2002	ROAD & DRAINAGE STANDARDS	10
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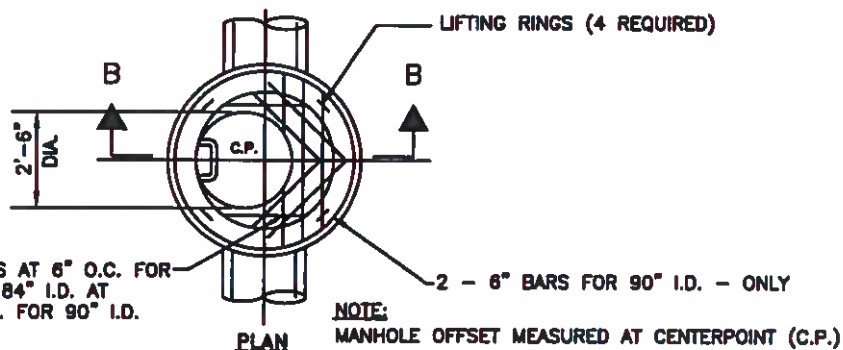
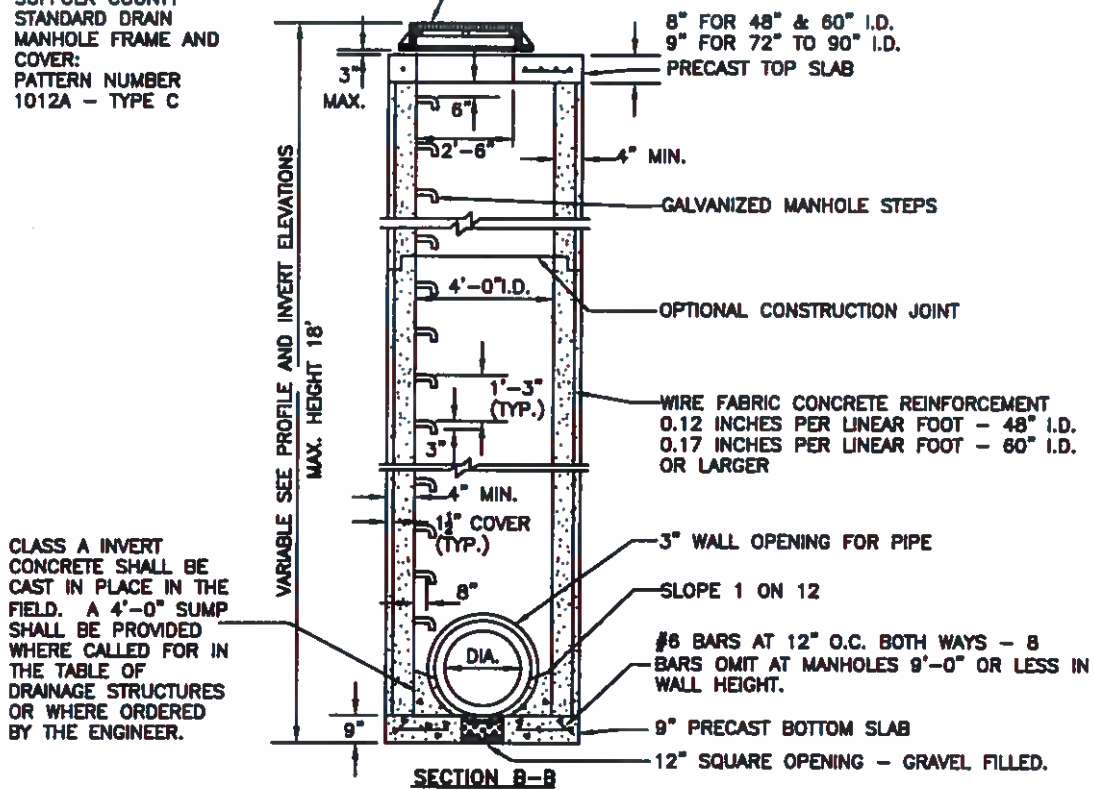
SUFFOLK COUNTY
STANDARD DRAIN
MANHOLE FRAME AND
COVER:
PATTERN NUMBER
1012A - TYPE C



NOTE:
MANHOLE OFFSET MEASURED AT CENTERPOINT (C.P.)

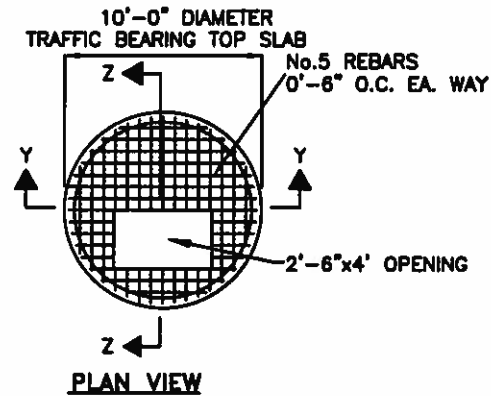
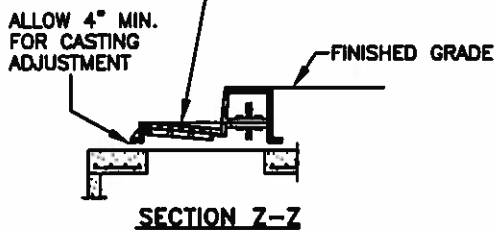
MANHOLE TYPE "A"
(NOT TO SCALE)

SUFFOLK COUNTY
STANDARD DRAIN
MANHOLE FRAME AND
COVER:
PATTERN NUMBER
1012A - TYPE C



MANHOLE TYPE "B"
(NOT TO SCALE)

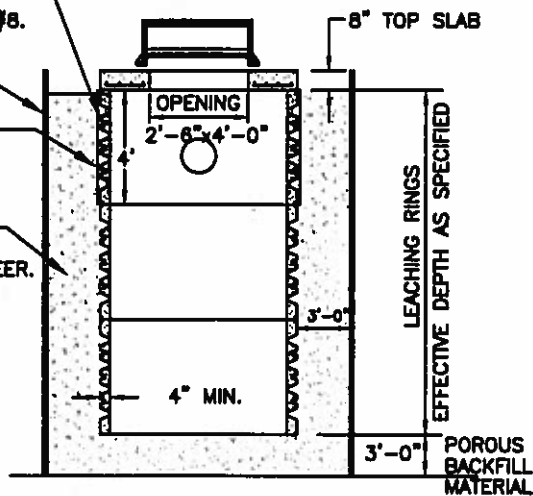
SUFFOLK COUNTY STANDARD CURB INLET
 FRAME & GRATE:
 FRAME - TYPE 2, PATTERN #2605
 NON-MOUNTABLE CURB BOX (7-1-7)
 GALVANIZED RETICULINE GRATE WITH LOCK
 - PATTERN #2605



STEEL REINFORCED WELDED
 WIRE FABRIC 6" x 12" #6x#8.
 SHEET PILING AS REQUIRED
 WRAP WITH 8oz. (MIN.) NONWOVEN
 GEOTEXTILE (FILTER FABRIC)
 EXCAVATE AND BACKFILL 3 FEET AROUND
 AND BELOW LEACHING RINGS WITH SUITABLE
 POROUS MATERIAL AS APPROVED BY THE ENGINEER.



DRAIN OPENING DETAIL
 (NOT TO SCALE)

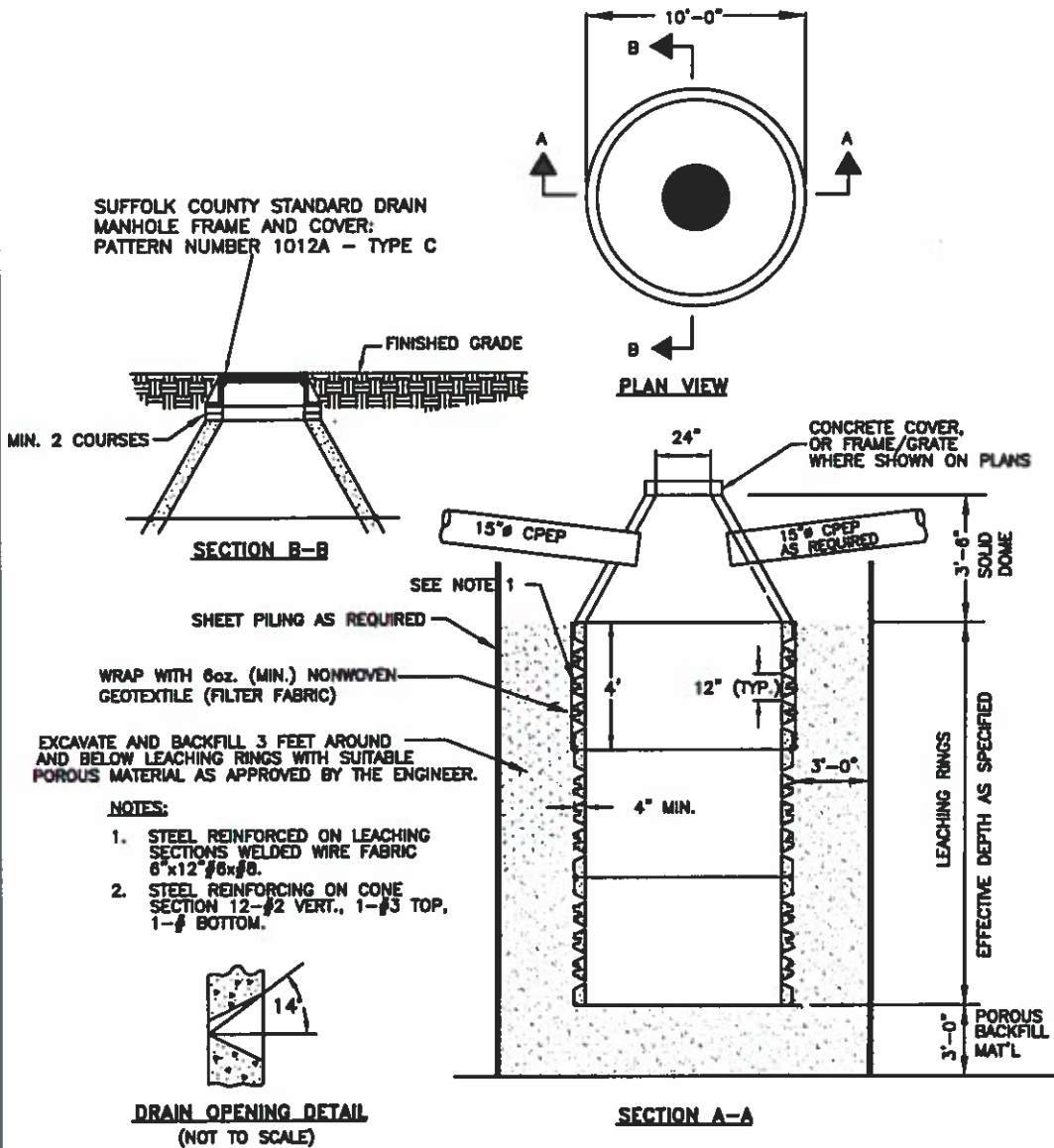


SECTION Y-Y

CURB INLET / 10'Ø LEACHING CATCH BASIN (C.I.-L.C.B.)
 NOT TO SCALE

LEACHING BASIN NOTES:

1. LEACHING POOLS SHALL BE PRECAST CONCRETE CEMENT STRUCTURES AS MANUFACTURED BY SUFFOLK PRECAST INC. OR EQUIVALENT. SHOP DRAWINGS SHALL BE SUBMITTED FOR APPROVAL.
2. ALL CONCRETE CEMENT USED IN THE MANUFACTURING OF PRECAST STRUCTURES SHALL DEVELOP A MINIMUM COMPRESSIVE STRENGTH OF 4000 P.S.I. IN 28 DAYS.
3. ALL DRAINAGE STRUCTURES SHALL BE PLACED IN ACCEPTABLE LEACHING SOILS AS APPROVED BY THE ENGINEER. IN THE EVENT POOR LEACHING MATERIAL IS ENCOUNTERED, EXCAVATION IS TO BE CONTINUED TO SUITABLE MATERIAL & BACK FILLED AS DETAILED.
4. NO TRENCH, PIT, OR OTHER EXCAVATION SHALL BE LEFT OPEN AND UNATTENDED. EXCAVATIONS SHALL BE PROTECTED AS DIRECTED BY THE ENGINEER WITH FENCE, BARRICADES, AND OTHER APPROVED METHODS.
5. INVERT ELEVATIONS MAY BE ADJUSTED BY THE ENGINEER IN THE FIELD TO MEET FIELD CONDITIONS TO AVOID CONFLICT WITH UTILITIES AS ORDERED BY THE ENGINEER.
6. ALLOW FOUR (4) INCHES FOR FINAL ADJUSTMENT OF CASTING. ADEQUATE BEARING WILL BE PROVIDED USING CONCRETE BLOCK, BRICK, AND/OR 1:2 MORTAR MIX.
7. THE CONTRACTOR SHALL PROVIDE ADEQUATE SHEETING AND SHORING DURING TRENCHING TO INSURE THE SAFETY OF WORKMEN AND THE GENERAL PUBLIC EXPOSED TO THE HAZARDS OF FALLING AND SLIDING MATERIAL IN CONFORMANCE WITH THE REQUIREMENTS OF TITLE 29 CODE OF FEDERAL REGULATIONS, PART 1926, SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION (OSHA).
8. BACKFILL AND COMPACTION SHALL BE IN 6" LIFTS.

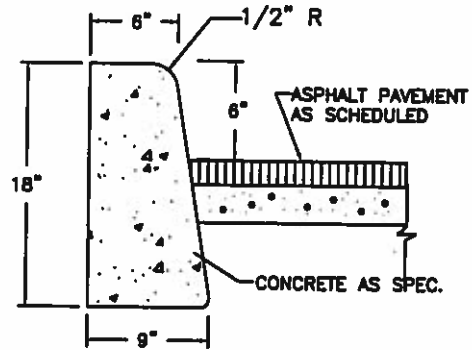


10'Ø LEACHING CATCH BASIN DETAIL (L.C.B.)

NOT TO SCALE

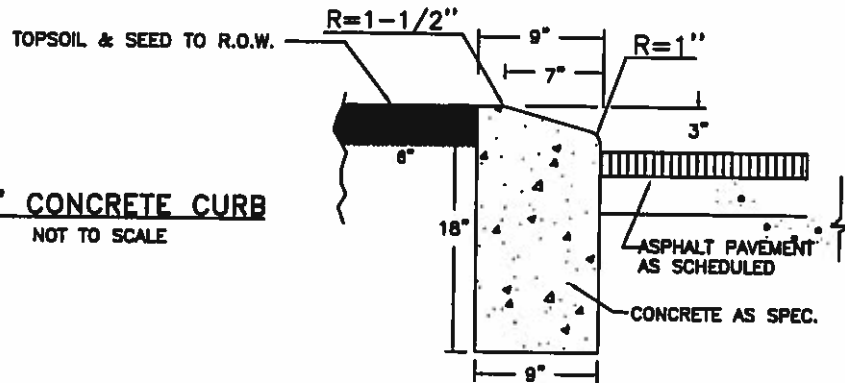
TYPE "A" CONCRETE CURB

NOT TO SCALE



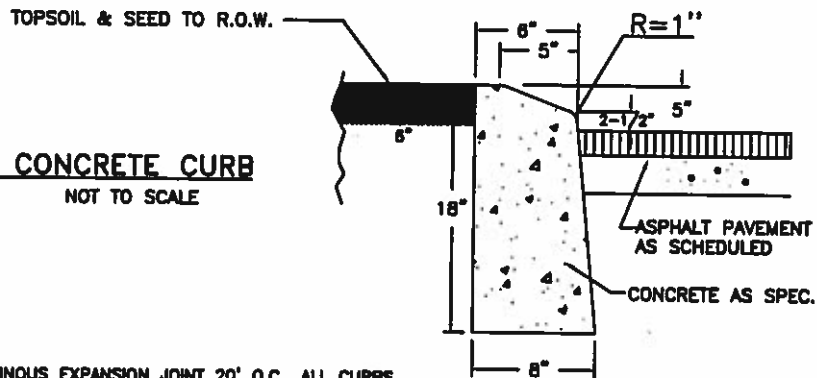
TYPE "B" CONCRETE CURB

NOT TO SCALE



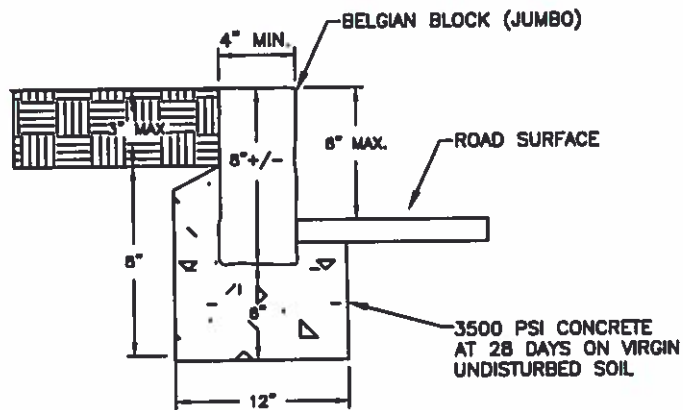
TYPE "C" CONCRETE CURB

NOT TO SCALE



NOTES:

1. INSTALL BITUMINOUS EXPANSION JOINT 20' O.C., ALL CURBS.

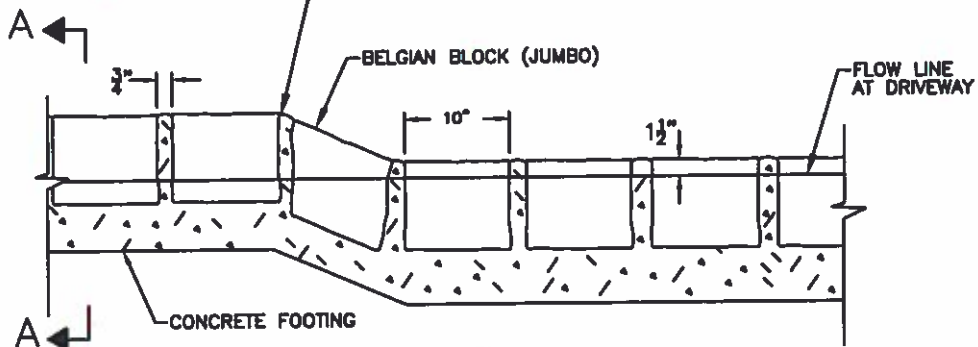


SECTION A-A

NOTES:

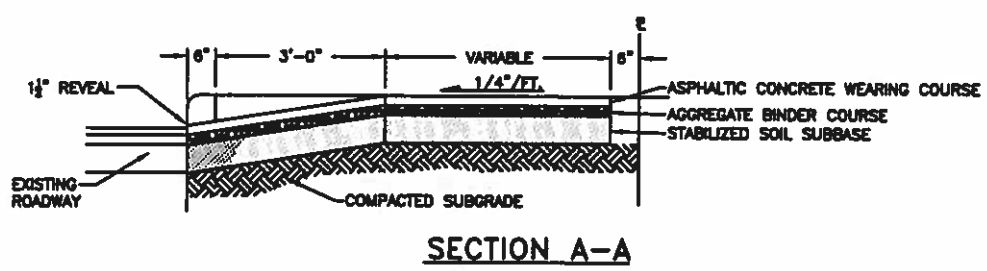
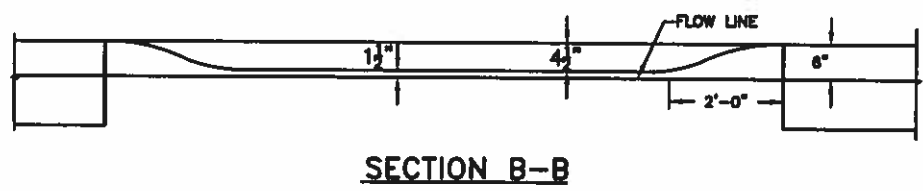
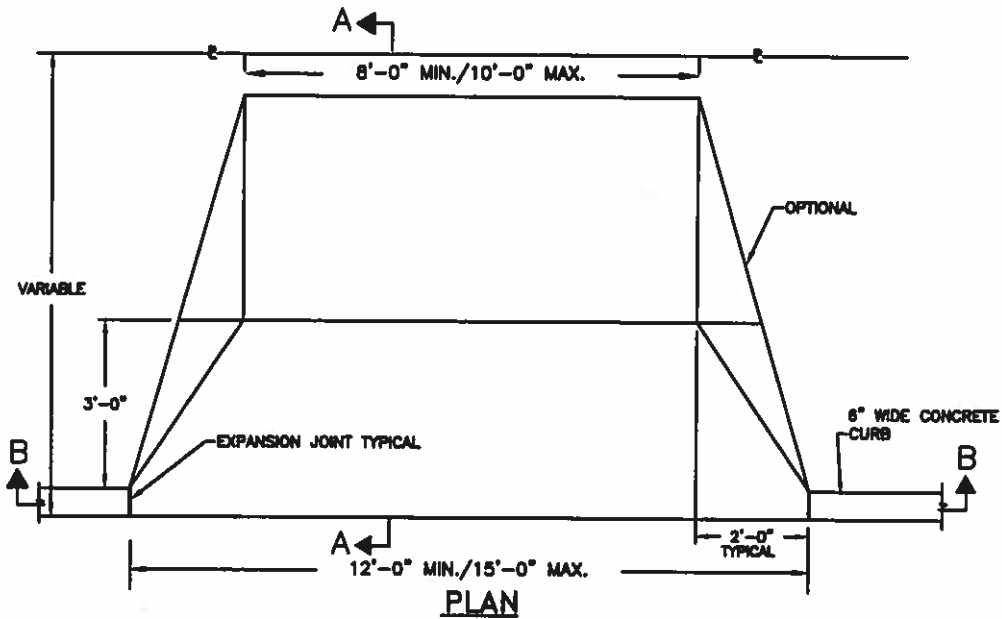
1. CURBS SHALL BE SET TRUE TO LINE AND GRADE ON POURED CONCRETE FOUNDATION TO PROVIDE A FIRM AND UNIFORM BEARING.
2. CURB DEPRESSED AT DRIVEWAYS $1\frac{1}{2}$ " MAXIMUM ABOVE FLOW LINE.

TYPE N MORTAR JOINT SMOOTH CONVEX FINISH (TYPICAL)

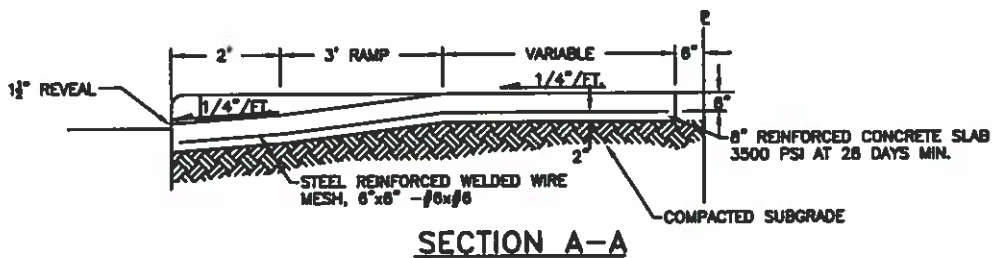
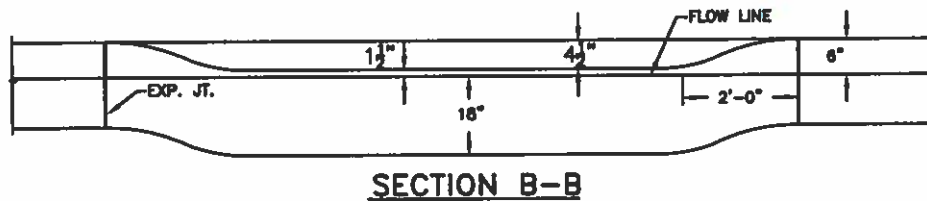
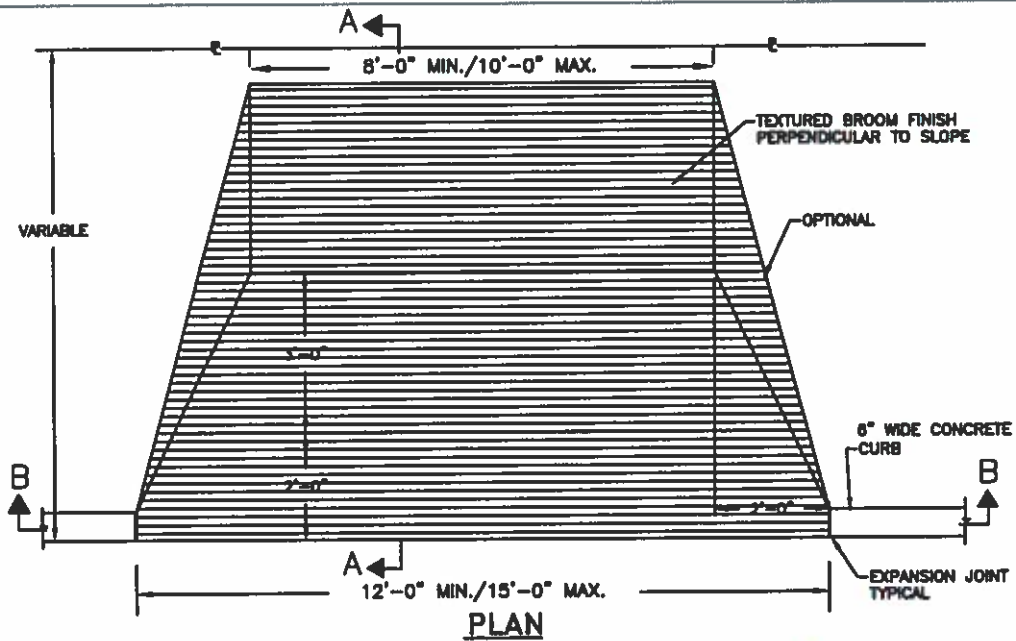


SETTING DETAIL

BELGIAN BLOCK CURB
(NOT TO SCALE)

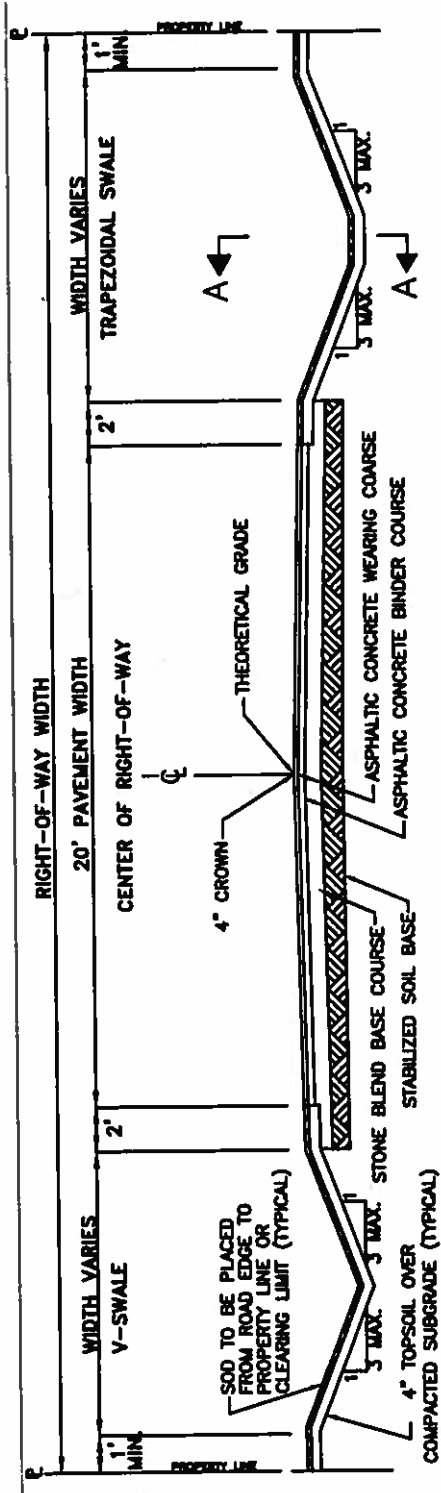


ASPHALT APRON & CONCRETE CURB
(NOT TO SCALE)



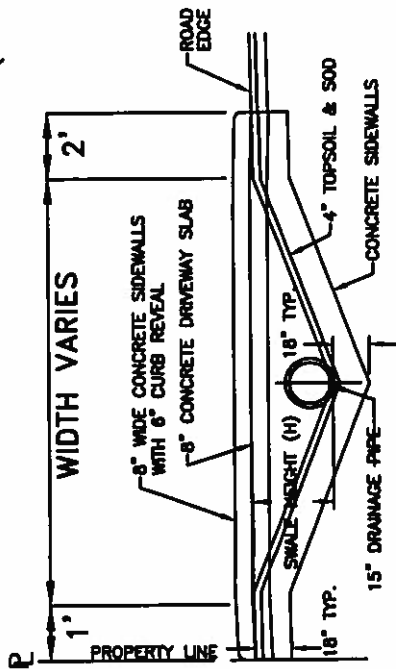
CONCRETE APRON & CONCRETE CURB
(NOT TO SCALE)

TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT

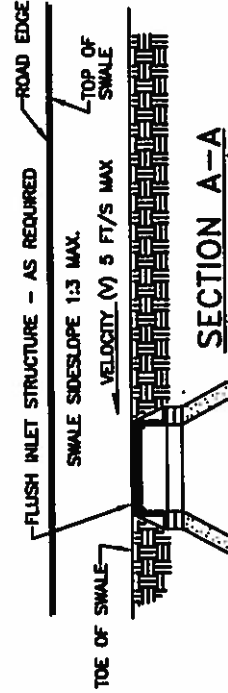


TYPICAL COUNTRY ROAD SECTION
(NOT TO SCALE)

- NOTES:
1. AT DRIVEWAY CROSSINGS - LEACHING CATCH BASIN STRUCTURES SHALL BE INSTALLED IN THE SWALE IF THE TOTAL FLOW (Q) IS GREATER THAN 2 CF/S.
 2. DESIGN VELOCITY (V) FOR SOD LINED SWALES SHALL BE 5 FT/S MAXIMUM AND OR AS APPROVED BY THE TOWN ENGINEER.
 3. MAXIMUM FLOW DEPTHS IN THE SWALES SHALL BE 90% OF DESIGN HEIGHT (BASED ON 10-YEAR STORM).



TYPICAL SECTION AT DRIVEWAY
(NOT TO SCALE)

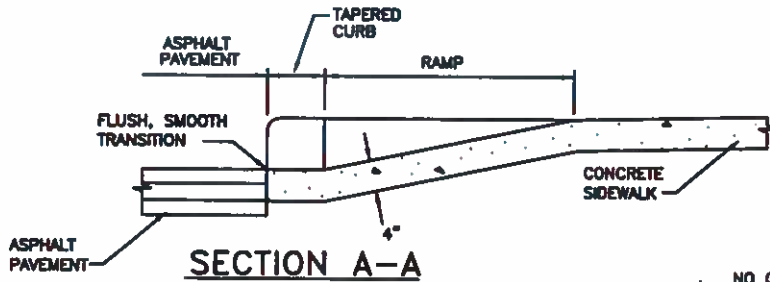


SECTION A-A
(NOT TO SCALE)

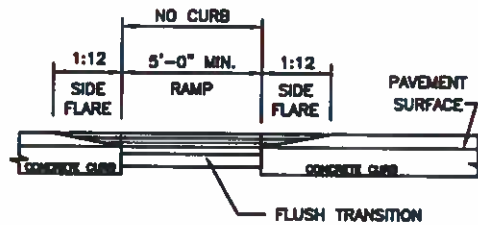
TOWN OF SOUTHAMPTON
SUFFOLK COUNTY, NEW YORK

REVISED
JULY, 2002

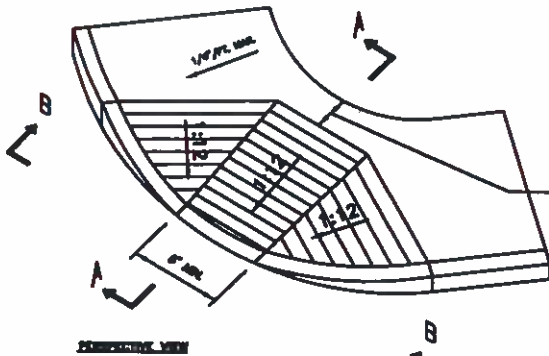
ROAD & DRAINAGE STANDARDS



SECTION A-A

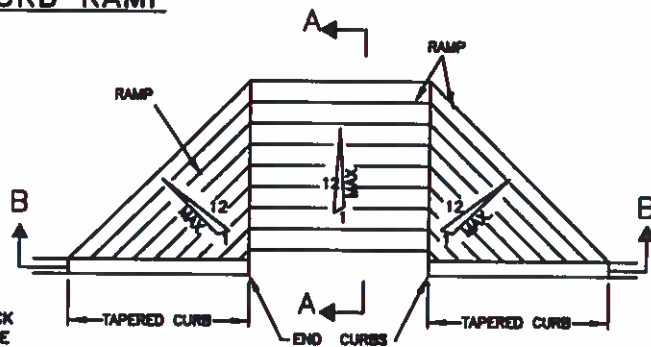


SECTION B-B



CORNER SIDEWALK CURB RAMP
(NOT TO SCALE)

NOTE : IF THE DISTANCE FROM THE TOP OF RAMP TO THE BACK EDGE OF THE SIDEWALK IS 4'-0" OR GREATER, THE SLOPE ON THE SIDE FLARES MAY BE 1:10

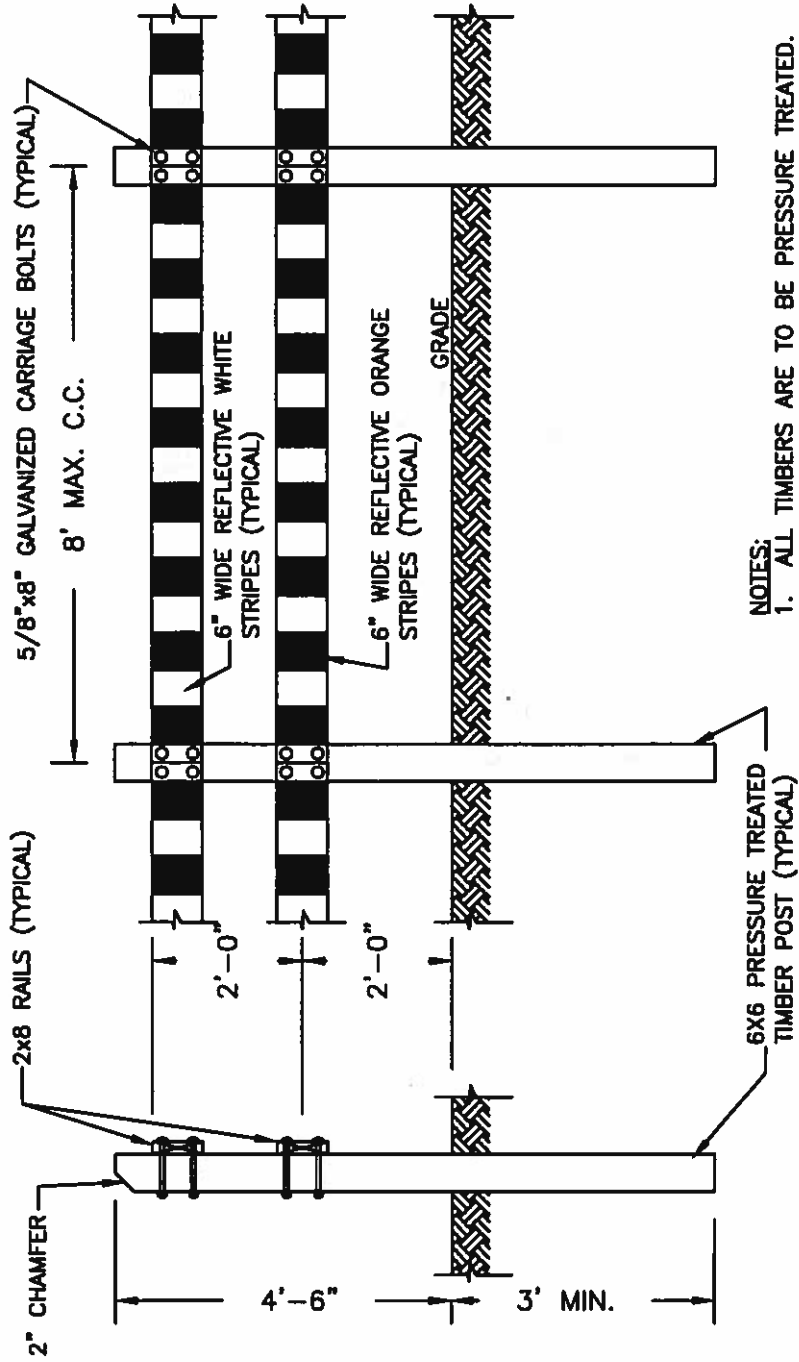


SIDEWALK CURB RAMP
(NOT TO SCALE)

NOTES:

1. IF THE DISTANCE FROM TOP OF RAMP TO BACK EDGE OF SIDEWALK IS 6'-0" OR GREATER, THE SLOPE ON THE SIDE FLARES MAY BE 1:10.
2. SURFACE TEXTURE OF RAMP SHALL BE DEEP GROVE, 1/2" WIDE BY 1/4" DEEP, 1" CENTERS TRANSVERSE TO THE RAMP.

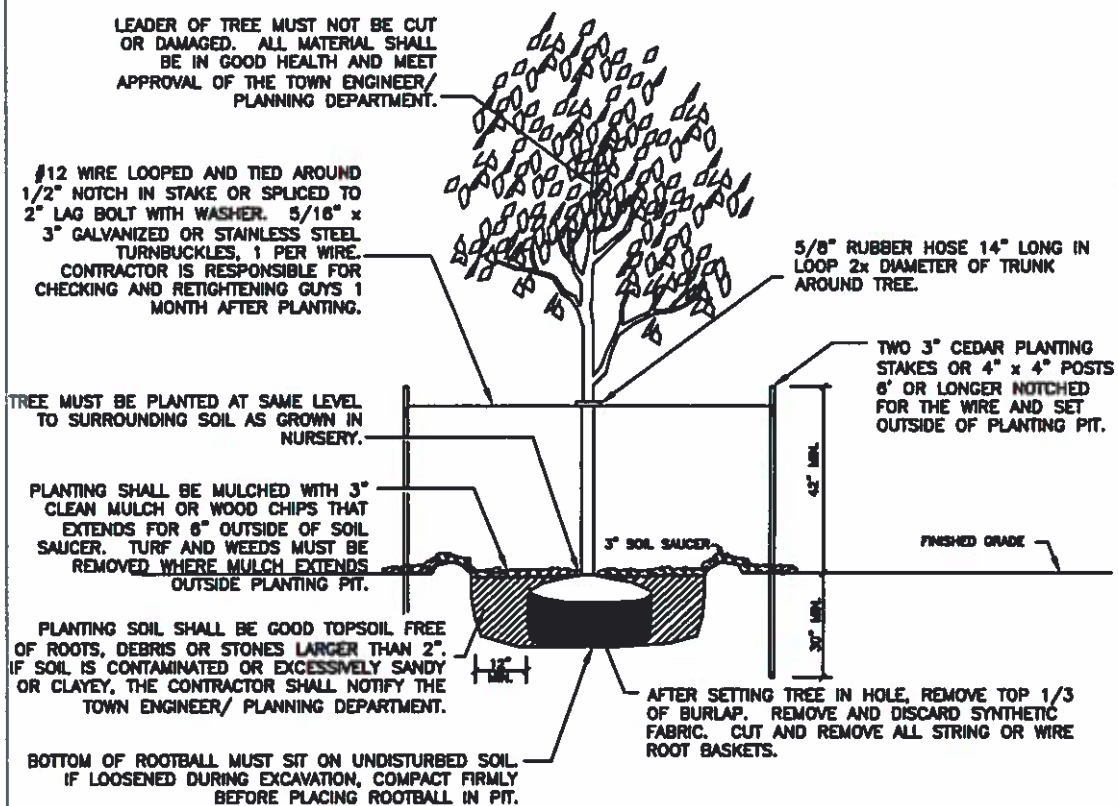
TOWN OF SOUTHAMPTON HIGHWAY DEPARTMENT



- NOTES:
1. ALL TIMBERS ARE TO BE PRESSURE TREATED.
 2. UNLESS OTHERWISE NOTED, ALL EXPOSED SURFACES SHALL BE PAINTED WHITE WITH THREE COATS OF EXTERIOR GRADE PAINT.
 3. ALL HARDWARE TO BE GALVANIZED.
 4. EACH CARRIAGE BOLT TO HAVE (1) FLAT WASHER, (1) LOCK WASHER & (1) HEX NUT

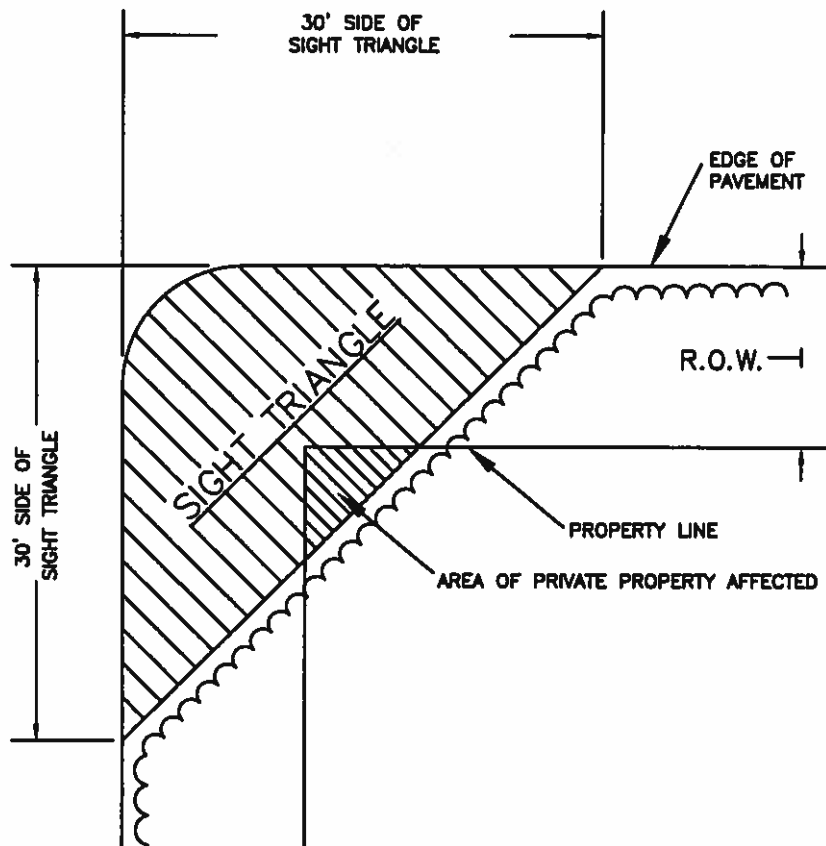
TIMBER BARRICADE DETAIL
(NOT TO SCALE)

21	ROAD & DRAINAGE STANDARDS
REVISED JULY, 2002	TOWN OF SOUTHAMPTON SUFFOLK COUNTY, NEW YORK



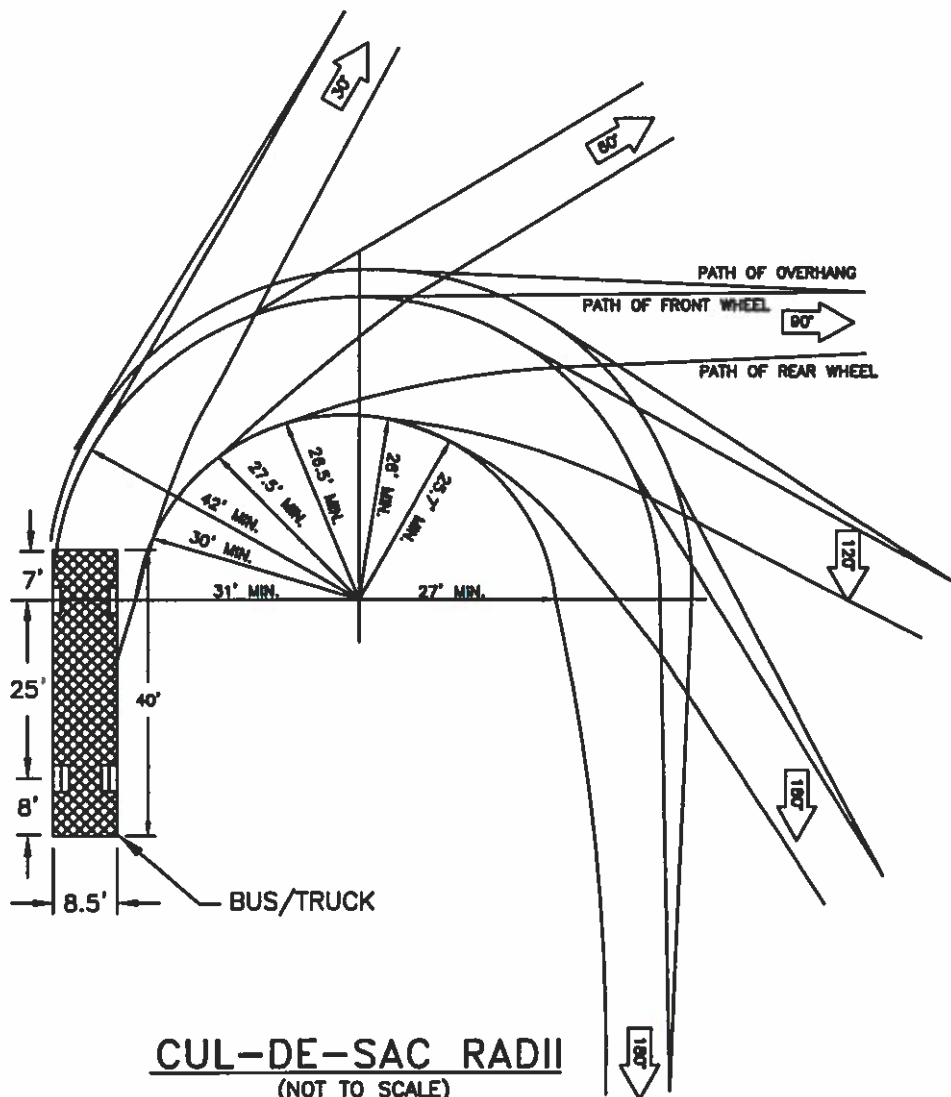
STAKED TREE AND BALL ROOT COVERING DETAIL

(NOT TO SCALE)



CORNER SIGHT TRIANGLE

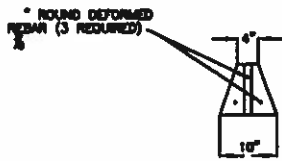
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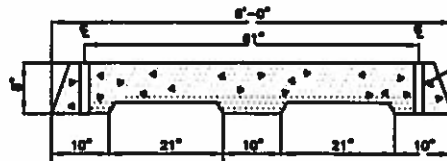
CUL-DE-SAC RADII
(NOT TO SCALE)

NOTES:

1. THIS TURNING TEMPLATE SHOWS THE MINIMUM TURNING PATH FOR BUS/TRUCK VEHICLES (FIGURE 11-3) IN ACCORDANCE WITH THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICES (AASHTO).
2. THE PATHS SHOWN ARE FOR THE LEFT FRONT OVERHANG AND THE OUTSIDE REAR WHEEL. THE LEFT FRONT WHEEL FOLLOWS THE CIRCULAR CURVE, HOWEVER, ITS PATH IS NOT SHOWN.



SECTION

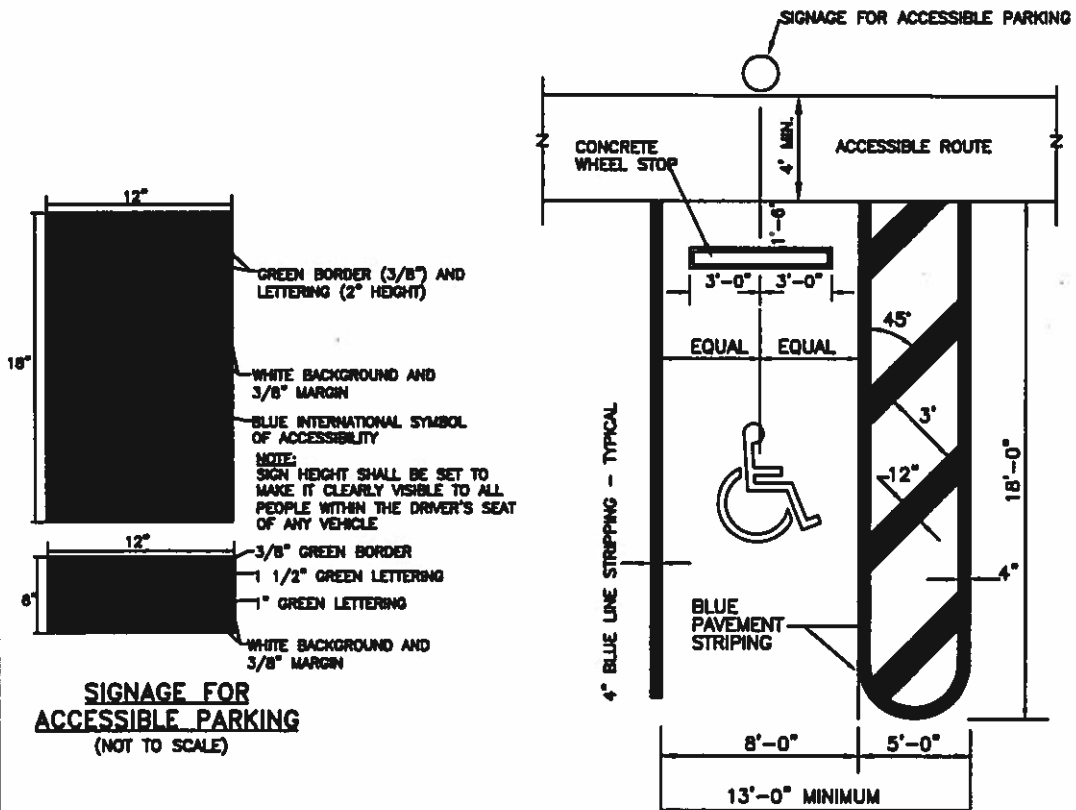


ELEVATION

NOTE:
INSTALL EACH WITH 2-#8 REBAR
AT A 3'-0" BURIAL AND TOP
FLUSH WITH TOP OF WHEELSTOP.

CONCRETE WHEEL STOP DETAIL

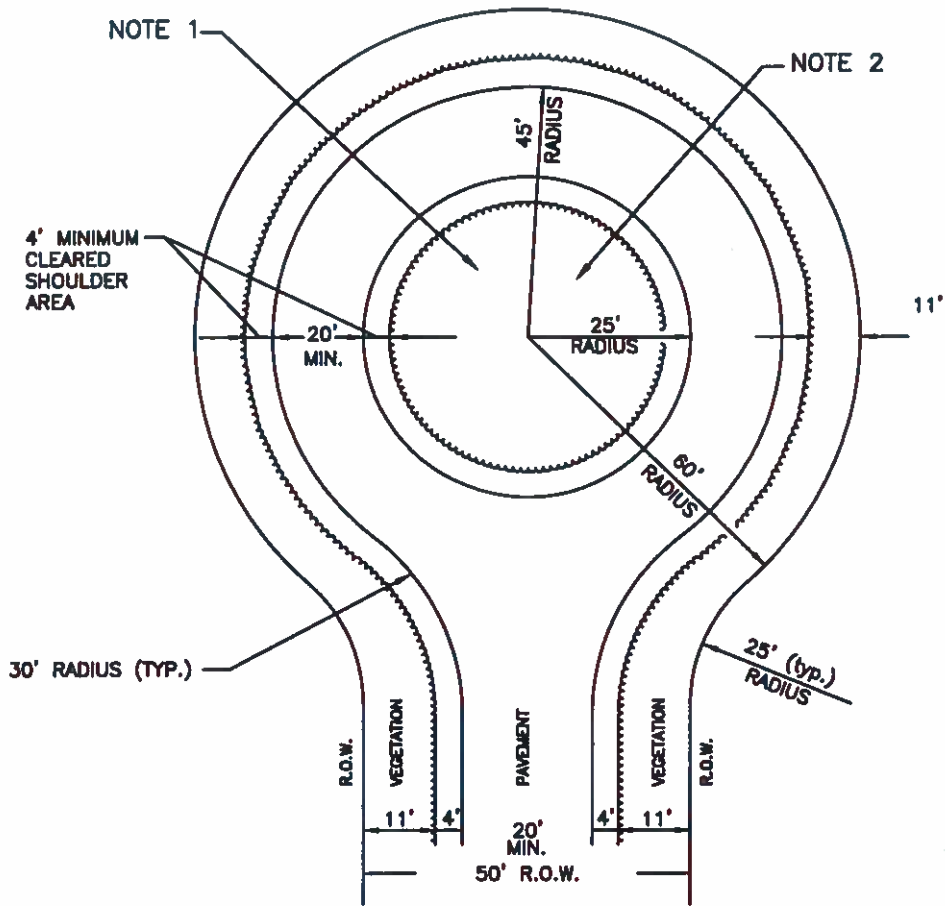
(NOT TO SCALE)



SIGNAGE FOR ACCESSIBLE PARKING
(NOT TO SCALE)

TYPICAL ACCESSIBLE PARKING SPACE

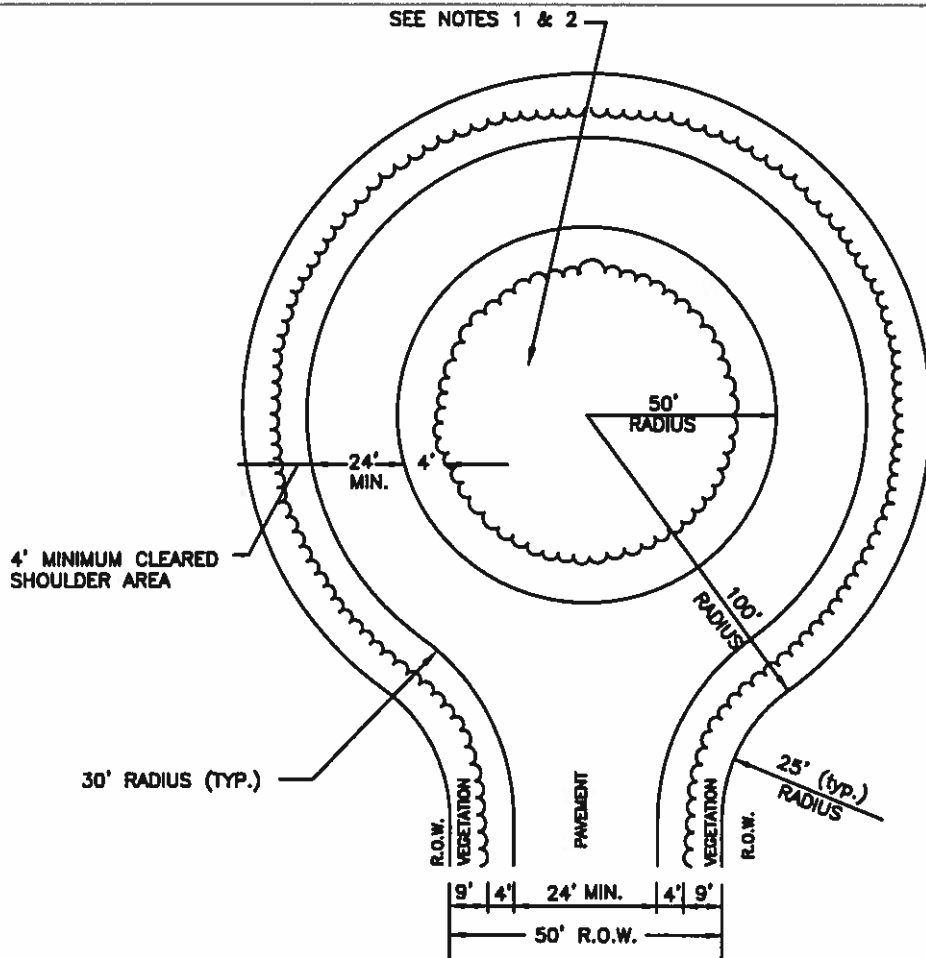
(NOT TO SCALE)



CUL-DE-SAC TYPE A
NOT TO SCALE

NOTES

1. THIS AREA NOT TO BE USED FOR RESERVED AREA.
2. ISLAND TO BE NATURAL OR REPLANTED VEGETATION.



CUL-DE-SAC TYPE B

NOT TO SCALE

NOTES

1. THIS AREA NOT TO BE USED FOR RESERVED AREA.
2. ISLAND TO BE NATURAL VEGETATION.



TABLE "A"

LOCATION OF ROAD BY ZONING DISTRICT	RIGHT-OF-WAY WIDTH	PAVEMENT WIDTH	CURB REQUIRED
R-10, R-15, R-20 & MULTI-FAMILY DISTRICTS			
HIGHWAYS	84-120 FEET	64 FEET	(b)
COLLECTOR STREET	70 FEET	40-50 FEET(a)	(b)
LOCAL STREET "A"	50-60 FEET	30-34 FEET	(b)
LOCAL STREET "B"	50 FEET	24-30 FEET	(b)
MARGINAL ROAD	50 FEET	30-34 FEET	(b)
COMMON DRIVEWAY	N/A	12-16 FEET	NO
OTHER RESIDENTIAL DISTRICTS			
HIGHWAYS	84-120 FEET	64 FEET	(b)
COLLECTOR STREET	70 FEET	40-50 FEET(a)	(b)
LOCAL STREET "A"	50-60 FEET	26-30 FEET	(b)
LOCAL STREET "B"	50 FEET	20-24 FEET	(b)
LANE	50 FEET	18 FEET	NO
MARGINAL ROAD	50 FEET	26-30 FEET	(b)
COMMON DRIVEWAY	N/A	12-16 FEET	NO
BUSINESS & INDUSTRIAL DISTRICTS			
HIGHWAYS	84-120 FEET	64 FEET	YES
COLLECTOR STREET	70 FEET	50 FEET	YES
LOCAL STREET "A"	60 FEET	34-40 FEET	(b)
LOCAL STREET "B"	-----	-----	---
MARGINAL ROAD	5 FEET	34-40 FEET	(b)
COMMON DRIVEWAY	N/A	24-34 FEET	(b)

(a) INCLUDES STABILIZED SHOULDERS

(b) AT THE DISCRETION OF THE PLANNING BOARD & SUPERINTENDENT OF HIGHWAYS

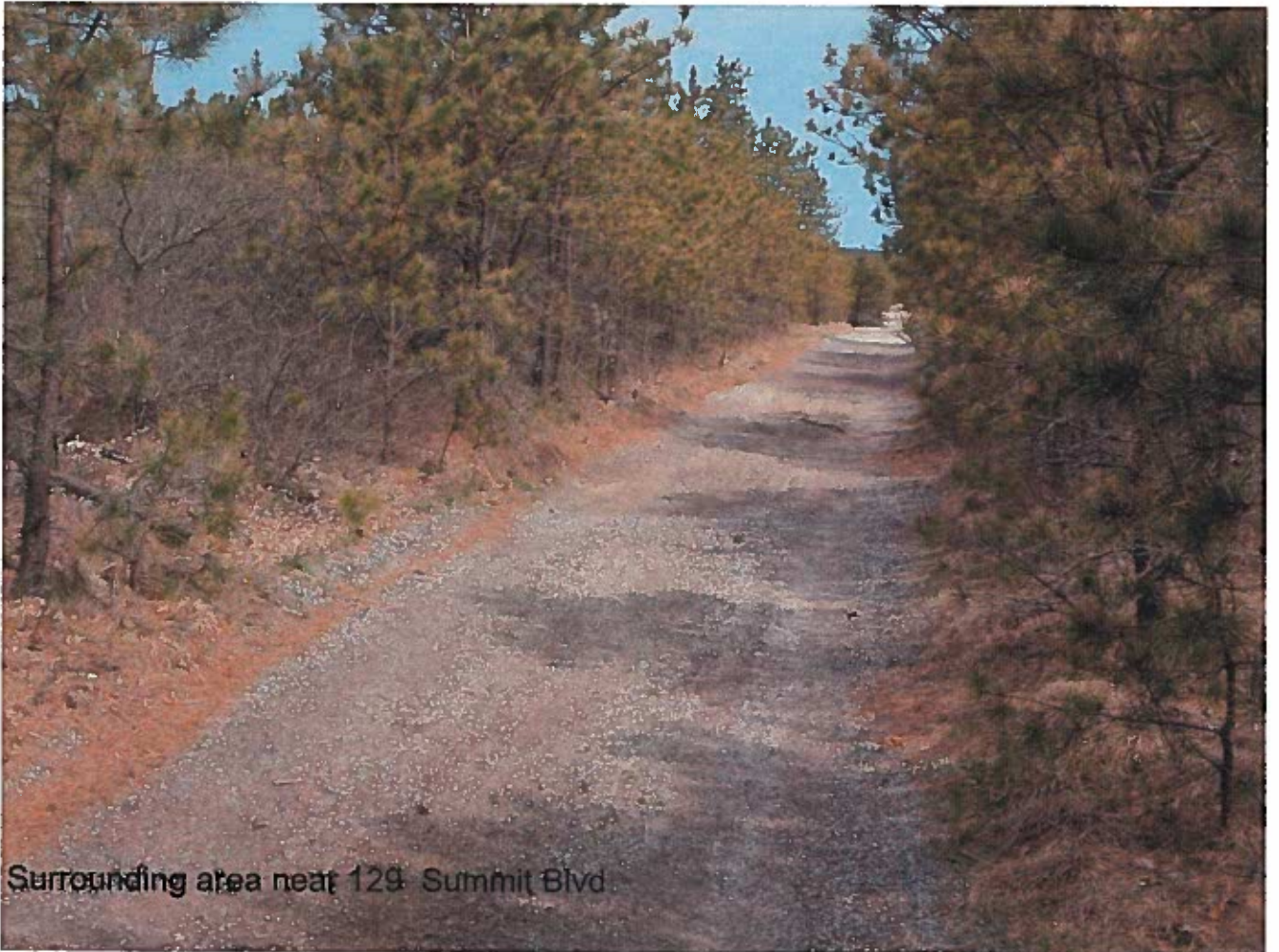
C B

C B

C B



129 Summit Blvd



Surrounding area near 129 Summit Blvd



Summit + BLVD. WE AT HAWYAN

Nov. 2004



GAZZA f

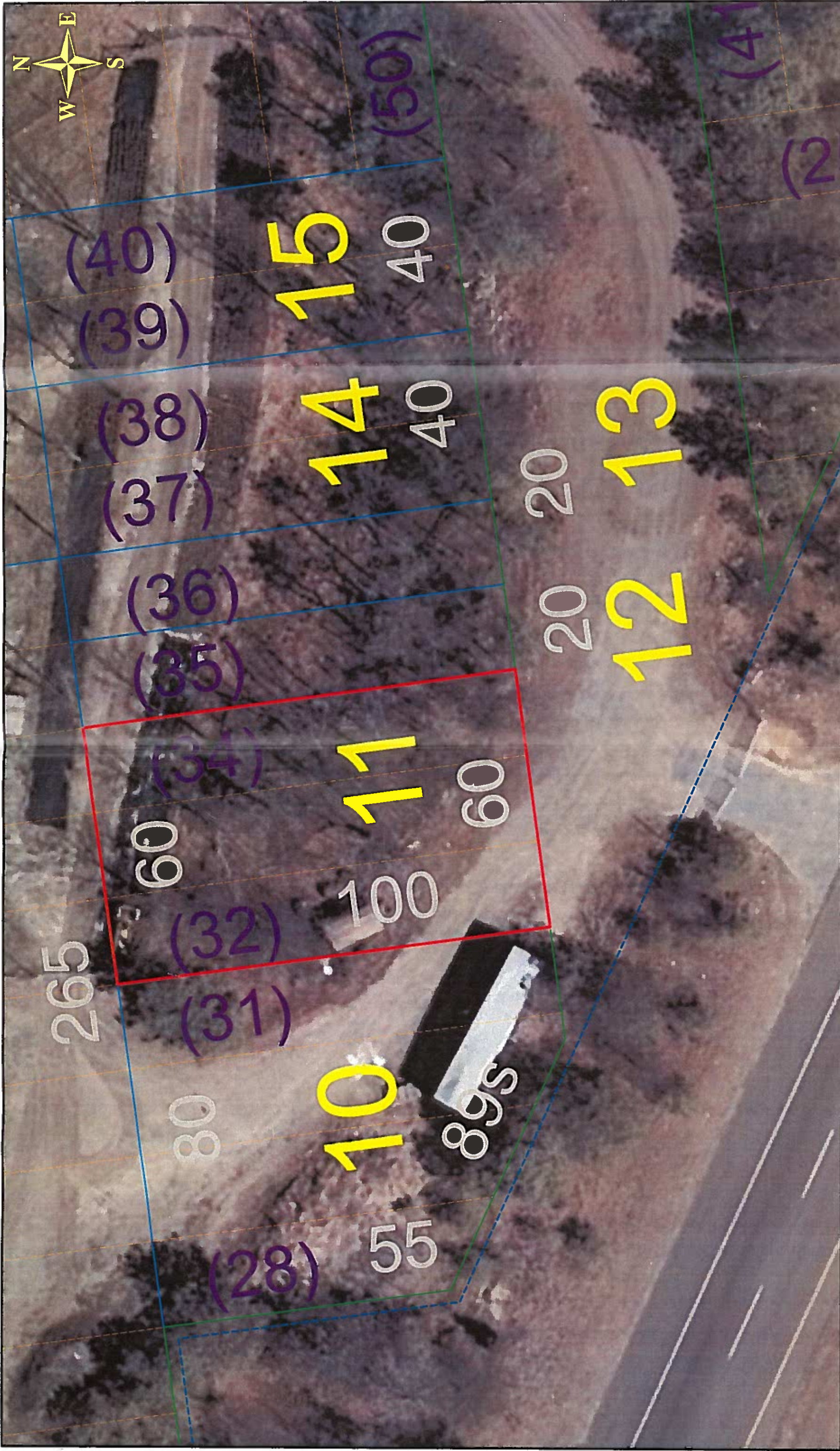
FOR DEPOSIT ONLY
JOSEPH FREDERICK GAZZA
ATTORNEY-AT-LAW
P. O. BOX 969 5 OGDEN LANE
QUOGUE, NEW YORK 11959
(631) 653-5766

2 Garza

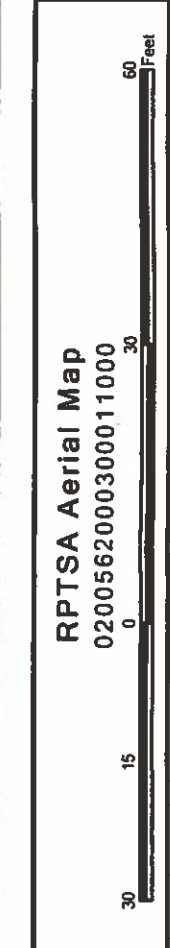
Certificate #	SCTM	Issued to	Acres	# of PBCs	School District	Issue Date (date on cert)	Liber	Page	
200-024	200-562-3-11	Lois Heisey	0.12	1	Eastport	2/21/1997	11,817	40	
200-148	200-529-3-35	The Woodstock Company	0.09	1	Longwood	10/7/1988	11,921	407	
200-154	200-270-2-12	Erik Skalenud	0.5	1	Riverhead	11/31/1988	11,925	956	
200-507	200-269-1-3	William Kroll	1.155	1	Riverhead	6/22/2005	12,397	835	
600-62	600-141.1-3-5	Henry Dittmer & James McLaughlin	0.042	1	Riverhead	2/9/2007	12,490	964	
900-145	900-331-3-28	Joseph Zachary Garza	1.7	1	Westhampton Beach	4/26/2004	12,315	223	before road improvement

#3 GRZZA

Harvey



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RPTSA Aerial Map
0200562000300011000

County of Suffolk
Real Property Tax Service Agency
300 Center Drive
County Center
Riverton, New York 11901



Steve Levy
Suffolk County Executive
Penny Wells Lovelle, MAI
Director

Ortho: 2007

777 6A222A
WILSON K COMPANY



Ortho: 2007

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- Parcel Lines
- Road Lines
- Condo Unit(s)
- Lot Line
- Block Line

RPTSA Aerial Map
0200529000300035000

30 15 0 30 60 Feet

County of Suffolk
Real Property Tax Service Agency
300 Center Drive
County Center
Riverhead, New York 11901

Steve Levy
Suffolk County Executive
Penny Wells Lavallee, MAI
Director

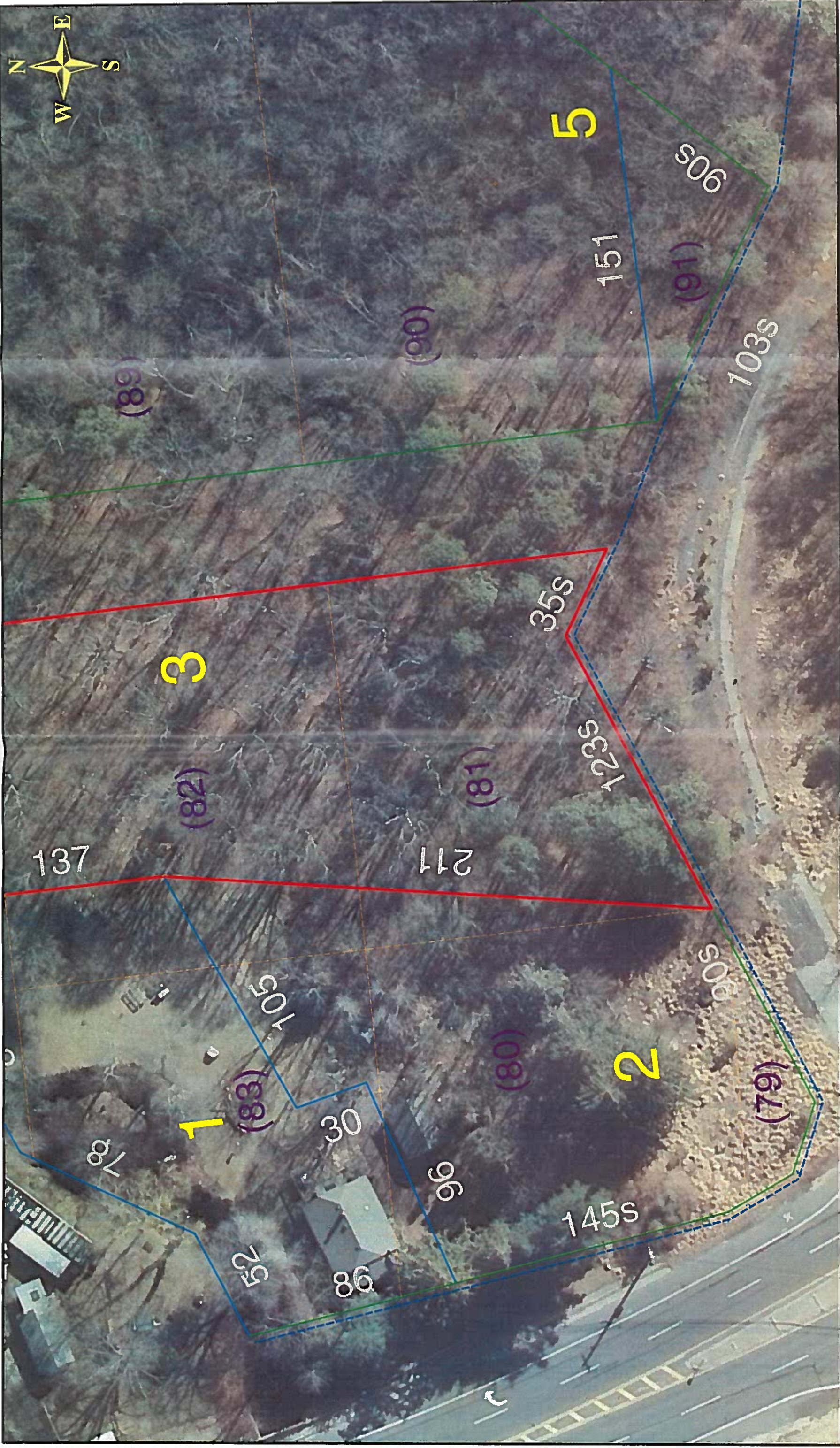




Pe



ALB 66-276 K1011



County of Suffolk
Real Property Tax Service Agency
 300 Center Drive
 County Center
 Riverhead, New York 11901

Steve Levy
 Suffolk County Executive
 Penny Wells Lyrvalle, MA
 Director

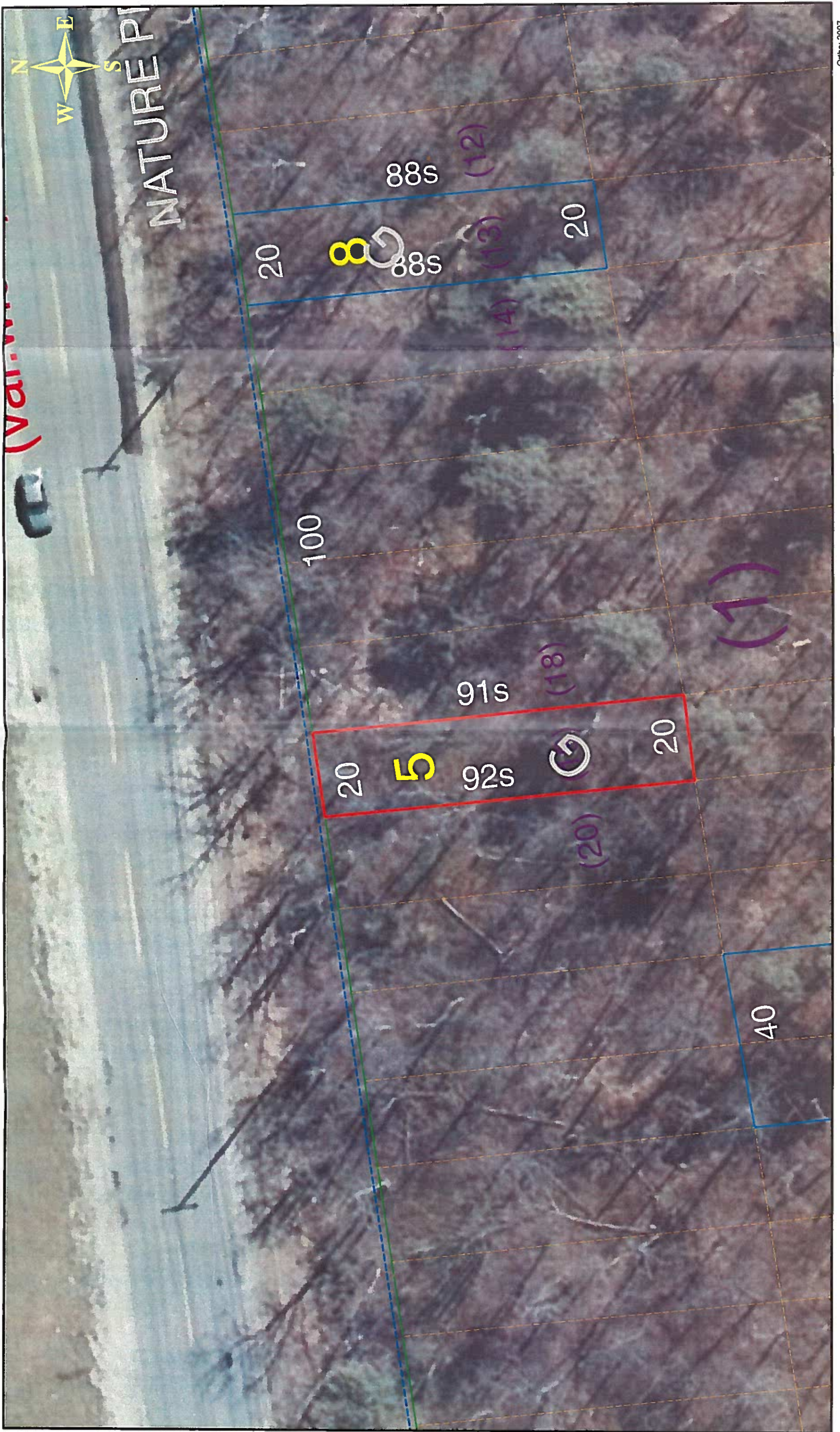
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Ortho: 2007

- Parcel Lines
- Road Lines
- Condo Unit(s)
- Lot Line
- Block Line

20 / 2025 / ...



NATURE PI



20

8
88s

88s

(12)

20

(13)

(14)

100

20

5

92s

91s

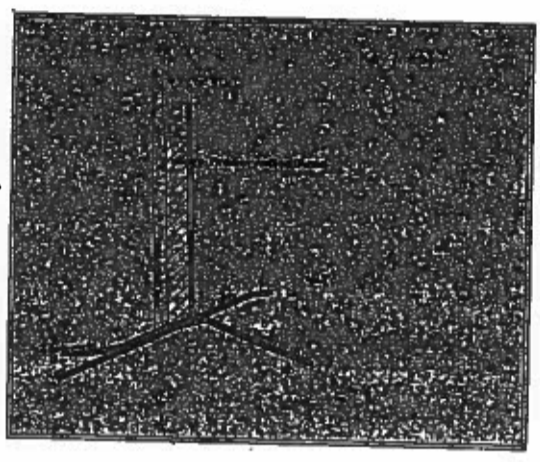
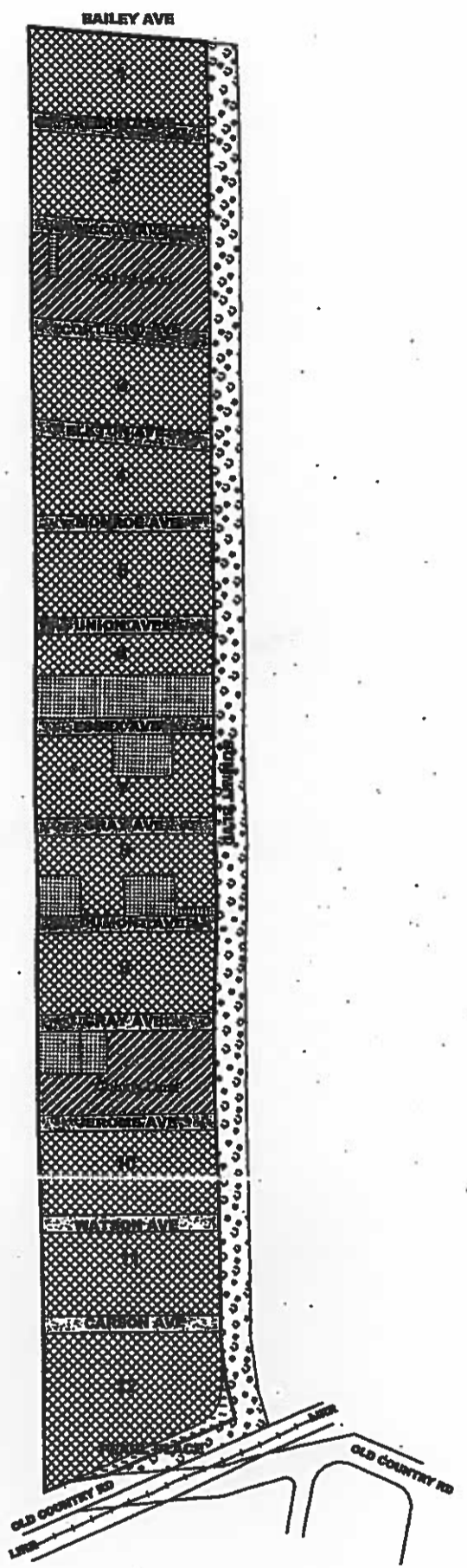
(18)

(20)








20

(11)

40



Legend

-  Development Section Boundary
-  Development Parcels
-  TDR Sending Parcels*
-  Roads to be opened
-  Roads to remain unopened
-  County land
-  Offer of dedication**

*Not buildable unless merged in title with adjoining Development Parcel
**17' Wide Offer of dedication to the Town of Southampton for future road widening

