



Town of Brookhaven Long Island

Edward P. Romaine, Supervisor

DATE: October 18, 2022

TO: Peter E. Fountaine, Town of Brookhaven, Division of Environmental Protection
Daniel P. Losquadro, Town of Brookhaven, Highway Department
Joseph T. Brown, P.E., Suffolk County, Department of Public Works
Gregson H. Pigott, Suffolk County, Department of Health Services
Judy Jakobsen, Central Pine Barrens Joint Planning & Policy Commission

FROM: Amy Moody
Planning Division

RE: Town Board Application: **7-Eleven @ Shirley**, Log # **2021-018-CZ**
Proposed change of zone from J2 and A1 to J5 and special permits and special permit waivers for proposed demolition of existing 7-Eleven convenience store and proposed new motor vehicle fueling station and accessory convenience store with associated site improvements.
1481 William Floyd Parkway, S/W/C William Floyd Parkway (CR 46) and Moriches Middle Island Road, and E/S Revilo Avenue, Shirley
SCTM: 0200 64200 0300 041003, 41004 & 41005, 1.38 acres

Please be advised that the Town Board of the Town of Brookhaven has received an amended submission for the above-referenced action. A copy of the revisions supplied by the applicant are enclosed for your consideration.

Please reply within 30 days of the date of this mailing. Also, kindly forward any information or concerns you may have regarding this proposal, particularly with respect to your agency's areas of expertise and jurisdiction, which would enhance the utilization of this site or provide additional protection to the community.

All correspondence should be forwarded to:

**Town of Brookhaven
OFFICE OF THE TOWN CLERK
Donna Lent
One Independence Hill
Farmingville, NY 11738**

Thank you for your continued cooperation. If you have any questions or need any further information, please contact this Division.

AM/ar

Cc: Donna Lent, Brookhaven Town Clerk
Leigh Rate, Town of Brookhaven Law Department

ENVIRONMENTAL PLANNING ASSESSMENT

Proposed Redevelopment of 7-11 Convenience Store, with Fuel Sales

Change of Zone and Special Permit Application

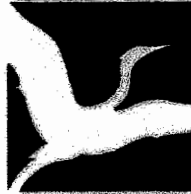
Hamlet of Shirley, Town of Brookhaven,
Suffolk County, New York

NPV No. 22147

Prepared for Submission to:

Town of Brookhaven Town Board
c/o Department of Planning and Environment
1 Independence Hill
Farmingville, NY 11738

Prepared by:



NELSON POPE VOORHIS

environmental • land use • planning

70 Maxess Road
Melville, NY 11747

Contact: Charles J. Voorhis, CEP, AICP
office: 631.427.5665 | cvoorhis@nelsonpopevoorhis.com

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Town of Brookhaven
Town Clerk's Office

October 3, 2022

ENVIRONMENTAL PLANNING ASSESSMENT

Proposed Redevelopment of 7-11 Convenience Store, with Fuel Sales

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October 3, 2022

ENVIRONMENTAL PLANNING ASSESSMENT

PROPOSED REDEVELOPMENT OF 7-11 CONVENIENCE STORE, WITH FUEL SALES

Change of Zone and Special Permit Application

SCTM: 0200-642.00-03.00-041.003, 041.004, & 041.005; 1481 William Floyd Parkway
Hamlet of Shirley, Town of Brookhaven
Suffolk County, New York

Applicant: 7-11, Inc.
c/o Gemstar Construction Corp.
163 Half Hollow Road, Suite 2
Deer Park, New York 11729
Contact: Keri Rosalia, Project Manager
(631) 667-0475
krosalia@gemstarcorp.com

For Submission to: Town of Brookhaven, Town Board (as Lead Agency)
c/o Department of Planning and Environment
1 Independence Hill
Farmingville, NY 11783
Contact: James M. Tullo, Commissioner
(631) 451-6400
Jtullo@brookhavenny.gov

Prepared by: Nelson, Pope, & Voorhis, LLC (*Environmental Planning & Analysis*)
70 Maxess Road
Melville, New York 11747
Contact: Charles J. Voorhis, CEP, AICP
(631) 427-5665
cvoorhis@nelsonpopevoorhis.com

Brown Altman & DiLeo, LLP (*Attorney*)
538 Broadhollow Road, Suite 301
Melville, New York 11747
Contact: Keith Brown, Esq., Partner
(516) 222-0222
brown@brownaltman.com

Stonefield Engineering & Design (*Engineering*)
584 Broadway, Suite 310
New York, New York 10012
Contact: Zachary E. Chaplin, PE
(718) 606-8305
zchaplin@stonefieldeng.com

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APPENDICES

- A** **Site and Area Photographs**, NP&V, LLC, *Taken August 1, 2022*
- B** **Traffic Impact Study**, Stonefield Engineering & Design, *Revised September 13, 2022*

In pouches at the back of this document:

- Boundary & Partial Topographic Survey**, Stonefield Engineering & Design, *revised 04/22/2022*
- Site Plan**, Stonefield Engineering & Design, *revised 09/13/2022*
- Landscaping Plan**, Stonefield Engineering & Design, *revised 09/13/2022*
- Landscaping Details**, Stonefield Engineering & Design, *revised 09/13/2022*
- Vehicle Maneuvering Exhibit**, Stonefield Engineering & Design, *revised 04/14/2022*

SECTION 1.0

DESCRIPTION OF THE PROPOSED PROJECT

1.0 DESCRIPTION OF THE PROPOSED PROJECT

1.1 Introduction

This environmental planning assessment document is for a proposed 7-11 convenience store with gasoline filling station on the site of an active 7-11 convenience store located at 1481 William Floyd Parkway in the hamlet of Shirley. The site of this proposal is 1.38 acres (or 59,951 square feet [SF]) in size and is composed of three contiguous tax lots, designated as District 0200; Section 642; Block 3; Lots 41.3, 41.4 and 41.5 (see **Figure 1-1** and **Boundary and Partial Topographic Survey** *[in a pouch at the back of this document]*). The site is presently zoned in the Town's J-Business-2 (1.14± acres) and A-Residential-1 (0.24± acres) zoning districts and is occupied by a 2,410 SF 7-11 convenience store and a 118 SF masonry shed.

Note that tax lot 41.1, which is contiguous to the south is, like tax lots 41.3, 41.4, and 41.5, owned by a single entity (Franklin Johnson, Inc.). However, the Applicant (7-11, Inc., c/o Gemstar Construction Corp.) has chosen to not include tax lot 41.1 in the project; it will be retained in its naturally vegetated condition, as a visual and noise buffer between the site and the neighbors to the south.


The Applicant seeks Brookhaven Town Board approval to change the zoning designation of the subject site to J-Business-5 (High Intensity Business) and to construct a one-story, 2,995 SF convenience store building having 1,850 SF of retail and display area. In addition, the project will include a motor vehicle fueling station consisting of eight (8) pumps under a free-standing canopy. Thus, up to sixteen (16) vehicles may be fueled at one time at this facility (see **Site Plan** *[in a pouch at the back of this document]*).

Under the Town Zoning Code, within the J-Business-5 district and as provided for in Section 85-460 G, a motor vehicle fueling station facility requires a Special Permit from the Town Board, and the accessory convenience store requires a Special Permit, also from the Town Board, per Section 85-463 A.

The project will also require variances and/or waivers from the Town Board, Board of Zoning Appeals and Planning Board for, among other things, the following relief:

- minimum lot area (*per Section 85-466 A. (3), 1.49 acres/65,000 SF min. required; 1.38 acres/59,951 SF proposed*)
- minimum rear yard setback (*per Section 85-466 E (1), 40 feet min. required; 25 feet proposed*)
- area of building wall sign (*per Section 57A-6 E, 40 SF max. allowed; 52.5 SF proposed*)
- area of ground sign (*per Section 57A-6 C, 24 SF max. allowed; 32 SF proposed*)
- fencing along property line adjacent to residentially-zoned land (*per Section 85-467 B. 8, min. 6 foot opaques fence required, not proposed along northern and western property lines*)



 NPV	FIGURE 1-1 LOCATION MAP	7-11 Shirley
	Source: ESRI WMS Scale: 1 inch = 150 feet	Environmental Planning Assessment

- depth of landscaping in rear yard setback (*per Section 85-467 B. 7, 40 feet min. required; 25 feet proposed*)
- spacing of street trees along street frontage (*per Section 85-843 A. 3, 30 feet required, not proposed*)
- minimum width of landscaping along street frontage (*per Section 85-843 A. 4, 15 feet min. required; 8.3 feet provided*).

Additionally, the project will require two (2) Town Board Special Permit waivers to:

- exceed the maximum allowed sales area in the accessory retail space (*per Section 85-467 B. 3, 750 SF max. allowed; 1,850 SF proposed*)
- exceed the maximum allowed total building area (*per Section 85-467 B. 3, 1,500 SF max. allowed; 2,995 SF proposed*)

As required under Section 85-467 B. 3 (b), the waiver to exceed the maximum allowed building area requires the redemption of Pine Barrens Credits (PBCs), calculated as follows:

- (b) Pine Barrens Credit redemption shall be calculated as equal to the percentage of relief requested, rounded up to the nearest tenth, such that each additional 150 square feet of total building area, above 1,500 square feet as permitted, shall be equal to 0.1 Pine Barrens Credit.

This indicates that one (1) PBC needs to be redeemed for the requested waiver, assuming that the requested percentage of relief is 100% (1,495 SF relief divided by 1,500 SF allowed = 0.997, then rounded up to the nearest tenth to 1.00). Alternatively, since the requested relief is 1,495 SF, and each 150 SF of this quantity requires purchase of 0.1 PBCs, $1,495 \text{ SF} \div 150 \text{ SF} = 9.97$, times 0.1 PBC = 1.00 PBC.

With the requested zone change, Special Permits, variances and waivers, the proposed project will be consistent with the Town of Brookhaven Zoning Code requirements of the J-Business-5 district.

The project will conform to all applicable flow and design requirements of the Suffolk County Department of Health Services (SCDHS). Sanitary wastewater from the project will be retained on-site, to be treated and recharged via the proposed septic system, which conforms to Suffolk County Sanitary Code (SCSC) Article 6 requirements. The project will also be subject to the review and approval of the Suffolk County Department of Public Works (SCDPW) in regard to roadway design and construction. The Town will forward the application to the Suffolk County Planning Commission (SCPC) for its review of the required zone change.

This document describes the project and analyzes its conformance to the patterns of land use and zoning in the vicinity, and its conformance to the standards under which the Town Board will review the required Special Permit, variance and waiver requests. This report will also

review the project's conformance to the Town Comprehensive Land Use Plan, the Carman's River Conservation and Management Plan, and the Town of Brookhaven Zoning Code standards for the Central Pine Barrens District in Chapter 85.

This document provides information to be used in the environmental review process for the proposed change of zone, special permit, variance, and waivers. This document is intended to provide the Town Board with additional information to render an informed decision on the application.

1.2 Project Background

The current use of the site is a commercial facility; specifically, a 7-11 convenience store. A series of photographs of the site and the immediate vicinity were taken in early August 2022, and are presented in **Appendix A**.

The application for the proposed project requires a Change of Zone, as the Town Zoning Code requires the J-Business-5 district for the proposed use (with a Special Use Permit issued by the Town Board). The application for the combined Change of Zone and Special Permit with Waivers was submitted to the Town in September 2021.

The proposed 2,995 SF, 7-11 convenience store/gas station will first require that the existing 2,410 SF 7-11 convenience store at this location be closed and demolished, along with its adjoining masonry shed, paved parking area surfaces and some of the existing landscaping.

As the change of zone will be administered under the jurisdiction of the Brookhaven Town Board, this Board will conduct a Coordinated Review under the New York State Environmental Quality Review Act (SEQRA) at the end of which it is expected to establish itself as the lead agency for the review mandated by SEQRA.

This document includes the following:

- a description of the design and layout of the proposed project,
- a detailed Traffic Impact Study (TIS) of the project,
- analyses that justify the rezoning of the site and the requested special use permits, variances and waivers, and
- conformance to the applicable recommendations of the Town Comprehensive Land Use Plan, the Town Zoning Code standards for the Central Pine Barrens, and the Carman's River Conservation & Management Plan.

This document provides important information and analysis that will assist the Brookhaven Town Board in its review and decision on the application that is before it.

1.3 Project Location and Existing Site Conditions

1.3.1 Location of the Proposed Project

The property is specifically identified as Suffolk County Tax Map District 0200, Section 642, Block 3, Lots 41.3, 41.4, and 41.5. The street address of the site is 1481 William Floyd Parkway, Shirley.

The subject site is 59,951 SF (1.38 acres) in size and is located at the southwestern corner of the intersection of Moriches-Middle Island Road and William Floyd Parkway. Moriches-Middle Island Road lies along the property's northern boundary, William Floyd Parkway lies along the site's eastern border, and Revilo Avenue is along the site's western boundary. To the north of the site are commercial properties, beyond which are residential lots. Across William Floyd Parkway to the east is a gas station, followed by residential properties along both sides of Moriches-Middle Island Road. As noted above, land abutting to the south is vacant and wooded, followed by residential lots. Area to the west is residential land (see **Figure 1-2**).

The site is within the following planning and/or service zones and districts:


- J-Business-2 Zoning District (1.14 acres)
- A-Residence-1 Zoning District (0.24 acres)
- Compatible Growth Area (CGA) of the Central Pine Barrens (CPB) Zone
- Carman's River Conservation and Management Plan Study Area
- Groundwater Management Zone VI (600 gallons per day per acre [gpd/ac])
- FEMA Flood Hazard Zone X, 0.2% annual chance flood hazard
- Longwood Central School District
- Ridge Fire District & Department
- Suffolk County Police Department, 7th Precinct
- Suffolk County Water Authority (SCWA), Distribution Area 12
- Public Service Enterprise Group, Long Island (PSEGLI; electricity)
- National Grid (natural gas)

The project site is not in the Central Suffolk Special Groundwater Protection Area.

1.3.2 Existing Site Use and Conditions

Figure 1-2 is an aerial photograph of the subject site, which depicts its existing conditions. As can be seen, the site is presently developed with a 7-11 convenience store, and includes three vehicle driveways (onto Moriches-Middle Island Road, William Floyd Parkway, and Revilo Avenue), and 47 striped parking spaces. The driveway onto Moriches-Middle Island Road is configured for all movements, the Revilo Avenue access is also configured for all movements, and the driveway on William Floyd Parkway allows only right turns entering and right turns exiting the site. Sections of chain-link fencing are located parallel to and 4 feet inside of the property's south and western property lines (see **Boundary and Partial Topographic Survey**).



 NPV	FIGURE 1-2 EXISTING CONDITIONS MAP	7-11 Shirley
	Source: NYS Orthophotography, 2020 Scale: 1 inch = 100 feet	Environmental Planning Assessment

The area between this fencing and the southern property line is occupied by unmaintained natural vegetation which totals about 1,000 SF. The remainder of the site is landscaped open lawn. A series of photographs of the site were taken in early August 2022 to document the existing site and area conditions (see **Appendix A**). These are described in **Section 2.1.1**.

1.4 Project Design and Layout

1.4.1 Overall Site Layout

Refer to **Table 1-1** for current and anticipated future site conditions. The proposed project involves construction of a single, one-story commercial structure, having a footprint of 2,995 SF. The building will be accessed by vehicles from the three roadways that border the property: Moriches-Middle Island Road, William Floyd Parkway, and Revilo Avenue. The TIS prepared for the proposed project concludes as follows with respect to the potential for adverse impacts on the local roadways, as well as the adequacy of the number of proposed parking spaces:

This [TIS] was prepared to examine the potential traffic impact of the proposed 7-Eleven convenience store with fuel sales redevelopment. The analysis findings, which have been based on industry-standard guidelines, indicate that the proposed development would not have a significant impact on the traffic operations of the adjacent roadway network. The site driveways and on-site layout have been designed to provide for effective access to and from the subject property. The proposed parking supply meets the Town's requirements and would be sufficient to support this project.

For the proposed project, the three bordering roadways will continue to provide vehicle access, via upgraded driveways in the same locations as presently exist. As determined in the TIS (see **Appendix B**), the Moriches-Middle Island Road access (currently configured for all movements) will be reconfigured to allow only right turns entering and right and left turns exiting the site; no left turns entering the site from Moriches-Middle Island Road will be allowed. The other two driveways will also be upgraded and will continue to provide all entering and exiting movements (Revilo Avenue), and right turns entering/right turns exiting (William Floyd Parkway) the site. Each access driveway will be STOP-controlled for exiting drivers. No internal vehicle access to adjoining sites is proposed. The project will install sections of sidewalk and striped crosswalks along William Floyd Parkway, Revilo Avenue and along Moriches-Middle Island Road, to fill gaps in the local network of this amenity.

The convenience store building will be placed in the site's southwestern corner and will face toward the northeast, toward the intersection of Moriches-Middle Island Road and William Floyd Parkway. An enclosure for two 8 CY dumpsters for rubbish and recycling will be placed on the building's western side; this enclosure will be accessed via a 24-foot-wide striped access for the carter contracted to remove this waste material. It is expected that this access area will also be used for delivery of retail goods in smaller trucks for the store. The **Vehicle Maneuvering Exhibit** (*in a pouch at the back*) shows the anticipated routes for larger truck deliveries and fuel deliveries.

TABLE 1-1
SITE AND PROJECT CHARACTERISTICS
Existing Conditions and Proposed Project

Parameter	Existing Conditions	Proposed Project
Use & Yield	Commercial; 7-11 Convenience Store	Commercial; 7-11 Convenience Store & Motor Vehicle Fueling
Zoning	J-Business-2 & A-Residence-1	J-Business-5 (High Intensity Business)
Wastewater Treatment System	On-Site Septic	
Coverages (SF):	---	---
Buildings	2,528	2,995
Paved	30,183	32,741
Landscaped	26,240 ⁽¹⁾	23,215 ⁽²⁾
Forest	1,000	1,000
TOTAL	59,951	
Water Resources:	---	---
Domestic Use/Wastewater (gpd) ⁽³⁾	150	150
Irrigation (gpd) ⁽⁴⁾	1,745	882
Total Water Use (gpd) ⁽⁵⁾	1,895	1,032
Vehicle Trips Generated (vph):	---	---
Weekday AM Peak Hour	232	327 (+95) ⁽⁶⁾
Weekday PM Peak Hour	171	286 (+115) ⁽⁷⁾
Miscellaneous:	---	---
Employees (capita)	2 to 3 per shift	
Hours of Operation	24 hours/day, 7 days/week	
Solid Waste Generation (lbs/day) ⁽⁸⁾	75	93
Parking Required (spaces) ⁽⁹⁾	24	32
Parking Provided (spaces)	47	32 ⁽¹⁰⁾

(1) Assumed to be irrigated at a rate of 16 inches/season.

(2) Of which 13,260 SF will be irrigated

(3) Based on SCDHS rates for SCSC Article 6.

(4) Anticipated usage in 150-day irrigation season (May-September); assume no irrigation use for remainder of year.

(5) Total Water Use at peak value during irrigation season; reduced to 150 gpd for remainder of year.

(6) Increase of 95 trips compared to Existing, of which 58 trips are "pass-by" and 37 are "new" trips (see **Appendix B**).

(7) Increase of 115 trips compared to Existing, of which 62 trips are "pass-by" and 53 are "new" trips (see **Appendix B**).

(8) Assuming 3.12 lbs/day/100 SF for commercial space.

(9) Per Town Code Section 85-852.

(10) Includes three landbanked spaces.

A total of 29 head-in parking spaces will be distributed around the site; an additional three spaces will be located on the property, to be installed if these become necessary to suit site patronage. Finally, the areas lying between the developed area and site's perimeter will be landscaped, using species listed in Figure 5-2 of the Town Zoning Code.

The eight proposed fuel pump islands are located in the central portion of the site and face, like the proposed building, in a northeasterly direction, towards the intersection of Moriches-

Middle Island Road and William Floyd Parkway. The pump islands will be beneath a single 40 foot x 120 foot metal canopy (to protect customers from the elements while refueling). Fuel will be stored in two 10-foot diameter underground fiberglass storage tanks set in a bed of pea gravel and located in the eastern portion of the property. Each tank will have a volume of 20,000 gallons: one tank will store regular unleaded gas, and the other tank will be split to contain 8,000 gallons of premium gas and 12,000 gallons of diesel fuel. The sump for each tank will be equipped with a mechanical line leak detector.

Exterior lighting will be provided on the building's walls, along the outer perimeter of the internal aisle, and at the site's three access driveways. All lighting will comply with Town Code and "dark sky" requirements and will be subject to site plan review. Sections of new sidewalk will be installed along both William Floyd Parkway and Moriches-Middle Island Road to fill gaps in the existing local sidewalk network. The chain-link fencing along the site's southern and western borders will be replaced with 6-foot-high vinyl fencing, and approximately 1,000 SF of unmaintained natural vegetation between the southern fence and the property line will remain.

1.4.2 Demolition, Clearing, Grading, and Drainage

Demolition

Demolition is anticipated to include the existing building and foundation, underground utility connections, the paved parking areas, curbing and sidewalks, the leaching pools for the existing drainage and septic systems, and the septic tank.

Clearing

Following the demolition phase, most of the balance of the site, comprised of lawn surfaces but not including the 1,000 SF of natural vegetation between the southern fence and the southern property line, will be cleared.

Grading

Soil disturbance will typically occur as part of the grading process, which is undertaken to establish suitable grades for the proposed building and paved roadway and parking spaces, as well as for utility trenching and the new underground fuel tanks. Additionally, the grading program must consider requirements for slopes to ensure proper drainage system performance, conformance with requirements of the Americans with Disabilities Act, and the convenience of the site's patrons. As the subject site is already developed, it is generally flat so that, other than for the underground fuel storage tanks and leaching pools (for the new septic and drainage systems) and foundations, grading requirements are reduced and grading operations need not extend to a substantial depth (e.g., in excess of one foot or so). Soil excavated for these systems will be retained on-site as far as possible and reused as fill. As a result, a significant volume of excess soil is not expected to be generated.

Drainage

All stormwater runoff generated on the property will be handled in an on-site drainage system which is designed by a licensed professional engineer and will be subject to review and approval by the Town Engineering Department. The drainage system will include leaching pools and catch basins, with a storage capacity in excess of the design standard specified by the Town. The Town requirement that at least two (2) feet of vertical separation be maintained between the bottom of the drainage system leaching pools and the water table will be satisfied.

1.4.3 Parking, Vehicle Access and Circulation

Parking

As noted on the **Site Plan**, the Town of Brookhaven Off-Street Parking Ordinance requires one (1) parking space per 100 square feet for convenience store uses. For the proposed 2,995 square-foot 7-11 store, this equates to 30 required spaces. Code Section 85-852 would require two (2) spaces for the fueling component, for a total minimum required parking capacity of 32 spaces. The project will provide 32 total parking spaces, including three (3) landbanked spaces and two (2) ADA accessible parking spaces. Therefore, the proposed 32 parking spaces are expected to be sufficient to support the proposed 7-11 convenience store. One loading space is required under Section 85-850 of the Town Code, and will be provided, adjacent to and on the northwestern side of the proposed structure.

Additionally, it is widely recognized throughout the traffic engineering industry that vehicle fueling positions function as parking spaces for patrons of both gasoline and convenience items; patrons rarely move their vehicle from a fueling position to a striped parking space between gasoline and convenience store transactions. Therefore, the proposed 16 vehicle fueling positions would be available to supplement the 32 proposed parking spaces for a total of 48 vehicle positions, which exceeds the Town's parking requirement. Therefore, the proposed parking supply is expected to adequately support the development. The spaces would be at least 9 feet wide by 19 feet deep in accordance with Town requirements and industry standards.

As part of the Town's site plan review process, the Town Highway Department will review the proposed project layout to ensure that the site conforms to all applicable design standards and requirements and will operate safely in accordance with its intent. In addition, the Town Fire Marshal and representatives of the Ridge Fire Department will evaluate the project design for proper access for emergency and fire personnel and equipment access.

Vehicle Access and Circulation

The proposed project will continue to use driveways in the same three locations onto Revilo Avenue, Moriches-Middle Island Road, and William Floyd Parkway that presently serve the site, though each access driveway will be improved with new curbing and striping; the access onto Revilo Avenue will be widened slightly, from 24 feet to 30 feet (see **Site Plan**). These improved driveways will conform to Town design requirements, and will be striped and configured for:

- Moriches-Middle Island Road - right turns entering, right turns exiting, and left turns exiting
- Revilo Avenue - all movements
- William Floyd Parkway - right turns entering and right turns exiting

As the subject site has no frontage on any other roadway, there will be no vehicular access onto any other street, nor will a separate emergency access be provided.

It is expected that deliveries to the convenience store will be made using trucks that could vary in size (in terms of length) ranging from vans to semi-tractor trailers. The project layout would accommodate this range in truck sizes, by locating a delivery area for non-articulated trucks adjacent to and to the northwest of the store. For articulated (i.e., longer, tractor-trailer sized trucks), use of the loading zone would not be feasible, so that these deliveries would be scheduled for off-peak hours, so that these vehicles would enter the site off William Floyd Parkway, park to the north of the fuel pump area, and depart via the Revilo Avenue access. The **Vehicle Maneuvering Exhibit** depicts this process. In conformance with Town Code requirements, the project's internal aisles will be 30 feet wide, to accommodate this size vehicle. Note that the access driveway for Revilo Avenue will be widened in part to facilitate the departure of large delivery trucks from the site.

A 7-11 convenience store is supplied by about 15 different suppliers, some of which deliver from only one producer (e.g., Coca-Cola, Entenmann's Bakery or Frito-Lay), and other suppliers provide a number of products from differing producers. Truck deliveries to the store will occur seven days a week, on schedules that vary for each provider; some providers deliver once per week, others twice weekly, others deliver daily. Deliveries may occur throughout the day, as the store is open 24 hours a day, seven days a week; however, all deliveries will be subject to Town regulations with respect to time. In order to minimize the potential for more than one delivery truck to be on the site at any one time, the store operator will generally schedule deliveries so that multiple delivery trucks would not be on the site simultaneously.

1.4.4 Sanitary Disposal and Water Supply Systems

Sanitary Wastewater Disposal

Sanitary wastewater flow and discharge requirements are determined by the SCDHS, under the jurisdiction of SCSC Article 6. The project site is located within Groundwater Management Zone VI as defined by the SCDHS. Based on the requirements of SCSC Article 6, for a property that

uses an on-site conventional sanitary system, no more than 600 gallons may be discharged per acre daily within this zone.

Assuming the wastewater flow rates for the proposed use stated in SCSC Article 6, the project would generate 150 gpd of wastewater. As a result, a conventional on-site septic system, consisting of a septic tank and sanitary leaching pools would be allowed, and is proposed. Approval from SCDHS will be required for this system, to be evaluated as part of the Site Plan review process.

Water Supply System

The quantity of wastewater generated by the project generally represents the amount of water consumed by the project. Thus, as 150 gallons of wastewater are expected to be generated daily, the project is expected to use the same 150 gpd of water. The Suffolk County Water Authority (SCWA) currently supplies and will supply potable water to the site.

Of the project's 23,215 SF of landscaping, 13,260 SF will also use water for irrigation. Assuming an irrigation rate of 16 inches over the five-month irrigation season (roughly May-September), this volume corresponds to an irrigation demand of 882 gpd during the irrigation season. Therefore, as shown in **Table 1-1**, the project would consume an estimated 1,032 gpd of water from May to September (when irrigation occurs), and 150 gpd from September-May when no irrigation occurs.

As the subject site is currently developed and operating as a 7-11 convenience store, it also consumes an estimated 150 gpd of water for domestic purposes, and its 26,240 SF of landscaping are estimated to use 1,745 gpd of water for irrigation. As a result, it is assumed that the site currently consumes 1,895 gpd of water during the irrigation season and, like the proposed project, 150 gpd outside of it.

Water for potable domestic use and fire protection purposes will be provided via the 12-inch diameter supply main beneath either Revilo Avenue or Moriches-Middle Island Road (depending on SCWA determination, during the Town's site plan review process). Any necessary associated meters, easements and installations will be provided to ensure adequate water supply.

1.4.5 Fuel Storage and Environmental and Public Safety Considerations

As noted on the **Site Plan**, the project includes two 20,000 gallon fuel storage tanks to be located beneath the parking area in the eastern portion of the site. These tanks will be of fiberglass construction and will conform to applicable NYS and SCDHS requirements for design, installation, maintenance and monitoring, and will be properly permitted. The sump for each tank will be equipped with a mechanical line leak detector. This feature would help minimize the potential for impacts to groundwater quality from leakage, so that timely notice would be

provided to the facility operator, to immediately implement the appropriate and required remediation procedures.

SCSC Article 12 regulates storage/handling of toxic and hazardous materials to “...maintain [Suffolk County’s] water resources as near to their natural condition of purity as reasonably possible for the safeguarding of the public health...” The project does not involve the use of toxic or hazardous materials (other than common household cleaners), and so will conform to this regulation.

1.4.6 Site Lighting, Landscaping and Utilities

Lighting

Generally, the lighting system will be designed to produce a safe and secure environment with illumination directed to those areas where it is appropriate and/or necessary, and shielded so as to minimize the potential for adverse fugitive lighting impacts to the nearby residences. The project’s lighting system is expected to include the following

- LED lights on the underside of the canopy over the fuel dispensing islands, 15-foot mounting height;
- LED area lights mounted on the store’s exterior walls, 10-foot mounting height; and
- pole-mounted LED lights, 18-foot mounting height, to be spaced along the perimeter of the paved parking area, and at each of the three site access driveways.

All of the wall-mounted and all of the pole-mounted lights will be provided with shrouds to minimize the potential for fugitive lighting. Additionally, all lighting fixtures will be directed downwards, to minimize the potential for fugitive lighting impacts upon adjacent properties. The minimum distance between the new structure and the nearest residence across Revilo Avenue will be about 140 feet.

The project’s lighting will be consistent with current Town standards and requirements; lighting for the project will conform to the applicable requirements of Town Zoning Code Sections 85-862 through 85-871, including the use of “dark-sky” compliant fixtures equipped with shielding to produce downcast lighting. It is expected that use of such fixtures will minimize the potential for adverse impacts to the visibility of the nighttime sky for site patrons, as well as impacts to the neighboring properties from increased skyglow and light pollution. Lighting will be provided consistent with adjacent and nearby properties and would be typical of a commercial property in an active suburban commercial corridor setting. Lights will be illuminated in conformance with Town Lighting Code requirements; as the proposed facility will be open for business on a 24/7 basis, it is expected that the site’s lighting will operate from dusk through dawn.

Landscaping

The **Landscaping Plan** (*in a pouch at the back of this document*) shows that the open areas between the proposed structure and/or the site's paved surfaces and all four of the site's borders will be landscaped, using a mix of deciduous trees, evergreen trees, shrubs (including evergreen shrubs) and grass. A total of 23,215 SF of the site (38.72%) will be landscaped, which exceeds the Town Zoning Code minimum required site coverage for landscaping (20%, or 11,990 SF). Additionally, the Code requires that at least half of the landscaping proposed be located in the front yard, or at least 11,634 SF. As the **Landscaping Plan** shows that 17,361 SF of the front yard will be landscaped, the proposed project will comply. As for trees, the Town Zoning Code requires that one tree be present for each 30 feet of roadway frontage. For the subject site, this would require three (3) trees along William Floyd Parkway, five (5) trees along Moriches-Middle Island Road, and 9 trees along Revilo Avenue. The **Landscaping Plan** shows that these numbers of trees will be provided. The requirement that the depth of landscaping along all street frontages be at least 15 feet will be satisfied. In addition, the depth of landscaping abutting any residential district must be at least 25 feet; this will also be met. Finally, the existing fencing along the site's southern and southwestern frontages will be replaced with solid 6-foot high vinyl fencing, totaling 312 feet.

Table 1-1 shows that a total of 23,215 SF of landscaping is proposed, which will include retaining four (4) trees along Revilo Avenue, new tree plantings along all four site borders, new trees to be planted within the parking area islands, abutting the building and the site's southerly landscaped area, to reduce the aesthetic impact of the developed nature of site.

Assuming a typical irrigation rate of 16 inches per irrigation season (assumed here to extend over a period of five months, from May through September), total irrigation demand is calculated at 882 gpd during the irrigation season (for the 13,260 SF of landscaping to be irrigated). According to the project's landscape architect, none of the proposed landscape plantings will require use of fertilizers (beyond an initial application).

Utilities

Electricity will be provided by PSE&I and natural gas will be provided by National Grid (Letters of Availability confirming that such services will be provided will be obtained by the Applicant and provided to the Town Board as part of the Town's Site Plan review process). Solid waste will be collected and deposited in two dumpsters in a gated enclosure on the west side of the store and removed by a licensed carter on a regular basis.

1.5 Construction Schedule and Operations

1.5.1 Construction Schedule

It is expected that the construction process (including demolition) will start shortly after issuance of the necessary permits from the Town, and last an estimated 4 months. Construction activities will conform to applicable Town Code regulations on hours and would not occur

between the hours of 8:00 PM and 7:00 AM on weekdays, and will conform to applicable Town regulations regarding construction noise generation.

1.5.2 Construction- Related Operations

It is expected that the majority of construction-related truck trips (particularly of larger trucks) will use the William Floyd Parkway driveway, as this major roadway is the likely route that drivers will use to approach the area. However, it is acknowledged that the accesses onto Moriches-Middle Island Road and, to a lesser extent Revilo Avenue, could be used by construction workers, depending upon the road chosen by the driver to reach the area of the site. It should be noted that the nature of the project is such that the phases of its construction process would occur in sequence, so that large amounts of construction equipment or construction workers would not be on-site at any one time, which would tend to minimize construction-related noise or traffic. Additionally, the scale of the project is relatively small, so that the construction process will not be lengthy.

It is expected that, because of the small size of the site, there would not be sufficient room to accommodate worker parking on-site during the initial phases of construction (e.g., demolition). As a result, during the demolition, clearing and grading phases, construction workers will park their private vehicles along the shoulders of the bordering roadways. Thereafter, an area within the site can be designated for construction worker parking. Generally, areas within the site that are not subject to active construction activities will be used for parking, material deliveries, and material storage/staging.

1.5.3 Excavation Process and Soil Removal

As nearly all of the subject site is already cleared and developed, it is expected that only a relatively small amount of soil will be disturbed to construct the new store building. Similarly, it is expected that any excess soil generated in this process will be re-used on the site to fill excavations created when removing the buried components of the sanitary and drainage systems, the old building foundation, and paved surfaces. Any excess soil remaining after this process will be removed from the site and taken by a licensed hauler to an approved disposal facility.

1.5.4 Brief Description of Building Construction Sequence and Process

After site preparations (i.e., demolition, clearing and grading) are completed, excavations for the building foundation, the fuel storage tanks, the canopy foundation, curbing, light pole foundations and the utility systems will commence. These activities will be followed by installing forms for and pouring of concrete. After the concrete has set, building construction can then begin, along with the various exterior installations such as sidewalks, driveway access aprons, the fuel pumps, and the parking area/internal roadway sub-base. Concurrent activities may include installation of the utility connections, while the completed shell receives its interior

electrical, plumbing and other mechanical systems (e.g., heating, ventilation and air conditioning equipment, etc.); installation of the site lighting system may be performed while the interior of the building is being completed. Utility system commissioning will complete the construction process.

1.5.5 Erosion Control During Construction

The drainage system will comply with requirements under NYSDEC State Pollutant Discharge Elimination System (SPDES) General Permit for Stormwater Discharges from Construction Activity (GP 0-20-001) and Chapter 47 of the Town Code. Under these requirements, a Storm Water Pollution Prevention Plan (SWPPP) must be prepared and submitted for Town review and approval as a condition to final site plan approval. The SWPPP evaluates the proposed drainage system to ensure that it meets the NYSDEC and Town requirements for treatment and retention of stormwater runoff. The SWPPP must demonstrate that the proposed stormwater management system is sized adequately to ensure that there is no net increase in peak stormwater discharges from a property once developed. Additionally, the SWPPP will include details of the erosion controls to be employed during construction; these measures are discussed below.

Drainage Plans and a Preliminary Network Diagram and Hydrograph Comparison will be prepared to establish that the SWPPP will ensure that post-development stormwater will be less than pre-development stormwater. The dimensions, materials, and installation for all erosion and sediment control practices are subject to Town review and approval under the site plan review.

Because of the measures taken to comply with the SWPPP as discussed above, significant amounts of sediment will not be transported off-site by runoff. Thus, no significant impact on adjacent sites is expected. During the construction process, inspections of the construction site will be regularly performed under the supervision of a qualified professional to ensure that erosion controls are properly maintained.

The construction manager, with the various specialized contractors, will be responsible for all construction activities, and installation and maintenance of the erosion and sediment controls. The construction manager will also be responsible for ensuring proper storage and stockpiling of construction materials and that building supplies will be stored in designated areas. Necessary steps will be taken to ensure that measures are implemented to prevent/reduce wind-blown dust. The construction manager will be responsible for securing an approved carter to empty the construction waste dumpsters and haul waste from the site to an approved location for disposal.

1.6 Permits and Approvals Required

As noted in **Section 1.1**, this document is intended to provide important information and analysis on the project to support the Town Board in reaching an informed decision on the application. **Table 1-2** lists the permits and approvals needed for the proposed project.

TABLE 1-2
PERMITS AND APPROVALS REQUIRED

Applicable Board/Agency	Permit/Approval Type
Town Board	Change of Zone
	Special Use Permit (motor vehicle fueling station)
	Special Use Permit (accessory convenience store)
	Special Permit Waivers ⁽¹⁾
Town Planning Board	Site Plan approval
	Variances from the Town Development Standards ⁽²⁾
Town Board of Zoning Appeals	Area and Sign Variances ⁽³⁾
Town Building Department	Demolition Permit
	Building Permit
Town Fire Marshal	Site Plan review
Town Highway Superintendent	Highway Work Permit
SCDPW	Roadwork Permit (239f review)
SCWA	Water Supply System Connection approval
SCDHS	Sanitary System review (SCSC Article 6)
	Water Supply Connection review
	SCSC Article 7 review
	SCSC Article 12 review
SCPC	Referral 239-m review
PSEGLI	Electrical Service Connection
NYSDEC	General Permit for Stormwater Discharges from Construction Activity

Notes:

- (1) Waivers for the building size, retail sales area and waivers from the Town Land Development Standards, which includes fencing along the property line adjacent to residentially zoned land, depth of landscaping in rear yard setback, spacing of street trees along frontage and minimum width of landscaping along street frontage.
- (2) Variances to permit parking in the front yard and variances from the Town Land Development Standards
- (3) Variances for the minimum lot area, minimum rear yard setback, area of the building wall sign, and area of the ground sign.

SECTION 2.0

LAND USE, ZONING AND TOWN PLAN CONFORMANCE ANALYSIS

2.0 LAND USE, ZONING, AND TOWN PLAN CONFORMANCE ANALYSIS

2.1 Land Use

The subject site is presently a developed property occupied by a 2,410 SF 7-11 convenience store. The site is operational and well maintained. **Appendix A** includes a series of photographs depicting the condition of the site as well as surrounding conditions. These photos represent properties in the vicinity which typify the type and pattern of local land uses. There is also a key map to identify the locations of the photographs.

Abutting properties include residential properties to the north, southeast, and northwest, a commercial use to the northeast and southwest (strip mall), a Mobil gas station to the east, vacant wooded land (owned by the owners of the subject site) to the south, and an office building to the west. The pattern of land uses in the general vicinity is shown in **Figures 2-1** and summarized in **Table 2-1**.

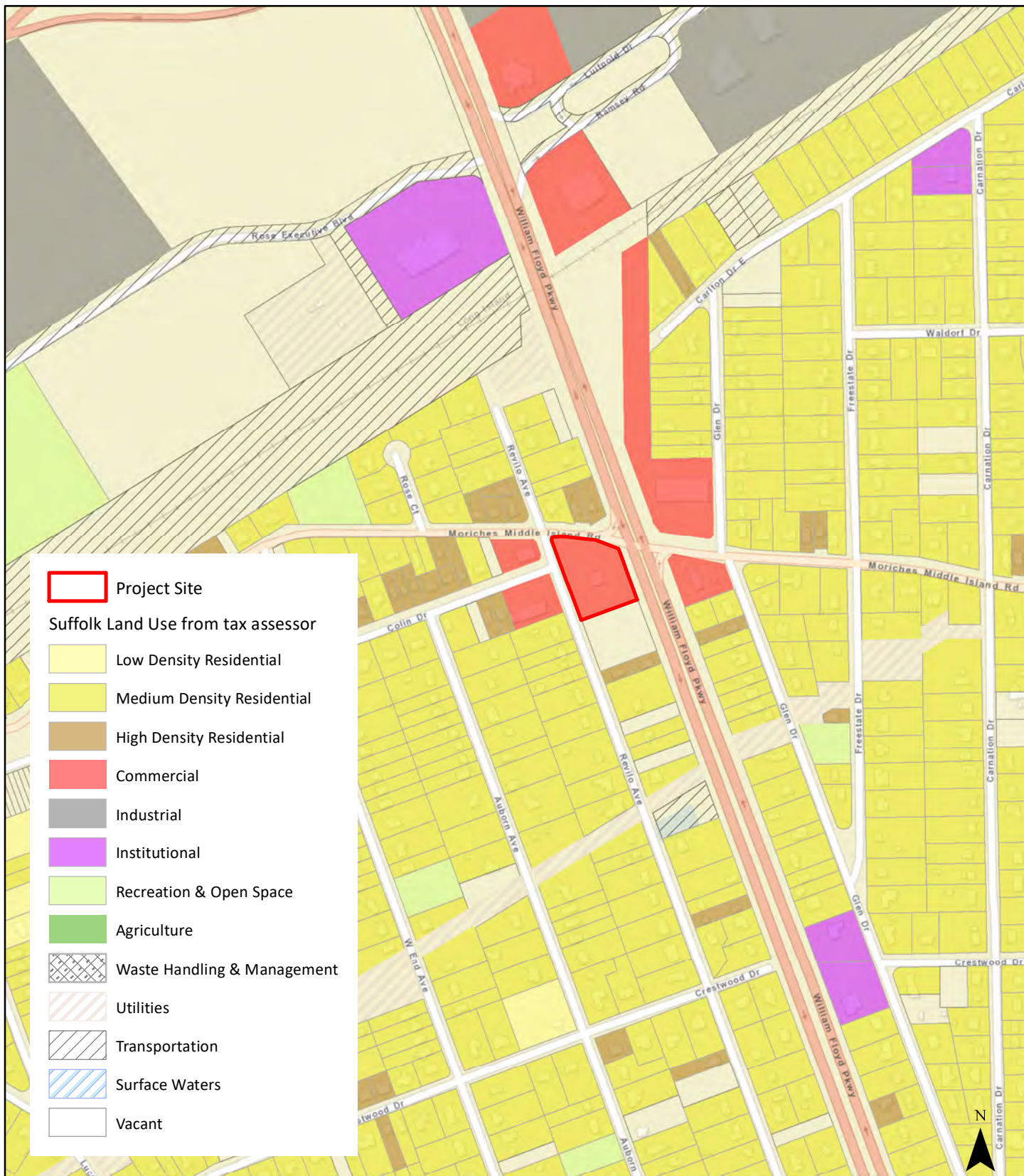
TABLE 2-1
LAND USES IN THE VICINITY

Property to the...	Abutting the Project Site	In the Vicinity
North	Residential	Commercial, Industrial, Residential, & Institutional
Northeast	Commercial (strip center)	Residential
East	Commercial (gas station)	Residential
Southeast	Residential	Residential & Institutional
South	Vacant (wooded)	Residential
Southwest	Commercial (strip center)	Residential
West	Commercial (office building)	Residential
Northwest	Residential	Residential

The pattern of land uses in the vicinity is dominated by residential and commercial uses, with commercial uses dominating the intersection of William Floyd Parkway and Moriches-Middle Island Road, and residential uses dominating the overall neighborhood.

Both the prior and proposed land usage of the subject site is commercial; however, the proposed land use would incorporate the addition of a motor fueling station. As a result, the general degree of this land use change is not substantial. In this case, the specific type of commercial use will be changed from a 7-11 convenience store to a 7-11 convenience store with a fuel station.

Unlike the proposed project, the existing 7-11 operation does not include a fueling station facility, which is significant impairment on sales and the ability to serve the current consumer market for such facilities. As a result, the 7-11 facility will be upgraded and modified with a newer, larger sales area and expanded product lines and services to include the fueling station.



With respect to the pattern of land uses, it is noted that the proposed project will not change the land use of the subject site, so that it would not impact the pattern of land used in the vicinity. The site is currently a commercial property and will remain so with the proposed project; the project will add a commercial operation on a site that is already in a commercial use. The proposed project reflects and complements the uses of the adjacent and nearby sites at the intersection of William Floyd Parkway and Moriches-Middle Island Road, particularly as facilities offering the same product and services already exist at this intersection. However, the project will provide these uses and services at one location. As a result, it is not expected that there will be a significant or an adverse impact with respect to land use, as the site had been a commercial site, and the proposed project will simply add a related commercial use to those that are currently found on the site.

2.2 Zoning

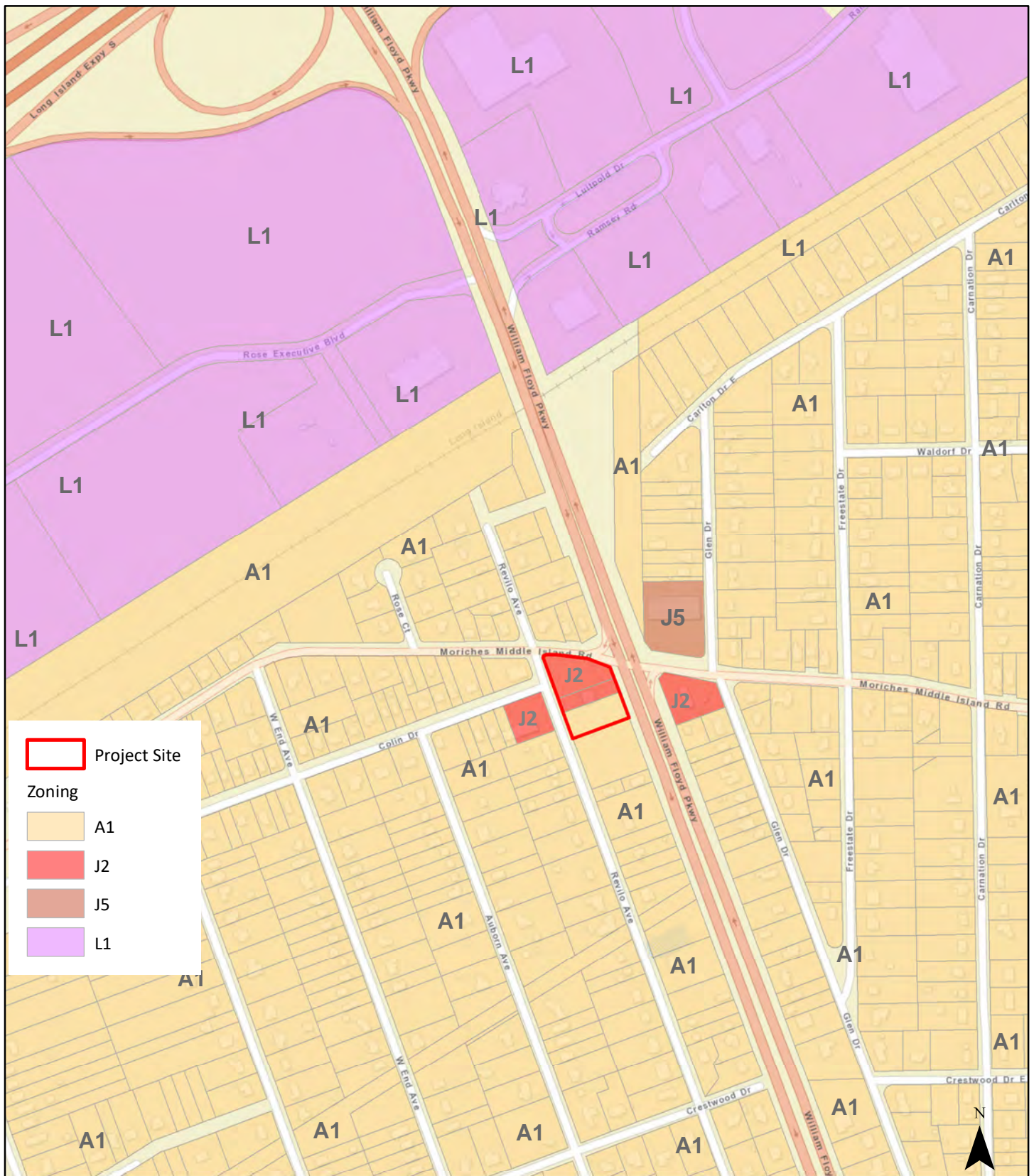
As shown in **Figure 2-2**, the subject site is currently zoned J-General Business-2. This zoning district is applied to this site based on the current 7-11 convenience store located on the property. The applicant is requesting a change of zone to J-High Intensity Business-5 as to allow for the construction of a fueling station.

The zoning classification of abutting properties include A-Residence-1 to the northwest and south, as well as J-2 to the east and southwest and J-5 to the northeast. The Mobil fuel station is located approximately 300 feet east of the subject site; this site is located in a J-2 district as a pre-existing use. The pattern of zoning districts in the vicinity is shown in **Figure 2-2** and summarized in **Table 2-2**.

TABLE 2-2
ZONING IN THE VICINITY

Property to the...	Abutting the Project Site	In the Vicinity
North	A-1	A-1
Northeast	J-5	A-1
East	J-2	A-1
Southeast	A-1	A-1
South	A-1	A-1
Southwest	J-2	A-1
West	A-1	A-1
Northwest	A-1	A-1

The zoning pattern in the vicinity closely follows that of land uses, in that commercial uses dominate the eastern corner of the intersection between William Floyd Pkwy and Moriches-Middle Island Rd (J-5 and J-2). The subject site is currently zones for commercial use, while the overall neighborhood is zoned residential.



The subject site is requesting a change of zone to the J-5 district to allow for a motor vehicle fueling station with a convenience store as an accessory use. As per Town Code Section 85-460 (G) a Town Board Special Permit is required for the construction of a motor vehicle fueling station in a J-5 district. A convenience store in the J-5 district usually requires a special permit from the Town Planning Board, unless it is an accessory to a motor vehicle fueling station, in which case the special permit is under the jurisdiction of the Town Board (Town Code Section 85-461 A).

Rezone from J-Business-2 and A-Residence-1 to J-High Intensity Business-5 - The proposed project will change the zoning of the site and will introduce a zoning district to the subject site that is consistent with the surrounding land uses and the proposed use. It is noted that there is a J-5 zoned site located approximately 500 feet northeast of the subject site. The J-5 site is occupied by strip mall with a pizzeria, nail salon, supermarket, bagel café, salon, liquor store, cleaners, and a Chinese and Japanese restaurant.

As a result, the proposed zoning will not introduce a new zoning district in this segment of the William Floyd Parkway Corridor.

It is noted that, while the project will slightly change the pattern of zoning in the area (increase intensity), the change in zoning is limited to a change from one Business district to another, and so does not reflect a significant change; it will not change the overall acreage of Business-zoned land in the area.

With respect to zoning, no significant adverse impacts are anticipated from the rezoning, or to the pattern of zonings in the area.

The proposed J-5 rezoning is necessary to enable the proposed project to be developed. The J-5 district provides the base zoning, and two (2) special permits are needed to accommodate the motor vehicle fueling station and accessory convenience store. This type of use is appropriate for a location along a major roadway as fueling facilities are necessary and in demand, and sale of convenience goods is a common accessory use to fueling stations.

Table 2-3 below lists the bulk requirements and development standards for the site in the J-5 district, with the corresponding values of the proposed project. For those requirements not met, the project will obtain the necessary relief, in the form of variances and waiver.

TABLE 2-3 - ZONING CONFORMANCE
Proposed J-High Intensity Business-5 District

Parameter	Required	Proposed Project
Bulk Requirements		
Lot Area (min.)	65,000 SF	59,951 SF (V; TB)
Lot Width Throughout (min.)	150 feet	221 FT
Permitted FAR (max.)	25%	5%
Front Yard Setback (min.)	50 feet	55.5 feet (canopy) 50.0 feet (building)
Side Yard (min.)	25 feet	N/A (canopy) N/A (building)
Rear Yard (min.)	40 feet	25 feet (V; BZA)
Building Height (max.)	35 feet, or 2-1/2 stories	<35 feet, 1 story
Sales Area (max.)	750 SF	1,850 SF (W; TB)
Total Building Area (max.)	1,500 SF	2,995 SF (W & PBCs; TB)
Land Development Standards		
<i>Natural/Landscaping:</i>	--	--
Percentage of Commercial Lot (min.)	20%	38.72%
In Front Yard (of Total Required; min.)	50%	144.79%
Street Trees	4 inch min., spaced 30 feet	Does not comply (V; PB)
Landscaping along street frontage (min.)	15 feet	8.3 feet (V; PB)
Depth of landscaping rear yard (min.)	40 ft	25 ft (V; PB)
<i>Buffer requirements:</i>	--	--
To Residential Use or Zone (min.)	25 feet	25 feet
Buffer along Street Frontage (min.)	25 feet	25 feet
<i>Curb Cut Requirements:</i>	--	--
Number of Curb Cuts (min.)	1 per 75 feet of frontage (768 LF frontage -10 curb cuts)	3
Distance from Adjacent Property (min.)	25 feet	27 feet
Driveway Width (min.)	20 feet	complies
Driveway Width (max.)	35 feet	complies
Distance between Driveways (min.)	25 feet	N/A
Parking/Loading Stall Requirements		
Parking Stall Size (min.)	9 feet x 19 feet	9 feet x 19 feet
Parking Stalls (min.)	1 stall/100 SF (2,995 SF - 30 spaces + minimum of 2 stalls for motor vehicle fueling station)	32 (V; PB)
Parking in Front Yard	Prohibited	Parking proposed in front yard
Loading Stalls (building <8,000 SF; min.)	1	1
Aisle Width (min.)	n/a	30 feet
Sign Requirements		
Ground Sign	1 sign, max 24 SF	32 SF (V; BZA)
Wall Sign	2 SF per LF, max 40 SF	52.5 SF (V; BZA)
Miscellaneous Requirements		
Outdoor Sales, Display, Storage	Prohibited	Will comply
Fence	6 feet high, opaque	Does not comply (V; TB)
Waste Receptacles	Provided for customer use	Will comply
Distance to Place of Public Assembly	Prohibited within 500 feet	Complies

Key: (V) Variance; (W) Waiver; (TB) Town Board; (PB) Planning Board; (BZA) Board of Zoning Appeals

Special Permits for fueling station and for an accessory convenience store - under Town Code Section 85-460 G, a motor vehicle fueling station facility requires a Town Board Special Permit, and the proposed convenience store requires a Special Permit as an accessory use to a motor vehicle fueling station under Town Code Section 85-463 A.

There are no specific Special Permit criteria for these uses listed in the Town Zoning Code. As a result, these Special Permits will be reviewed under general Special Permit criteria as outlined in Section 85-68 B (1) and Section 85-68 B (2). **Table 3-1** presents the standards under which the Town Board will review the applications for these special use permits along with analyses of whether and how the project conforms to each. The analyses support a conclusion that the project conforms to the standards.

Analyses for the Special Permits indicate that the project conforms to the applicable standards, so that no substantial adverse impacts are anticipated.

Town Board Variance and Waivers - As indicated in **Table 4-1**, the proposed project will require variances and waivers from strict compliance with Town Zoning Code requirements.

Under Town Code Section 85-68 B (3) (b), the Town Board is empowered by NYS Town Law Section 274b to grant waivers from strict conformance to its Code requirements, provided that analysis of the requests is made using the same standards as apply to a zoning board of appeals for area variances. These standards are given in NYS Town Law Section 267-b (3)(b).


With respect to the requested Town Board Variances and Waivers, analyses indicate that the project conforms to the applicable standards; no substantial adverse impacts are anticipated.

2.3 Town Comprehensive Land Use Plan

Brookhaven Town completed an update to the Town's Comprehensive Plan in 1996. The Town of Brookhaven Comprehensive Land Use Plan (hereafter, "the Town Plan") reviewed zoning, land use, demographic and environmental trends and provided a basis for land use recommendations to guide the Town into the 21st Century. The subject site is recognized in the Town Plan and is recommended for residential use on lots of one acre or less (see **Figure 2-3**). The J-2 and J-5 parcels on the corner of William Floyd Parkway and Moriches-Middle Island Road were also recommended for residential use in the Town Plan.

The project does not conform to the one acre or less residential use recommended for the site; it is a business use on a business-zoned site across from a business-zoned site in a business land use, located on a major roadway and within an established commercial corridor, and proximate to other, complementary businesses. Thus, the site and nearby properties to the east, northeast, and west have developed in a land use that does not conform to their respective Town Plan recommendations. The site currently operates as a 7-11 store, and the existing business use of the site will not change as a result of the project.



 NPV	FIGURE 2-3 RECOMMENDED LAND USES 1996 Town Comprehensive Land Use Plan	7-11 Shirley
	Source: ESRI WMS; Town of Brookhaven, 1996 Scale: 1 inch = 2,000 feet	Environmental Planning Assessment

SECTION 3.0

TOWN BOARD SPECIAL PERMIT STANDARDS CONFORMANCE ANALYSIS

3.0 TOWN BOARD SPECIAL PERMIT STANDARDS CONFORMANCE ANALYSIS

As noted in **Section 1.1**, in the J-5 zoning district, as provided for under Sections 85-460 G and Section 85-463 A of the Town Zoning Code, the proposed project will require approval of Special Permits from the Town board for a motor vehicle fueling station and the accessory convenience store, respectively. In reaching its decision on these requests, the Town Board will consider the review standards given in Town Code Sections 85-68 B (1) and 85-68 B (2).

Table 3-1 lists these standards and provides the Applicant's responses to each, for both of the special permit requests.

Based the analysis contained **Table 3-1**, the project is found to be in conformance with the review standards for a Town Board Special Permit to permit a motor vehicle fueling station and the accessory convenience store.

TABLE 3-1
CONFORMANCE TO TOWN BOARD STANDARDS FOR SPECIAL USE PERMITS

Standard		For Motor Vehicle Fueling Station Use in J-5 District	For Convenience Store as Accessory Use to Motor Vehicle Fueling Station in J-5 District
		85-68 B (1) No special use permit shall be granted by the Town Board unless it shall determine...	
(a) That the use will not prevent the orderly and reasonable use of adjacent properties or of properties in the surrounding area or impair the value thereof.		The subject site is currently a convenience store. The site is of sufficient size to accommodate the proposed use. Minimum site size is 65,000 SF, and the site is 59,951 SF, which is a 5,049 SF/7.8% shortfall of the requirement. This portion of the William Floyd Parkway corridor is currently established in commercial uses, and the proposed use will be consistent with this established pattern of use. Access to the project will continue to be from Revilo Avenue, Moriches-Middle Island Road, and William Floyd Parkway and via driveways at the same locations as presently exist. The multiple access points allow cars to safely and easily enter and exit the site. The southern boundary of the property abuts the only contiguous parcel of land (all other sides of the site abut roads), and this parcel is owned by the applicant, intended to remain vacant and is currently wooded, providing a buffer from other residential properties. The site is on the corner of a major intersection and across from another motor vehicle fueling station and two strip centers. The nature of the project is such that its use, design, operation and maintenance would not tend to pose any significant potential impact on the public. The project is subject to the review and approval of the Town Board and Town Planning Board, which will ensure proper site use/design.	
(b) That the use will not prevent the orderly and reasonable use of permitted or legally established uses in the district wherein the proposed use is to be located or of permitted or legally established uses in adjacent districts.		The proposed project will not prevent the orderly/reasonable use of permitted or legally established uses in the area. The site is of a size that can accommodate the proposed use. Residential uses to the south will be buffered by the presence of a wooded lot (41.1), which is owned by the same entity that owns the project site, the proposed use will not adversely impact uses on nearby properties, and is consistent with the pattern of land use in the area. The nature and scale of the project is such that it would not tend to cause significant impacts within the larger vicinity from traffic, safety, noise, odors or operational concerns.	
(c) That the safety, health, welfare, comfort, convenience or order of the Town will not be adversely affected by the proposed use and its location.		The project is designed to conform with applicable requirements of the SCDHS and provisions of Town Code except as otherwise waived or varied based on specific considerations, so the potential for any impact to the safety, health, welfare, comfort or convenience of the public are minimized. The proposed project will include proper design, installation and monitoring of tankage associated with fuel supply in conformance with SCSC Article 12. The project will contain all stormwater and will provide appropriate systems for sanitary waste handling and refuse management. The nature of the project is such that its use, design, operation and maintenance would not tend to pose any significant potential impact on the public.	
(d) That the use will be in harmony with and promote the general purposes and intent of this chapter.		The subject site is currently a developed and occupied 7-11 convenience store, and the proposed project is a new 7-11 convenience store with a fueling station. As such, the site has proven to be of sufficient size to accommodate a 7-11 store use, and the fueling facility is designed to operate in a safe and efficient manner within the property area. As noted, the vacant, wooded site to the south will remain. The area is currently established in commercial uses, and the proposed use will be consistent with this established pattern. The project is designed to provide appropriate design and buffering. The nature of the project is such that its use, design, operation and maintenance will be in harmony with sound planning practices and will not be a detriment to the community. The project is subject to the review and approval of the Town Board and Town Planning Board, which will ensure proper site use/design.	
		85-68 B (2) In making such determination, the Town Board shall give consideration, among other things, to...	
(a) The character of the existing and probable development of uses in the district and the peculiar suitability of such district for the location of any of such permissive uses.		The subject site is located in an existing commercial area that is zoned for business uses and on a site that is presently zoned for commercial uses; the project requires that the site be rezoned to a district that is specifically designed and intended for the proposed gas station/accessory convenience store operation, a zone that is already present on the adjacent property (across existing roads). Within this area, other commercial uses of similar and/or complementary natures predominate, so that the project would not tend to contrast with the pattern of land uses or zoning.	
(b) The conservation of property values and the encouragement of the most appropriate uses of land.		The subject site is currently a developed and occupied 7-11 convenience store, and the proposed project is a new 7-11 convenience store with a fueling station. The proposed project will continue a use on the site that will provide tax revenue and job creation. The project is a commercial gas station/convenience store operation on a site that will be, with approval of the Town Board, rezoned to the commercial district designed and intended for that specific use. Additionally, the site is in an established commercial area, and abuts other similar and/or complementary commercial uses. The project design conforms to most of the applicable Town Zoning Code requirements, and in those cases where conformance cannot be provided, the project will obtain the necessary Code relaxations. To protect the values of the nearby properties, the project's lighting system will produce a safe and secure environment with illumination directed to those areas where it is appropriate and/or necessary and shielded so as to minimize the potential for fugitive lighting. Over 38% of the site will be landscaped and will include retaining four (4) trees along Revilo Avenue, new tree plantings along all four site borders, new trees to be planted within the parking area islands, abutting the building and the site's southerly landscaped area, to reduce the aesthetic impact of the developed nature of site. As noted, the vacant, wooded site to the south will remain. It's retention as a natural wooded site provides buffering. Based on these factors, the proposed project would continue to conserve property values and continue to be an appropriate land use on the site.	
(c) The effect that the location of the proposed use may have upon the creation or undue increase of traffic congestion on public streets, highways or waterways.		The three roads that access the project have the capacity to serve the project. The TIS prepared for the project concludes: "The analysis findings, which have been based on industry-standard guidelines, indicate that the proposed development would not have a significant impact on the traffic operations of the adjacent roadway network. The site driveways and on-site layout have been designed to provide for effective access to and from the subject property. The proposed parking supply meets the Town's requirements and would be sufficient to support this project." Overall, the project will not cause an undue increase in traffic.	

Standard	For Motor Vehicle Fueling Station Use in J-5 District	For Convenience Store as Accessory Use to Motor Vehicle Fueling Station in J-5 District
(d) The availability of adequate and proper public or private facilities for the treatment, removal or discharge of sewage, refuse or other effluent, whether liquid, solid, gaseous or otherwise, that may be caused or created by or as a result of the use.	Not applicable; no wastes will be generated by the fueling station component of the project.	The nature of the convenience store component is such that no hazardous wastes will be generated, stored or disposed of on the site, so that no potential for impact from such material would occur. This operation will generate a small amount of sanitary wastewater, which will be treated and recharged on-site in a septic system that conforms to Suffolk County Sanitary Code (SCSC) Article 6 requirements. Stormwater will be managed consistent with Town engineering requirements; stormwater will be retained on-site and the drainage system will be designed by a licensed professional engineer and reviewed by the Town through the site plan review process. No other liquid wastes will be generated. The refuse generated on-site will be handled by site employees and removed by a licensed carter for disposal at an approved facility.
(e) Whether the use or materials incidental thereto or produced thereby may give off obnoxious gases, odors, smoke or soot.	It is unlikely that gasoline odors may occur in association with the fueling station operation. The project will incorporate appropriate odor and gas fume control measures available including but not limited to nozzle attachments that seal the opening to the gas tank filler port on vehicles. Additionally, when fuel tanker trucks come to fill the site's storage tanks, similar technology is used to minimize fugitive odors and fuel spillage. The fueling station component does not include other activities that emit smoke or soot. There is another fueling station in the area and the subject site is surrounded by three roads and a vacant parcel. As a result, no air or odor impacts are expected to occur.	Not applicable; the nature of the accessory convenience store component of the project is such that no emissions of obnoxious gasses, odors, smoke or soot would occur.
(f) Whether the use will cause disturbing emission of electrical discharges, dust, light, vibration or noise.	The project is a commercial gas station/convenience store operation and as such would not tend to cause any electrical discharges, raise dust (outside of the construction period), or generate noise or vibrations. The project does include pole-mounted light fixtures, but these will be shrouded and oriented to cast all lighting downwards and into the site, thereby minimizing the potential for fugitive lighting to impact the residences to the north, west or south. Any homes are across existing roads to the north and west, and vacant land to the south. East of the site is William Floyd Parkway. The lighting system, along with the overall project, will be subject to the review and approval of the Town during the site plan review process. The project will comply with all applicable Town Code requirements regarding exterior lighting.	
(g) Whether the operation in pursuance of the use will cause undue interference with the orderly enjoyment by the public of parking or of recreational facilities, if existing or if proposed by the Town or by other competent governmental agency.	The proposed project is a commercial gas station/convenience store operation and as such would not cause any undue interference with the public's use and enjoyment of any existing or planned parking or recreational facilities. Additionally, no such public facilities exist or are planned for the immediate area.	
(h) The necessity for an asphaltic or concrete surfaced area for purposes of off-street parking and loading of vehicles incidental to the use and whether such area is reasonably adequate and appropriate and can be furnished by the owner of the plot sought to be used within or adjacent to the plot wherein the use shall be had.	The project design conforms to the applicable Town Zoning Code requirements for on-site parking spaces, parking aisle widths and loading area. Generally, the amount of asphalt and concrete surfaces has been minimized to the extent practicable, considering the need to provide access and circulation on the site for the building and associated gas fueling station components.	
(i) Whether a hazard to life, limb or property because of fire, flood, erosion or panic may be created by reason or as a result of the use or by the structures to be used therefor or by the inaccessibility of the plot or structures thereon for the convenient entry and operation of fire and other emergency apparatus or by the undue concentration or assemblage of persons upon such plot.	The project is a gas station/convenience store operation and as such would not tend to be associated with a potential for hazard to life, limb or property in the area. The gas station component may cause concern with respect to fire safety, but the project's conformance to all applicable Town, County and/or NYS requirements regarding this operation and overall site layout and design will fully address such concerns. Such fuel stations are common and employ measures to ensure safe operations, particularly conformance with Article 12 of the Suffolk County Sanitary Code for tank storage and NYS Fire Code requirements. The project will also undergo Planning Board and Fire Marshal review. Additionally, no concerns with respect to flooding or erosion are anticipated, as the site is well outside any flood hazard zone, the site includes an on-site drainage system, and construction will include erosion control measures and conformance with any applicable Town or NYSDEC SWPPP requirements.	
(j) Whether the use or the structures to be used therefor will cause an overcrowding of land or undue concentration of population.	The project is a commercial gas station/convenience store operation and as such would not tend to cause any undue or significant concentration of people on the site. A fuel station and convenience store involve temporary, short-term occupancy for sale of fuel and goods and do not tend to cause a concentration of people.	
(k) Whether the plot area is sufficient, appropriate and adequate for the use and the reasonable anticipated operation and expansion thereof.	The project is a commercial gas station/convenience store operation and as such would not tend to require a large site in order to operate properly and safely. Minimum site size is 65,000 SF, and the site is 59,951 SF in size, a 5,049 SF/7.8% shortfall. The site is of a sufficient size to include the standard store building for this use, adequate area for the fuel pump islands, parking, internal circulation aisles and loading/unloading area, with adequate front, side, and rear yard buffers.	

(l) The physical characteristics and topography of the land.	The physical characteristics of the site, including but not limited to its size, shape, topography, soils, water resources and its vegetation, pose no limitations on development of the project, or on its design and operations. The site is topographically flat and has been developed and occupied by a similar use for a number of years. There are no physical or topographic constraints on the property.
(m) Whether the use to be operated is unreasonably near to a church, school, theater, recreational area or place of public assembly.	Based on field inspection of the site and area, there are no places of public assembly within 500 feet of the subject site, nor are there facilities serving children, the elderly, or people with disabilities (e.g., schools, hospitals, licensed day care facilities, group homes, etc.) within 1,500 feet of the site.

SECTION 4.0

VARIANCE AND WAIVER STANDARDS

CONFORMANCE ANALYSIS

4.0 VARIANCE & WAIVER STANDARDS CONFORMANCE ANALYSIS

As noted in **Section 1.1**, the proposed project will require approval of eight (8) variances and two (2) waivers. In reaching its decision on these requests, the each board will consider the review standards given in NYS Town Law Section 267-b 3.

Table 4-1 lists these standards and provides the Applicant's responses to each, for each of the variance and waiver requests.

Based the analysis contained **Table 4-1**, the proposed project is found to be in consistent with the review standards for the variances and waivers requested.

TABLE 4-1
CONFORMANCE TO STANDARDS FOR VARIANCES AND WAIVERS

Standard ^(1, 2)	Waiver for maximum allowed sales area in the accessory retail space <i>per Section 85-467 B. 3, 750 SF max. allowed; 1,850 SF proposed)</i>	Waiver for maximum allowed total building area <i>(per Section 85-467 B. 3, 1,500 SF max. allowed; 2,995 SF proposed)</i>	Variance for minimum lot area <i>(per Section 85-466 A. (3), 1.49 acres/65,000 SF min. required; 1.38 acres/59,951 SF proposed)</i>
<i>267-b 3. Area Variances. (b) In making its determination, the zoning board of appeals shall take into consideration the benefit to the applicant if the variance is granted, as weighed against the detriment to the health, safety and welfare of the neighborhood or community by such grant. In making such a determination, the board shall also consider...</i>			
(1) whether an undesirable change will be produced in the character of the neighborhood or a detriment to nearby properties will be created by the granting of the area variance;	The project is a commercial use on a site that is currently developed commercially and with a use that is identical to that existing use (other than the addition of a motor vehicle fueling station which is customary for 7-11 convenience store uses), and is located in an existing commercial area. Thus, the project will not change the character of the neighborhood; the project's design will be subject to the review and approval of the Town and County, ensuring that the project will not prove a detriment to the properties in the area. As required under Town Zoning Code Section 85-467 B. 3 (b), the waiver to exceed the maximum allowed building area requires the redemption of one (1) PBC. The applicant will purchase 1.00 PBC as required.	The 1.38 acre site is developed and occupied by a 7-11 convenience store, while the proposed project is a new 7-11 convenience store with an accessory fueling station. There will be no removal of buffering natural forest; site lighting and traffic conditions are not expected to substantially change from those already occurring and to which the community has become accustomed. As such, the character of the use on the site will not substantially change, and the project would not present a significant impact on the community.	
(2) whether the benefit sought by the applicant can be achieved by some method, feasible for the applicant to pursue, other than an area variance;	The Applicant has considered other feasible methods to achieve the benefit of the project; however, none exist. The vacant, wooded site to the south will remain and this provides buffering which should be considered in the context of this criterion. The project involves developing a standardized building design, having a standardized amount of accessory convenience goods sales area, and based on a standardized site layout. As a result, the Applicant has little flexibility to alter any of these aspects of the project. The building as designed fits well within the site and though variances are needed, these are justified by the safe operational characteristics of the site, the fact that the site is bordered by three roads and vacant land, and the lack of adverse impacts expected to occur as a result of the project.		The only way to meet the minimum lot size requirement would be to increase the size of the site, by adding the adjacent tax lot 41.1 to the site. This could be easily accomplished, as the owner of lot 41.1 is also the owner of the project site. However, this addition would decrease (if not eliminate) the naturally-vegetated buffer that tax lot 41.1 represents for the residences to the south. It is the Applicant's opinion that the benefit of a larger site would not outweigh the detriment to those neighbors by eliminating this natural buffer.
(3) whether the requested area variance is substantial;	The maximum allowed sales area is 750 SF, and the requested sales area is 1,850 SF, a 1,100 SF/147% waiver. One of the reasons for the size of the convenience store is to meet ADA requirements for aisle width and restroom size. Given the ability of the site to accommodate the request, and the typical size of similar sales facilities, this request is not considered substantial.	The maximum allowed building area is 1,500 SF, and the proposed building area is 2,995 SF, a 1,495 SF/99.7% waiver. One of the reasons for the size of the convenience store is to meet ADA requirements for aisle width and restroom size. Given the ability of the site to accommodate the request, and the typical size of similar sales facilities, this request is not considered substantial.	The minimum site size is 65,000 SF, and the site is 59,951 SF, which is a 5,049 SF/7.8% shortfall of the requirement, which is not considered to be substantial. The vacant, wooded site to the south will remain and this provides buffering which should be considered in the context of this criterion.
(4) whether the proposed variance will have an adverse effect or impact on the physical or environmental conditions in the neighborhood or district;	As the subject site had previously been cleared and developed (including topography, soils, and vegetation), are not natural in character, and no surface water bodies are present. The project involves the redevelopment of a convenience store operation with a convenience store/gas station operation which would tend to present minimal potential for adverse impacts to the environment, particularly in consideration of safeguards for site operation and the common and necessary function that fueling stations provide to consumers in commercial area settings. Finally, the project design conforms to most of the applicable the applicable Town Zoning Code requirements, and in those cases where conformance cannot be provided, the project will obtain the necessary Code relaxations, to protect the nearby residences from potential impacts from the project. Considering these factors, the potential for adverse impact on conditions in the neighborhood would be minimized.	As the subject site had previously been cleared and developed (including topography, soils, and vegetation), are not natural in character, and no surface water bodies are present. The project involves the redevelopment of a convenience store operation with a convenience store/gas station operation which would tend to present minimal potential for adverse impacts to the environment, particularly in consideration of safeguards for site operation and the common and necessary function that fueling stations provide to consumers in commercial area settings. Finally, the project design conforms to most of the applicable the applicable Town Zoning Code requirements, and in those cases where conformance cannot be provided, the project will obtain the necessary Code relaxations, to protect the nearby residences from potential impacts from the project. Considering these factors, the potential for adverse impact on conditions in the neighborhood would be minimized.	Meeting the Zoning Code requirement would increase impacts on the neighborhood, while the project would reduce this existing impact. The need for this variance would be obviated by simply adding tax lot 41.1 (owned by the same entity that owns the subject site) to the proposed project site. However, this addition would decrease (if not eliminate) the naturally-vegetated buffer that tax lot 41.1 represents for the residences to the south. It is the Applicant's opinion that the benefit of a larger site would not outweigh the detriment to those neighbors by eliminating this natural buffer.
(5) whether the alleged difficulty was self-created, which consideration shall be relevant to the decision of the board of appeals, but shall not necessarily preclude the granting of the area variance.	The proposed facility involves a standard building design, which has a sales area of 1,850 SF. As a result, the Applicant has no flexibility in regard to the size of the sales area and so is seeking the requested Waiver. This consideration is relevant but is not a sole determining factor.	The proposed facility involves a standard building design, which is 2,995 SF in size. As a result, the Applicant has no flexibility in regard to the size of the building and so is seeking the requested Waiver. This consideration is relevant but is not a sole determining factor.	The Applicant deliberately chose to not include tax lot 41.1 in the project site, so that the naturally-vegetated buffer on lot 41.1 would remain to serve as an aesthetic, noise and lighting buffer to the residences to the south.

Standard ^(1, 2)	Variance for minimum width of landscaping along the street frontage (per Section 85-843 A. 4, 15 feet min. required; 8.3 feet provided)	Variance for area of building wall sign (per Section 57A-6 E, 40 SF max. allowed; 52.5 SF proposed)	Variance for area of ground sign (per Section 57A-6 C, 24 SF max. allowed; 32 SF proposed)
267-b 3. Area Variances. (b) In making its determination, the zoning board of appeals shall take into consideration the benefit to the applicant if the variance is granted, as weighed against the detriment to the health, safety and welfare of the neighborhood or community by such grant. In making such a determination, the board shall also consider...			
(1) whether an undesirable change will be produced in the character of the neighborhood or a detriment to nearby properties will be created by the granting of the area variance;	The site is relatively small (1.38 acres) and is designed using a corporate standard layout, leaving little ability to provide deeper, conforming landscaped frontages. Additionally, the site is occupied by a 7-11 convenience store, a use that requires that this facility be easily seen by approaching drivers, in order to access the site safely and easily. As such, a deep belt of landscaping along the site’s frontages is not a desirable feature where the goal is attracting customers. The vacant, wooded site to the south will remain and this provides buffering which should be considered in the context of this criterion. This site provides vacant, wooded frontage on William Floyd Parkway.	The site is already developed with a 7-11 convenience store, which includes ground and wall signs that are corporation-standard features. Maintaining signage on the site that readily identifies the 7-11 use is important to the success and operation of the business. As such, the Applicant has no flexibility in regard to the sizes of these features and so is seeking the requested variances. The subject site is in an area with commercial uses and such uses involve signage for identification of the uses.	
(2) whether the benefit sought by the applicant can be achieved by some method, feasible for the applicant to pursue, other than an area variance;	The shortfall in landscaped yard depth occurs in a limited (40 feet) portion of the frontage on William Floyd Parkway. The potential for adverse impact would be minimal over such a short width. The landscaping proposed in this area could be made denser, to disguise the presence of the parking spaces here. The site is bordered by three roads and currently has minimal landscaping. The new site plan will improve site conditions and the applicant is retaining the vacant, wooded land associated with Lot 41.1 to the south.	The site is already developed with a 7-11 convenience store, which includes wall signs that are corporation-standard features. Maintaining signage on the site that readily identifies the 7-11 use is important to the success and operation of the business. The Applicant seeks to keep the identifying ground signage and continue to 7-11 use that already exists on the property and so is seeking the requested variance.	The site is already developed with a 7-11 convenience store, which includes ground signs that are corporation-standard features. Maintaining signage on the site that readily identifies the 7-11 use is important to the success and operation of the business. The Applicant seeks to keep the identifying ground signage and continue to 7-11 use that already exists on the property and so is seeking the requested variance.
(3) whether the requested area variance is substantial;	The minimum width of landscaping along the street frontage is 15 feet, and the site plan provides for a minimum width of 8.3 feet, which is a 6.7 foot/44.7% shortfall of the requirement.	The maximum area of a building wall sign is 40 SF, and the site plan provides for a wall sign having 52.5 SF, which is 12.5 SF in excess of this requirement.	The maximum area of a ground sign is 24 SF, and the site plan provides for one ground sign having 32 SF, which is 8 SF in excess of this requirement.
(4) whether the proposed variance will have an adverse effect or impact on the physical or environmental conditions in the neighborhood or district;	This shortfall in landscaped yard depth is not expected to cause an adverse impact on the neighborhood, as the impact is small in magnitude (6.7 feet), and is limited to a roughly 40-foot wide section of the frontage on William Floyd Parkway.	The site is already developed with a 7-11 convenience store that includes ground and wall signs which are corporation-standard features. Maintaining signage on the site that readily identifies the 7-11 use is important to the success and operation of the business. As such, the Applicant has no flexibility in regard to the sizes of these features and so is seeking the required variances. However, these signs are not known to have caused any adverse impacts on the physical or environmental conditions in the neighborhood, so that the proposed project’s inclusion of these signs is likewise not expected to result in such impacts in the future.	The site is already developed with a 7-11 convenience store that includes ground and wall signs which are corporation-standard features. Maintaining signage on the site that readily identifies the 7-11 use is important to the success and operation of the business. As such, the Applicant has no flexibility in regard to the sizes of these features and so is seeking the required variances. However, these signs are not known to have caused any adverse impacts on the physical or environmental conditions in the neighborhood, so that the proposed project’s inclusion of these signs is likewise not expected to result in such impacts in the future.
(5) whether the alleged difficulty was self-created, which consideration shall be relevant to the decision of the board of appeals, but shall not necessarily preclude the granting of the area variance.	The site is not sufficiently large to allow for deeper, conforming yard depths over the site’s entire periphery. The site is already occupied by a 7-11 store which indicates the difficulty is not self-created. The addition of a fueling station at the site is a reaction to market conditions which are determined by consumers and are not self-created. The 7-11 use with fueling is a successful business model that is driven by consumer demand.		

Standard ^(1, 2)	Variance for fencing along property line adjacent to residentially-zoned land (per Section 85-467 B. 8, min. 6 foot opaque fence required, not proposed along northern and western property lines)	Variance for depth of landscaping in rear yard setback (per Section 85-467 B. 7, 40 feet min. required; 25 feet proposed)	Variance for minimum rear yard setback (per Section 85-466 E (1), 40 feet min. required; 25 feet proposed)
267-b 3. Area Variances. (b) In making its determination, the zoning board of appeals shall take into consideration the benefit to the applicant if the variance is granted, as weighed against the detriment to the health, safety and welfare of the neighborhood or community by such grant. In making such a determination, the board shall also consider...			
(1) whether an undesirable change will be produced in the character of the neighborhood or a detriment to nearby properties will be created by the granting of the area variance;	The project site is contiguous to residentially-zoned land to its south; residentially-zoned land is also present across Revilo Avenue (to the west) and Moriches-Middle Island Road (to the north). The project will replace the existing 4-foot high chain-link fencing along the southern and the western property lines, with opaque, 6-foot tall vinyl fencing. However, the project will not include opaque fencing along the remainder of its western property line, or along its northern (front) property line, in order to provide open and appropriate views of the front of the site and facility for passing drivers (and potential patrons). Additionally, the site (particularly its front yard) will be attractively landscaped, allowing the entire site to be open to view of passersby. It is not expected that the neighborhood’s character will be adversely impacted. Lot 41.1 will stay vacant/wooded.	The minimum depth of the rear yard, and the minimum depth of the landscaping within the rear yard, is 40 feet, while the site plan provides a rear yard depth of 25 feet and rear yard landscaping that is 25 feet deep. Conforming rear yard depth and conforming depth of landscaping in the rear yard cannot be provided because the project is based on the interplay among design considerations that include: corporate standards for building size, parking, and Town requirements for fueling station and front and side yards. It is noted that the site is currently occupied by a 7-11 use. The site is also bound on three sides by existing roads. The proposal will retain Lot 41.1 as a vacant, wooded parcel. The configuration of the site does not allow for the required rear yard and depth of landscaping. Given the existing use, three road frontages, retention of Lot 41.1 and the configuration of the site, the current site design is the most efficient and protective site design that achieves the intended goal of providing fuel services and therefore a variance is requested for these bulk standards.	
(2) whether the benefit sought by the applicant can be achieved by some method, feasible for the applicant to pursue, other than an area variance;	There is no feasible alternative to providing the required fencing on the site’s western and northern property lines that would also meet the Applicant’s need to provide a commercial facility that needs to be attractive and openly visible for patrons.	Conforming rear yard depth and conforming depth of landscaping in the rear yard cannot be provided because the project is based on the interplay among design considerations that include: corporate standards for the features proposed; and Town requirements for building size, parking, internal circulation, a 16-pump fueling station and front and side yards. The result is an inability to retain sufficient space to provide a conforming rear yard depth.	
(3) whether the requested area variance is substantial;	This variance is necessary because the project needs to be attractive and readily visible in order to draw patrons, while conformance to the Town Zoning Code would contradict these goals. This variance is not considered to be substantial.	The minimum depth of the rear yard, and the minimum depth of the landscaping within the rear yard, is 40 feet, while the site plan provides a rear yard depth of 25 feet and rear yard landscaping that is 25 feet deep. This indicates 15 foot shortfalls of these requirements.	
(4) whether the proposed variance will have an adverse effect or impact on the physical or environmental conditions in the neighborhood or district;	It is not expected that approval to not provide fencing across the site’s front yard would cause any significant adverse impact on the neighborhood; to the contrary, not providing the fencing would provide a visually-attractive and appropriate commercial use on the site.	It is not expected that a shortfall in the depth of the vegetated rear yard abutting tax Lot 41.1 (which is entirely naturally-vegetated and separates the site from its residential neighbors to the south) will cause any discernable adverse impact on these potential receptors.	
(5) whether the alleged difficulty was self-created, which consideration shall be relevant to the decision of the board of appeals, but shall not necessarily preclude the granting of the area variance.	The difficulty was not self-created; regardless of how the proposed project is designed, fencing along the site’s northern, southern and western borders would have been required. But a commercial facility would require that it be visible for approaching drivers to note its presence and safely access it, which would tend to argue against installing opaque fencing that would hide the facility.	Conforming rear yard depth and conforming depth of landscaping in the rear yard cannot be provided because the project is based on the interplay among design considerations that include: corporate standards for the features proposed; and Town requirements for building size, parking, internal circulation, a 16-pump fueling station and front and side yards. The result is an inability to retain sufficient space to provide a conforming rear yard depth.	

Standard ^(1, 2)	Variance for spacing of street trees along street frontage (per Section 85-843 A. 3, 30 feet required, not proposed)
(1) whether an undesirable change will be produced in the character of the neighborhood or a detriment to nearby properties will be created by the granting of the area variance;	The project’s landscaping design decision to not space the street trees 30 feet apart is not expected to cause any significant, adverse effect on the character of the neighborhood. The Landscaping Plan shows that all five (5) of the existing street trees on the site will be retained, to be supplemented with new plantings including groundcovers, shrubs and eighteen (18) additional trees. As required, species listed in Figure 5-2 of the CLUP will be used, the project’s landscaping design will enhance the neighborhood’s character, by providing a naturalistic appearance to the site, rather than a commercial appearance similar to that of the adjacent and nearby properties.
(2) whether the benefit sought by the applicant can be achieved by some method, feasible for the applicant to pursue, other than an area variance;	An alternative benefit is not practicable; the Applicant seeks to provide an attractive, natural appearance to the site by implementing a landscaping design that does not space the twenty three (23) street trees (five retained and eighteen new) a regular, 30-foot apart, but spaces them irregularly but separated enough to allow views into and out of the property.
(3) whether the requested area variance is substantial;	The requested variance is not substantial; the Landscaping Plan shows that the site’s street trees will be spaced between thirty (30) and up to about seventy (70) feet apart. In contrast, street trees along the site’s street frontage are required to be spaced 30 feet apart.
(4) whether the proposed variance will have an adverse effect or impact on the physical or environmental conditions in the neighborhood or district;	As noted above, the decision to not plant the new trees between the existing trees to provide a 30-foot spacing between trees will enhance the neighborhood’s character, by providing a naturalistic appearance to the site rather than a commercial appearance similar to that of the adjacent and nearby properties.
(5) whether the alleged difficulty was self-created, which consideration shall be relevant to the decision of the board of appeals, but shall not necessarily preclude the granting of the area variance.	The Applicant and Landscaping Plan designer made the decision to not space the street trees 30 feet apart, in order to provide a project that would balance the need to be attractive to the neighborhood (by use of appropriate species naturally arranged to replicate natural conditions) and provide visibility in attracting patrons to the site.

Notes:

(1) NYS Town Law Section 274-b. Approval of special use permits. 1. Definition of special use permit. As used in this section the term "special use permit" shall mean an authorization of a particular land use which is permitted in a zoning ordinance or local law, subject to requirements imposed by such zoning ordinance or local law to assure that the proposed use is in harmony with such zoning ordinance or local law and will not adversely affect the neighborhood if such requirements are met. 3. Application for area variance. Notwithstanding any provision of law to the contrary, where a proposed special use permit contains one or more features which do not comply with the zoning regulations, application may be made to the zoning board of appeals for an area variance pursuant to section two hundred sixty-seven-b of this article, without the necessity of a decision or determination of an administrative official charged with the enforcement of the zoning regulations.

(2) Per standards given in NYS Town Law Section 267-b 3.

SECTION 5.0

TOWN OF BROOKHAVEN CENTRAL PINE BARRENS DISTRICT STANDARDS CONFORMANCE ANALYSIS

5.0 TOWN OF BROOKHAVEN CENTRAL PINE BARRENS DISTRICT STANDARDS CONFORMANCE ANALYSIS

The Long Island Pine Barrens Act of 1993 divided the Long Island Pine Barrens into two geographic areas, the entire Central Pine Barrens (CPB) of about 100,000 acres, and within this larger area is the smaller 52,500 acre Core Preservation Area (CPA). Areas not contained within the CPA are referred to as the Compatible Growth Area (CGA) and comprise approximately 47,500 acres. As discussed in **Section 6.3** of this document and shown in **Figure 6-2**, in response to a recommendation of the Carmans River Plan, the boundary of the CGA was shifted eastwards, to run along William Floyd Parkway in the vicinity of the site, so that the subject site is now within the CGA.

An aspect of the CPB Comprehensive Land Use Plan (CLUP) is the provision for the individual Towns having lands within the Pine Barrens area to regulate development by writing and implementing Town Code standards that conform to those of the CLUP. Sections 85-718 through 85-726 of the Zoning Code of the Town of Brookhaven contain the Town's regulations for that portion of the Central Pine Barrens that are within the Town, and therefore are subject to Town control.

The following presents each of the standards of the Town Zoning Code for development within the CGA, with brief descriptions/discussions of the project's conformance to each.

A. Nitrate-nitrogen

- (1) *SCSC Article 6 compliance. All development proposals subject to Article 6 of the SCSC shall meet all applicable requirements of the SCDHS. Projects which require variances from the provisions of Article 6 shall meet all requirements of the SCDHS's Board of Review in order to be deemed to have met the requirements of this standard.*

The estimated 150 gpd of wastewater generated by the proposed project will be treated and recharged to groundwater through an on-site septic system conforming to SCSC Article 6 requirements. Appropriate County approvals and permits will be obtained. The proposed project will not exceed SCSC Article 6 flow allowed in Groundwater Management Zone VI (828 gpd), and will in fact be much less than the allowable flow for the site.

- (2) *Sewage treatment plant discharge. Where deemed practical by the County or State, sewage treatment plant discharge shall be outside and downgradient of the Central Pine Barrens. Denitrification systems that are approved by the NYSDEC or the SCDHS may be used in lieu of STPs.*

The proposed project will conform to SCSC Article 6 requirements, and no sewage treatment plant (STP) will be necessary. It is acknowledged that the project's effluent will be recharged

within the CGA. However, the project will operate under the jurisdiction of the SCDHS and in conformance with SCSC Article 6, thereby assuring that no impact to underlying groundwater quality will occur.

- (3) *SCSC Articles 7 & 12 compliance. All development projects must comply with the provisions of Articles 7 and 12 of the SCSC, including any provisions for variances or waivers if needed, and all applicable state laws and regulations in order to ensure that all necessary water resource and wastewater management infrastructure shall be in place prior to, or as part of, the commencement of construction.*

These regulations concern water pollution control and storage of hazardous or toxic materials. The proposed project is consistent with SCSC Article 7 in that it will not involve an industrial process, or store or use hazardous or toxic materials in excess of the quantities allowed under Article 7 of the SCSC. SCSC Article 12 (which regulates toxic and/or hazardous materials storage and handling facilities) is also applicable. All tank storage for the fueling station will conform with Article 12 of the SCSC. The sump for each fuel storage tank will be equipped with a mechanical line leak detector and all other Article 12 requirements will be met for the tank installation.

B. Wellhead protection

Significant discharges and public supply well locations. The location of nearby public supply wells shall be considered in all applications involving significant discharges to groundwater, as required under the NYS ECL Article 17.

This standard restricts activities that could degrade the public water supply within a 200-foot radius of a public supply well. However, the subject site is not within 200 feet of any public water supply wellfield, there are no public water supply wellfields between the subject site and the Carmans River to the west of the site, and the anticipated 150 gpd of treated sanitary wastewater to be recharged from the proposed project does not represent any “significant discharge” such that it would have the potential to impact public water supply. The nearest SCWA well field is the Sills Road well field which is approximately 7,250 feet (1.37 miles) northwest of the site which is upgradient from the site and therefore recharge at the site would not impact the water supply for that well field. The potential for impacts to groundwater quality from the fuel storage tanks is minimized by equipping each such tank with a mechanical line leak detector, so that timely notice would be provided to the facility operator to immediately implement the appropriate and required remediation procedures. Conformance with Article 12 of the SCSC for tank storage ensures that no discharge will occur.

C. Wetlands and surface waters

- (1) *Nondisturbance buffers. Development proposals for sites containing or abutting freshwater or tidal wetlands or surface waters must be separated by a nondisturbance buffer area which shall be no less than that required by the New York State Tidal Wetland, Freshwater Wetland, or Wild, Scenic and Recreational Rivers Act and/or Chapter 81 of the Town of Brookhaven Code, Wetlands and Waterways. local ordinance. Distances shall be measured horizontally from the wetland edge as mapped by the NYSDEC and the Town of Brookhaven. Projects which require variances or exceptions from these laws and associated regulations, shall meet all requirements imposed in a permit by the NYSDEC and the Town of Brookhaven in order to be deemed to have met the requirements of this standard.*

Not applicable; there are no wetlands on or adjacent to the subject site (the nearest mapped regulated wetland is approximately 2,708 feet to the north within the cloverleaf of the Long Island Expressway).

- (2) *Buffer delineations, covenants and conservation easements. Buffer areas shall be delineated on the site plan or subdivision map, and covenants and/or conservation easements, pursuant to the NYS ECL and Chapter 81 of the Town of Brookhaven Code, wetlands and waterways, shall be imposed to protect these areas as deemed necessary.*

An appropriate instrument will be provided to ensure protection of areas of the subject site not intended to be developed. Note that tax lot 41.1, which is contiguous to the south, is owned by the same entity (Franklin Johnson, Inc.) that owns the subject site. However, tax lot 41.1 is not a part of the subject site, so that it would not be included in any Condition applied to the subject site by the Town Board in association with an approval for the proposed project. It is suggested that the site plan approval provides a binding assurance that the areas of the site intended to remain natural, will remain.

- (3) *Wild, Scenic & Recreational Rivers (WSRR) Act compliance. Development proposals shall conform to the provisions of the WSRR Act, where applicable. Projects which require variances or exceptions under the NYS WSRR Act shall meet all requirements imposed by the NYSDEC in order to be deemed to have met the requirements of this standard.*

Not applicable; the subject site is not within the area regulated under the WSRR Act of the Carmans River, and is not subject to regulation under the WSRR Act.

D. Stormwater runoff/stormwater recharge

Development projects must provide that all stormwater runoff originating from development on the property is recharged on site unless surplus capacity exists in an off-site drainage system.

This standard requires that adequate drainage capacity be provided for retention and recharge of stormwater runoff generated on-site. All stormwater runoff generated on developed project surfaces will be retained on-site and recharged to groundwater in a drainage system designed in conformance with Town requirements. No runoff from developed surfaces will be allowed to exit the site, based on the stringent retention and design requirements of the Town. The project's drainage system will be subject to the review and approval of the Town engineering and planning staff and the project will comply with SPDES GP 0-20-001 for stormwater project notification and preparation of a SWPPP as applicable. The proposed stormwater design conforms to the intent of this standard.

E. Natural vegetation and plant habitat

(1) *Vegetation clearance limits. Clearing is defined, for the purposes of this standard, as the removal of any portion of the natural vegetation found on a site, exclusive of any vegetation associated with active agricultural or horticultural activity or formalized landscape and turf areas. Excessive clearing of natural vegetation can result in severe soil erosion, excessive stormwater runoff, and the destruction or reduction of Pine Barrens plant and wildlife habitat.*

(a) *The clearance of natural vegetation shall be strictly limited. Site plans, surveys and subdivision maps shall delineate the existing naturally vegetated areas and calculate those portions of the site that are already cleared due to previous activities.*

(b) *Areas of the site proposed to be cleared combined with previously cleared areas shall not exceed the percentages in Figure 5-1. These percentages shall be taken over the total site and shall include, but not be limited to, roads, building sites and drainage structures. The clearance standard that would be applied to a project site if developed under the existing residential zoning category shall be applied if the proposal involves multi-family units, planned retirement units, attached housing, or clustering. Residential development within residentially zoned areas shall comply with the residential clearing limit categories contained in Figure 5-1. Commercial development in residentially zoned areas shall comply with "Commercial, Industrial, and Other or Mixed Use" clearing limit category. Site plans, surveys and subdivision maps shall delineate the clearing limit line and calculations for clearing to demonstrate compliance with this standard.*

(c) *To the extent that a portion of a site includes Core property, and for the purpose of calculating the clearance limits, the site shall be construed to be the combined Core*

and CGA portions. However, the Core portion may not be cleared except in accordance with Section 5.2 of the Plan.

The subject site is zoned in a mix of the A-1 (0.24 acres) and J-2 (1.14 acres) districts, for which Figure 5-1 the CLUP lists the maximum allowed amount of clearing for development (53% for A-1 and 65% for J-2). For the overall 1.38 acre site, applying these limits yields an overall allowed maximum clearing of 0.87 acres, or 63% of the site. However, the subject site is already developed (the Certificate of Occupancy for the existing convenience store is dated in 1977, and Planning Board approval for the site plan is dated in 1986; in contrast, the Long Island Pine Barrens Protection Act, under which the CLUP was established, was enacted in 1993), and only a small amount of natural vegetation remains on the site (estimated to be 1,000 SF, or 0.02 acres); the proposed project will occupy all of the previously-developed area and will retain the natural vegetation that currently exists on the site. Overall, 1.36 acres (98.6%) of the site will be utilized for the proposed project and all of this clearing will occur on existing developed surfaces (e.g., the building, paved surfaces and landscaping). No clearing of the 1,000 SF of natural vegetation will occur.

(2) *Unfragmented open space*

- (a) Subdivision and site design shall support the preservation of natural vegetation in large unbroken blocks that allow contiguous open spaces to be established when adjacent parcels are developed. Subdivision and site plan designs should also be configured in such a way so as to prioritize the preservation of native pine barrens vegetation to the maximum extent practicable.*
- (b) For the purpose of Sections 85-718 through 85-726, native pine barrens vegetation shall include pitch pines and various species of oak trees, understory and ground cover plants such as blueberry, wintergreen, bearberry and bracken fern, grasses and sedges such as little bluestem, Pennsylvania sedge and indian grass as well as those ecological communities listed in sections 5.6 and 5.7 in Chapter 5, Volume 2 of the Plan.*
- (c) It is recognized that the preservation of nonnative but ecologically important habitats may be consistent with the intent and goals of the plan when such action would result in the creation of large contiguous natural open space areas and the protection of rare, threatened or endangered species or their habitat.*

This standard concerns preservation of natural vegetation in large unbroken blocks to establish open spaces contiguous to on-site and, if possible, off-site property. However, the subject site is already developed, and only a small (1,000 SF) amount of natural vegetation exists on the site; the proposed project will occupy all of the previously developed area, and retain this natural vegetation. All existing natural vegetation on the site will remain and all existing disturbed areas of the site will be used for the re-development. There is no ability to align natural vegetation in any other manner than what currently exists.

- (3) *Fertilizer dependent vegetation limit. No more than 15% of an entire development project site shall be established in fertilizer-dependent vegetation including formalized turf areas. Generally, nonnative species require fertilization; therefore, planting of such nonnative species shall be limited to the maximum extent practicable. The use of the nonnative plants in Figure 5-2 [of the Plan] is specifically not recommended.*

The proposed project will comply with this standard. The site landscape architect indicates that no portion of the site will include fertilized vegetation. This Standard would allow up to 15% of the site, or 8,993 SF to be fertilized. Since none of the site is planned to be fertilized, this Standard is met. Final site plans, site plan review and Planning Board approval will ensure that no more than 15% of the site (i.e., less than 8,993 SF) is established in fertilizer-dependent vegetation.

No nonnative plant species listed in Figure 5-2 (Planting Recommendations) of the CLUP will be used as part of the final site plan landscape design plans.

- (4) *Native Plantings. Development projects shall consider the native planting suggestions contained in Figure 5-2 [of the Plan].*

Native plant species listed in Figure 5-2 (Planting Recommendations) of the CLUP will be used as part of the final site plan landscape design plans.

F. Species and communities of special concern

Where a significant adverse impact upon a habitat essential to those species identified on the New York State maintained lists as rare, threatened, endangered or of special concern, or upon natural communities classified by the New York State Natural Heritage Program as G1, G2, G3 or S1, S2 or S3, or on any federally listed endangered or threatened species is proposed, the appropriate mitigation measures as determined by the appropriate state, county or local government agency shall be taken to protect these species.

The subject site is a 1.38 acre developed commercial property (a 7-11 convenience store), and only 1,000 SF of natural vegetation remains on the property. The site now is developed with an existing 7-11 and only a small amount of natural vegetation is present. The site is bordered by three roads and the area surrounding the site is developed in a mix of transportation, commercial and residential uses. The existing disturbed areas will be used for the redevelopment and the existing natural vegetation on the site will remain. There are no known rare, threatened or endangered species or communities on or in the vicinity of the subject site that would be impacted by the proposed project. As a result, no impacts are expected with respect to special species and/or ecological communities.

G. Coordinated design for open space management

All applications must specify the entity/agency to which dedicated open space will be transferred.

The proposed project does not include any provision for open space dedications. It is suggested that the site plan approval provides a binding assurance that the existing 1,000 SF of natural vegetation on the site that is intended to remain natural, will remain.

H. Commercial and industrial development

All commercial and industrial development applications shall comply with the provisions of the SCSC as applied by the SCDHS, and all other applicable federal, state or local laws. Projects which require variances from the provisions of the SCSC shall meet all requirements of the SCDHS's Board of Review in order to be deemed to have met the requirements of this standard.

Like the existing 7-11 convenience store, the proposed project will obtain all necessary and required permits and approvals from the State, County or Town entities having jurisdiction.

SECTION 6.0

CARMANS RIVER CONSERVATION AND MANAGEMENT PLAN CONFORMANCE ANALYSIS

6.0 CARMANS RIVER CONSERVATION AND MANAGEMENT PLAN CONFORMANCE ANALYSIS

6.1 Introduction

The Carmans River is one of Long Island's four major river systems and is one of the two major rivers that flow through the Central Pine Barrens Region of Long Island. The Carmans River was designated a New York State Wild, Scenic, and Recreational River in 1972 and is a New York State designated Significant Coastal Fish and Wildlife Habitat. Portions of the Carmans River and its watershed are situated within the Long Island Central Pines Barrens Core Preservation Area (CPA) and the Long Island Central Pine Barrens Compatible Growth Area (CGA).

An estimated 94% of the flow of the Carmans River originates from groundwater, which means that the groundwater contributing area for the river is integral to the health and quality of the river. Thus, land uses overlying the river's groundwater contributing areas are of particular concern to the quality of surface waters in the Carmans River. Data had suggested that the water quality and aquatic habitats in the Carmans River were experiencing degradation over several decades. There was concern that the development and redevelopment of the land in the Carmans River watershed would cause further degradation of the river's water quality and aquatic habitats, which is the focus the Carmans River Conservation and Management Plan (hereafter, "the Carmans River Plan"). The study area for the Carmans River Plan includes all areas within the 100 year groundwater contributing area, which is those areas where groundwater would reach and outflow into the river within a 100-year period of its being recharged as stormwater. The study area includes 23,000 acres of land (see **Figure 6-1**).

The Carmans River Plan was completed in 2013, and its recommendation to expand the CGA boundary has been implemented.

The goals of the Plan include:

1. Environmentally sensitive lands that are critical to the ecological health and water quality of the Carmans River, or are significant habitats within the Carmans River watershed, should be protected and preserved.
2. There should be no further degradation of water quality in the Carmans River, and a concerted effort should be made to reduce the concentrations of water quality contaminants from their present levels.
3. Any new development or redevelopment in the Carmans River watershed should not adversely impact water quality in the Carmans River or the key ecological communities in the Carmans River watershed.
4. Degraded habitats should be restored, stormwater discharges mitigated, and the abundance of invasive species significantly reduced and, if possible, eliminated.

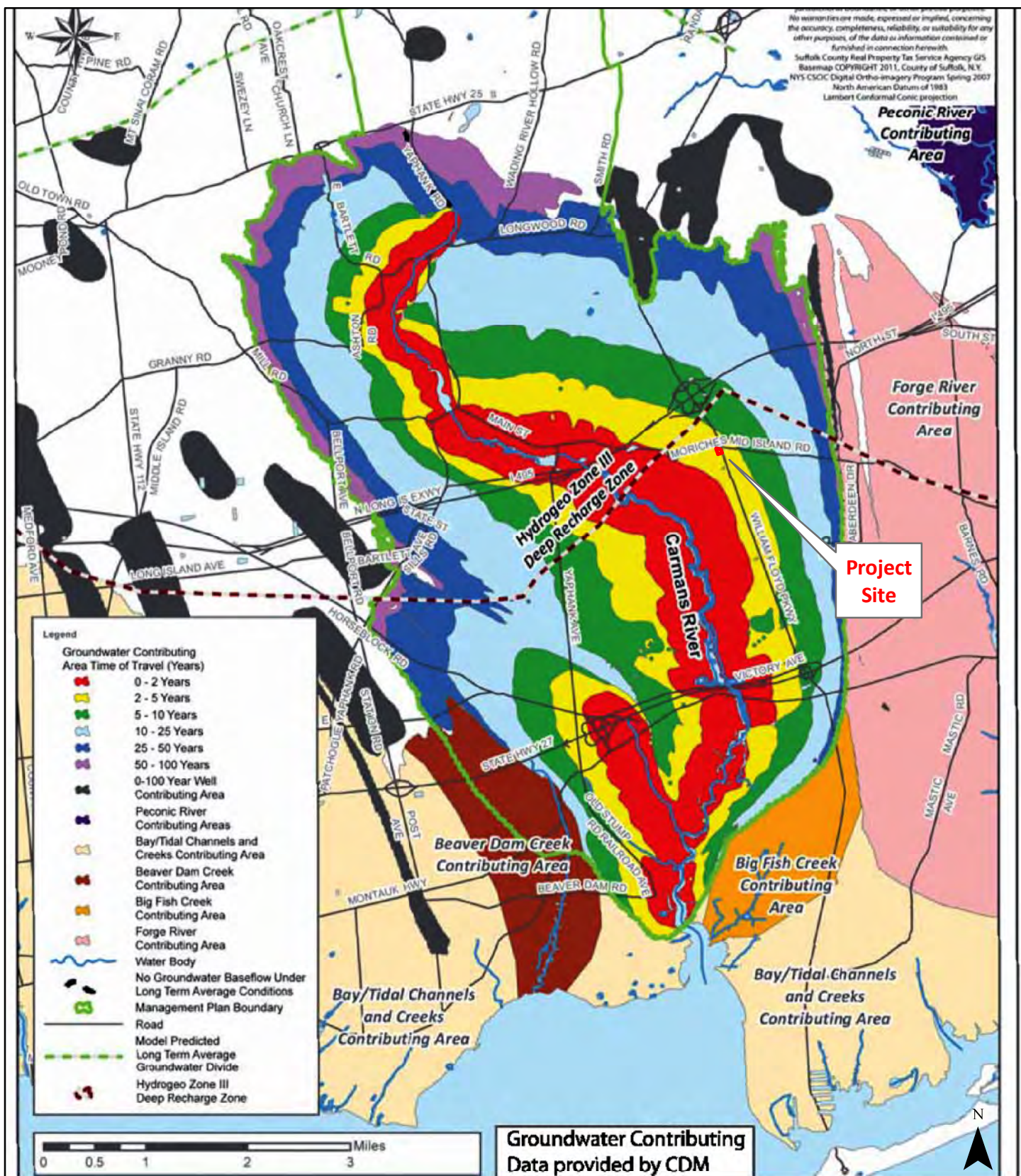


FIGURE 6-1
TRAVEL TIMES TO CARMANS RIVER

Source: Carmans River Conser. & Mgmt. Plan, 2013
Scale: 1 inch = 7,000 feet

7-11
Shirley

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Assessment

5. Environmental stewardship, outreach and education should be promoted by the Town of Brookhaven.

A key component of the Carmans River Plan is adding selected properties to the Central Pine Barrens CPA and CGA.

6.2 Applicable Recommendations of the Carmans River Plan

As shown in **Figure 6-1**, the subject site is located within the 2-5 year groundwater contributing area of the Carmans River. The Carmans River Plan recommended that within the 0-2 and the 2-5 year groundwater contributing areas of the plan study area be added to the Central Pine Barrens area, and so would be protected under the Central Pine Barrens Comprehensive Land Use Plan (hereafter, the “CLUP”; see **Section 5.0** for an analysis of the project’s conformance to the CLUP). **Figure 6-2** shows the areas that the plan recommends be protected by the CLUP by expanding the boundaries of the CGA. Note that these areas have been added to the CGA by action of the Central Pine Barrens Joint Planning & Policy Commission, which administers the CLUP.

Figure 6-2 also shows the plan recommends that some undeveloped properties within the 0-2 and 2-5 year groundwater time of travel contributing area be added to the CPA. Presumably, because the subject site is in a predominantly-developed area, the Carmans River Plan recommends that it be added to the CGA and not the CPA. Note that the boundary of land subject to the CLUP runs along William Floyd Parkway, so that the project site is subject to the CLUP, while the existing Mobil gas station across the parkway and opposite the site, is also within the 2-5 year groundwater contour, but is not subject to regulation under the CLUP.

6.3 Consistency with the Applicable Recommendations

As recommended by the Carmans River Plan, the boundary of the Central Pine Barrens CGA has been expanded, so that the subject site and proposed project are subject to the CLUP standards and guidelines. The detailed evaluation in **Section 5.0** shows that the project conforms to the CLUP, and thereby is consistent with the recommendations and goals of the Carmans River Plan.

Additionally, a large focus of the Carmans River Plan is protecting groundwater. The proposed project will incorporate the following features that will achieve this goal:

- It is expected that fuel will be dispensed by individual drivers/consumers, but all fuel delivery and dispensing operations will be overseen by employees properly trained to ensure that the potential for spillage is minimized.
- In case of accidental spillage, the spill will be immediately isolated by employees trained in such operations, and clean-up started; the pertinent State, County and/or Town entities will

Proposed Pine Barrens Expansion over existing Pine Barrens

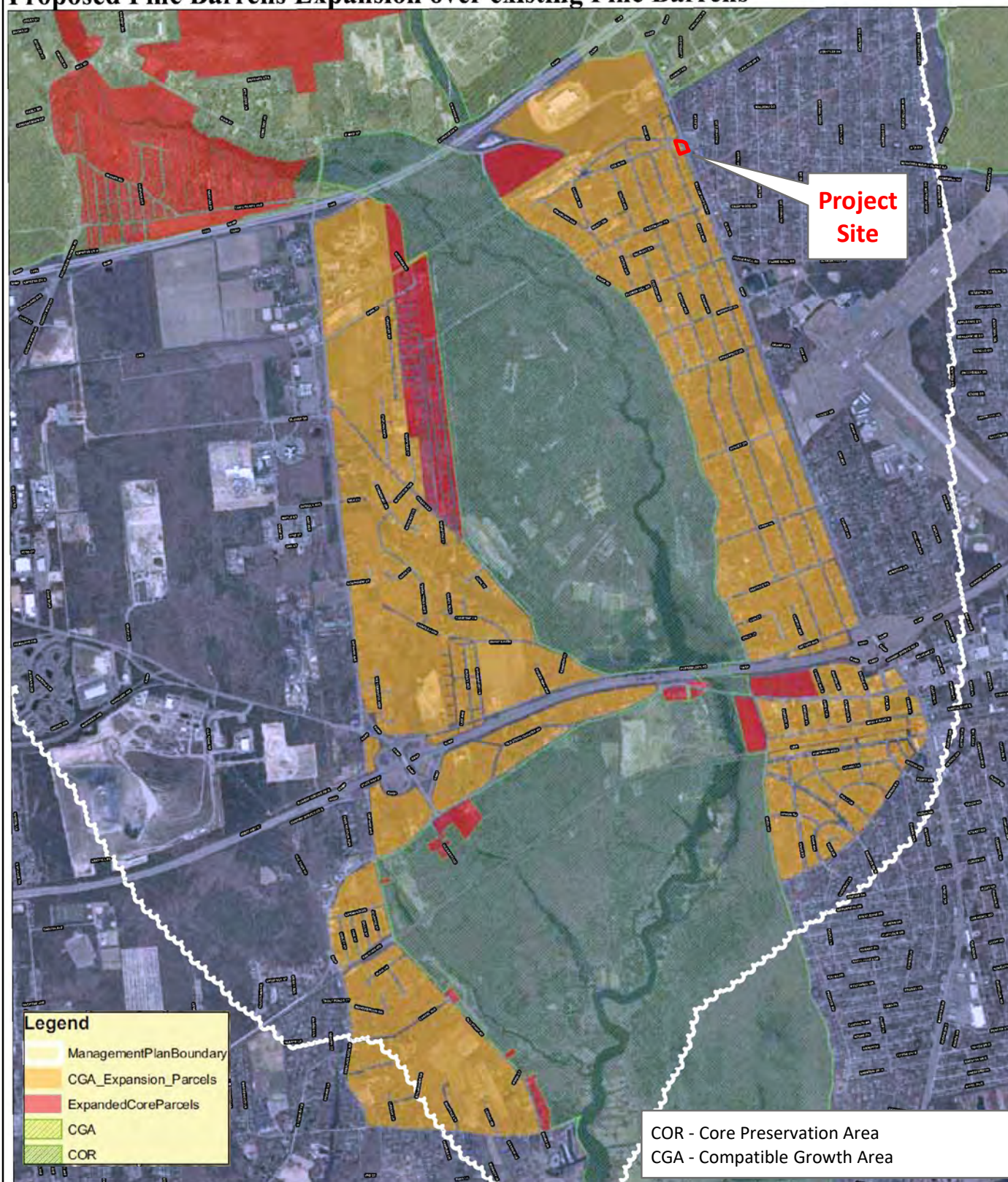


FIGURE 6-2

RECOMMENDED EXPANSION OF CGA

Source: Carmans River Conser. & Mgmt. Plan, 2013
Scale: 1 inch = 3,250 feet

7-11
Shirley

Environmental Planning
Assessment



NPV

be notified to ensure that the emergency actions have been properly conducted and the incident is inspected, reviewed, and closed by the entities having jurisdiction.

- The fuel storage tanks will be installed in a bed of pea gravel and will be equipped with sensors to detect the occurrence of tank leakage.
- The project will obtain all necessary and/or required permits and approvals related to the fuel dispensing and storage tank facilities from the State, County or Town entities. Importantly, the proposed project will be designed in accordance with Article 12 of the SCSC and will obtain approval of the SCDHS for the installation.
- The project will utilize a Town-reviewed and SCDHS- approved septic system to treat and recharge all sanitary wastewater generated on the site.
- The project will include a Town-reviewed and approved on-site drainage system that will retain all stormwater runoff generated on the site's developed surfaces and recharge it on the site, using subsurface leaching pools.
- The project will not include the use of landscape fertilizers, beyond an initial application, to ensure healthy growing conditions.

The subject site is currently developed with a 7-11 store and this use will remain unchanged except that fueling facilities will be added. The redevelopment of the site will all be within existing disturbed areas, so that there is a minimal change in the developed nature of the site. It is noted that the site is bordered on three sides by existing roads and the areas proximate to the site include existing transportation, commercial and residential development. To the extent applicable for an existing developed site within a developed area, the proposed project will conform with the Carmans River Conservation and Management Plan.

SECTION 7.0

SUMMARY AND CONCLUSION

7.0 SUMMARY AND CONCLUSION

This Environmental Planning Analysis addresses the proposed change of zone, special permit, variance, and waivers associated with the redevelopment of an existing 7-11 use, to add a fueling station. The analysis is based on the current site design plan included in a pouch at the end of the document, and the applicable zoning requirements/variances, Special Permit requirements, Central Pine Barrens District Standards and presence of the site within the area addressed by the Carmans River Conservation and Management Plan. This document is intended to provide the Town Board with additional information to render an informed decision on the application.

The following findings are identified in connection with this analysis:

- The site is currently developed with a 7-11 store.
- The use, 7-11, seeks to maximize success of the operation and meet market conditions and consumer demand by providing fueling facilities on the site.
- The project is consistent with the parameters for a Special Permit for the intended use.
- The variance criteria have been analyzed and the proposed project conforms with each of the provisions.
- The proposed project is consistent with the Central Pine Barrens District Standards per Chapter 85 of the Town Code.
- To the extent applicable for an existing developed site within a developed area, the proposed project will conform with the Carmans River Conservation and Management Plan.
- Given the analysis contained herein, and the project's conformance with the various zoning and land use provisions, there is a sound basis for the Town Board to grant the requested change of zone to facilitate the proposed redevelopment of the site.

It is requested that the Town Board and Planning staff of the Town review this Environmental Planning Analysis and consider approval of the requested change of zone and special permit. This analysis also provides a basis for continuing review by the Planning Board in connection with the site plan once Town Board action is taken.

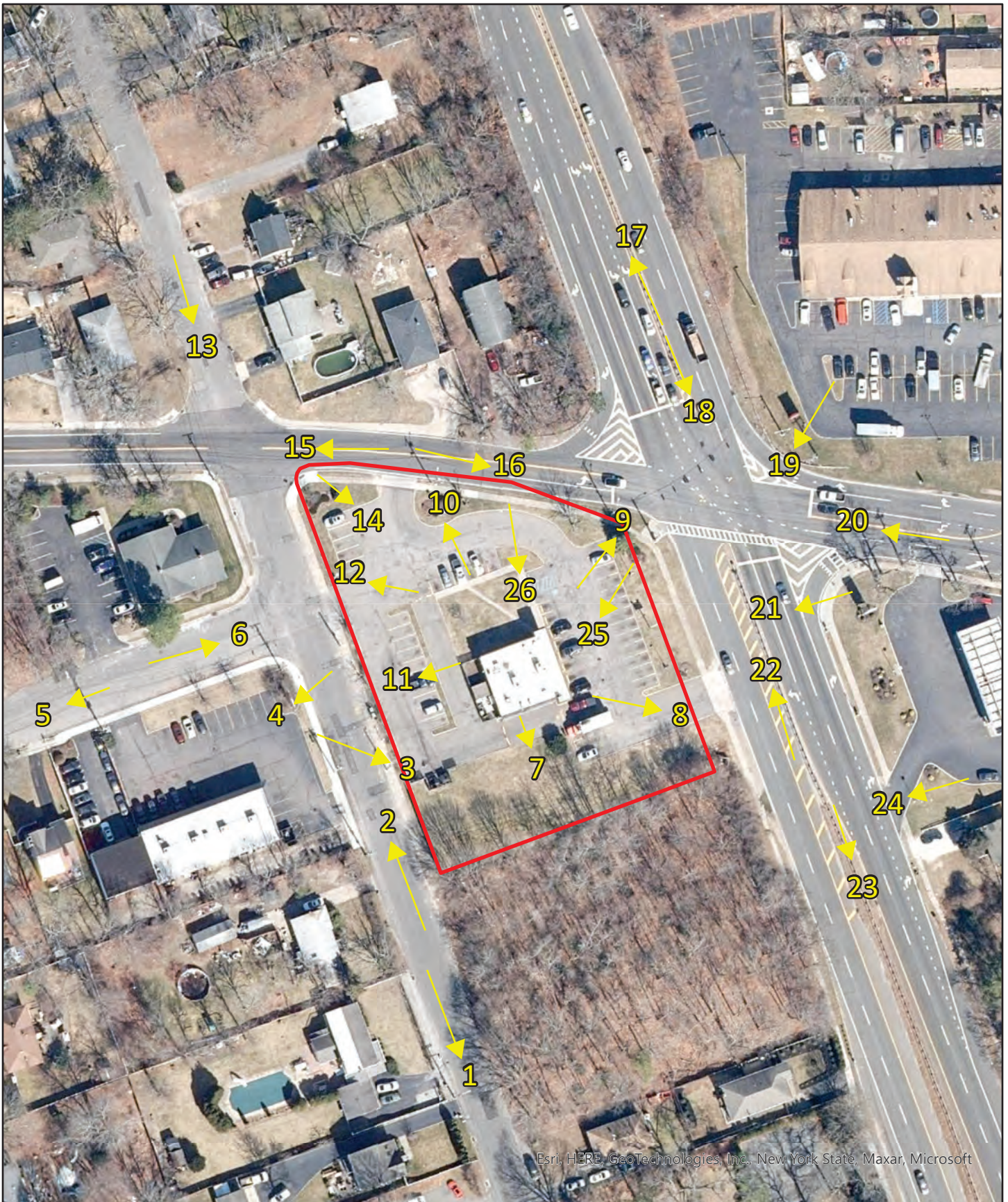
APPENDICES

APPENDIX A

SITE AND AREA PHOTOGRAPHS

NPV, LLC

Taken August 1, 2020



Esri, HERE, GeoTechnologies, Inc., New York State, Maxar, Microsoft



PHOTO LOCATION MAP

Sources: ESRI WMS; Suffolk County 2016
Scale: 1 inch equals 100 feet

7-11
Shirley

Environmental Planning
Assessment

Proposed Redevelopment of 7-11 Convenience Store, with Fuel Sales
Change of Zone and Special Permit Application, Shirley
Environmental Planning Assessment
Photographs Taken August 1st 2022



Photo 1: View looking south on Revilo Ave.



Photo 2: View looking north on Revilo Ave.

Proposed Redevelopment of 7-11 Convenience Store, with Fuel Sales
Change of Zone and Special Permit Application, Shirley
Environmental Planning Assessment
Photographs Taken August 1st 2022



Photo 3: Southwest corner of the property, wooded southerly portion of site



Photo 4: View looking southwest at the strip center across Revilo Ave.

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Photo 5: View looking west on Colin Dr.



Photo 6: View looking east on Colin Dr, Revilo Ave. access to site in center

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Photo 7: View looking at the southern portion of the subject site



Photo 8: View looking east at the Mobile gas station across William Floyd Pkwy

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Photo 9: View looking northeast at the strip center across William Floyd Pkwy



Photo 10: View looking north across Moriches Middle Island Rd. towards residences

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Photo 11: View southwest across Revilo Ave.



Photo 12: View looking northwest at the office building across Revilo Ave.

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Photo 13: View looking South on Revilo Ave. (subject site on the left)



Photo 14: View of the subject site from the northwest corner of the property

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Photo 15: View looking west on Moriches Middle Island Rd. (from northwest corner of the site)



Photo 16: View looking east on Moriches Middle Island Rd. (from northwest corner of the site)

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Photo 17: View looking north on William Floyd Pkwy (from north side of Moriches Middle Island Road)



Photo 18: View looking south on William Floyd Pkwy (From north side of Moriches Middle Island Road)

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Photo 19: View looking at the subject site from the strip center across William Floyd Pkwy



Photo 20: View looking west on Moriches Middle Island Rd. (from east side of William Floyd Pkwy)

Proposed Redevelopment of 7-11 Convenience Store, with Fuel Sales
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Photo 21: View of subject site from mobil gas station



Photo 22: View looking north on William Floyd Pkwy (south of Moriches Middle Island Rd.)

Proposed Redevelopment of 7-11 Convenience Store, with Fuel Sales
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Photo 23: View looking south on William Floyd Pkwy (south of Moriches Middle Island Rd.)



Photo 24: View of vacant lot 41.1 from the east

Proposed Redevelopment of 7-11 Convenience Store, with Fuel Sales
Change of Zone and Special Permit Application, Shirley
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Photo 25: Subject site (from east end of site)



Photo 26: Subject site (from north end of site)

APPENDIX B

TRAFFIC IMPACT STUDY

Stonefield Engineering & Design
Revised September 28, 2022



TRAFFIC IMPACT STUDY

7-ELEVEN CONVENIENCE STORE WITH FUEL SALES

7-Eleven with Fuel Sales
1481 William Floyd Parkway
Hamlet of Shirley, Town of
Brookhaven, Suffolk County,
New York

Prepared For:
7-Eleven, Inc.

Date: September 3, 2021

Revised: September 13, 2022

SE&D Job No. K-18112



Joshua Herman, PE
Project Manager
NY P.E. License #100672



Matthew J. Seckler PE, PP, PTOE
Principal
NY P.E. License #092725

STONEFIELD

584 Broadway, Suite 310, New York, NY 10012

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Intersection of Moriches-Middle Island Road and Revilo Avenue

Intersection of Moriches-Middle Island Road and Site Driveway

Intersection of Revilo Avenue and Site Driveway

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2018 William Floyd Parkway Counts – 563 ft South of Moriches-Middle Island Road

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EXECUTIVE SUMMARY

This Traffic Impact Study was prepared to investigate the potential impacts of the proposed 7-Eleven convenience store with fuel sales redevelopment on Section 642, Block 3, Lots 41.3, 41.4, and 41.5 in the Hamlet of Shirley, Town of Brookhaven, Suffolk County, New York. The following is a summary of the revisions to this report and the analysis findings contained herein:

1. This report has been updated per the revised Site Plan prepared by Stonefield, dated September 13, 2022.
2. The revisions to the site plan include the prohibition of the left-turn movement entering the subject site from westbound Moriches-Middle Island Road via the existing full-movement driveway. The proposed driveway along Moriches-Middle Island Road would be modified and improved as one (1) right-in/left-out/right-out driveway.
3. Existing and future site-generated vehicle trips entering the subject site via the left-turn movement from westbound Moriches-Middle Island Road have been rerouted to utilize the other two (2) site driveways.
4. The trip generation projections for the proposed development have been revised to utilize Institute of Transportation Engineers' (ITE) Trip Generation Manual, 11th Edition.
5. The revised analysis findings, which have been based on industry-standard guidelines, indicate that the proposed 7-Eleven convenience store with 16 vehicle fueling positions would not have a significant impact on the traffic operations of the adjacent roadway network. The study intersections and site driveway are calculated to operate at acceptable Levels of Service.

INTRODUCTION

This Traffic Impact Study was prepared to investigate the potential impacts of the proposed 7-Eleven convenience store with fuel sales on the adjacent roadway network. The subject property is located at the southwesterly quadrant of the intersection of William Floyd Parkway (County Road 46) and Moriches-Middle Island Road in the Hamlet of Shirley, Town of Brookhaven, Suffolk County, New York. The site location is shown on appended **Figure 1**.

The subject property is designated as Section 642, Block 3, Lots 41.3, 41.4, and 41.5 as depicted on the Suffolk County Tax Map. The site has approximately 200 feet of frontage along William Floyd Parkway, approximately 235 feet of frontage along Moriches-Middle Island Road, and approximately 300 feet of frontage along Revilo Avenue. The existing site is occupied by a 2,410-square-foot 7-Eleven convenience store. Access is presently provided via one (1) full-movement driveway along Moriches-Middle Island Road, one (1) full-movement driveway along Revilo Avenue, and one (1) right-in/right-out driveway along William Floyd Parkway.

Under the redevelopment program, the existing building would be razed and a 2,995-square-foot 7-Eleven convenience store with 16 vehicle fueling positions would be constructed. Access to the site will be generally maintained and improved via one (1) right-in/left-out/right-out driveway along Moriches-Middle Island Road, one (1) full-movement driveway along Revilo Avenue, and one (1) right-in/right-out driveway along William Floyd Parkway.

METHODOLOGY

Stonefield Engineering & Design, LLC has prepared this Traffic Impact Study in accordance with the recommended guidelines and practices outlined by the Institute of Transportation Engineers (ITE) within Transportation Impact Analyses for Site Development. A detailed field investigation was performed to assess the existing conditions of the adjacent roadway network. A data collection effort was completed to identify the existing traffic volumes at the study intersections to serve as a base for the traffic analyses. Capacity analysis, a procedure used to estimate the traffic-carrying ability of roadway facilities over a range of defined operating conditions, was performed using the Highway Capacity Manual, 6th Edition (HCM) and the Synchro II Software for all study conditions to assess the roadway operations.

For an unsignalized intersection, Level of Service (LOS) A indicates operations with delay of less than 10 seconds per vehicle, while LOS F describes operations with delay in excess of 50 seconds per vehicle. For a signalized intersection, LOS A indicates operations with delay of less than 10 seconds per vehicle, while LOS F describes operations with delay in excess of 80 seconds per vehicle. The Technical Appendix contains the Highway Capacity Analysis Detail Sheets for the study intersections analyzed in this assessment. The traffic

signal timing utilized within the signalized analysis is based on timing directives provided by Suffolk County Department of Public Works (SCDPW).

2021 EXISTING CONDITION

2021 EXISTING ROADWAY CONDITIONS

The proposed 7-Eleven convenience store with fuel sales is located at the southwesterly quadrant of the intersection of William Floyd Parkway (County Road 46) and Moriches-Middle Island Road in the Hamlet of Shirley, Town of Brookhaven, Suffolk County, New York. The subject property is designated as Section 642, Block 3, Lots 41.3, 41.4, and 41.5 as depicted on the Suffolk County Tax Map. The site has approximately 200 feet of frontage along William Floyd Parkway, approximately 235 feet of frontage along Moriches-Middle Island Road, and approximately 300 feet of frontage along Revilo Avenue. Land uses in the area are predominantly commercial and residential.

William Floyd Parkway (County Road 46) is classified as an urban principal arterial expressway with a general north-south orientation and is under the jurisdiction of SCDPW. Along the site frontage, the roadway is divided by a concrete median and provides two (2) lanes in each direction with additional turning lanes provided at key locations. The roadway has a posted speed limit of 45 mph along the site frontage. Along the site frontage, curb and sidewalk are provided along both sides of the roadway, shoulders are not provided along both sides of the roadway, and on-street parking is not regulated along either side of the roadway. William Floyd Parkway provides north-south mobility within the Town of Brookhaven for primarily residential and commercial uses along its length.

Moriches-Middle Island Road is classified as an urban minor arterial roadway with a general east-west orientation and is under the jurisdiction of the Town of Brookhaven. Along the site frontage, the roadway provides one (1) lane in each direction with additional turning lanes provided at key intersections. The roadway has a posted speed limit of 30 mph. Along the site frontage, curb and sidewalk are provided along both sides of the roadway, shoulders are not provided along both sides of the roadway, and on-street parking is not regulated along either side of the roadway. Moriches-Middle Island Road connects River Road to Montauk Highway (County Road 80) for primarily residential and commercial uses along its length.

Revilo Avenue is classified as a local roadway with a general north-south orientation and is under the jurisdiction of the Town of Brookhaven. Along the site frontage, the roadway provides one (1) lane in each direction and has a posted speed limit of 30 mph. Along the site frontage, curb and sidewalk are provided along both sides of the roadway, shoulders are not provided along either side of the roadway, and on-street

parking is not regulated along either side of the roadway. Revilo Avenue connects Moriches-Middle Island Road to Victory Avenue for primarily residential uses along its length.

Moriches-Middle Island Road and William Floyd Parkway intersect to form a signalized four (4)-leg intersection. The northbound approach of William Floyd Parkway provides one (1) exclusive left-turn lane, two (2) exclusive through lanes, and one (1) channelized right-turn lane operating under yield control. The southbound approach of William Floyd Parkway provides two (2) exclusive left-turn lanes, two (2) exclusive through lanes, and one (1) channelized right-turn lane operating under yield control. The eastbound approach of Moriches-Middle Island Road provides one (1) exclusive left-turn lane and one (1) shared through/right-turn lane and the westbound approach of Moriches-Middle Island Road provides one (1) exclusive left-turn lane, one (1) exclusive through lane, and one (1) channelized right-turn lane operating under yield control. Crosswalks and pedestrian signals with pushbuttons are provided across the southerly leg of the intersection.

Moriches-Middle Island Road and Revilo Avenue intersect to form an unsignalized four (4)-leg intersection with the northbound and southbound approaches of Revilo Avenue operating under stop control. The northbound and southbound approaches of Revilo Avenue provide one (1) shared left-turn/through/right-turn lane. The eastbound and westbound approaches of Moriches-Middle Island Road provide one (1) shared left-turn/through/right-turn lane. Crosswalks and pedestrian signals are not provided across any legs of the intersection.

2021 EXISTING TRAFFIC VOLUMES

Manual turning movement counts were collected during the typical weekday morning and weekday evening time periods to evaluate existing traffic conditions and identify the specific hours when traffic activity on the adjacent roadways is at a maximum and could be potentially impacted by the development of the site. Turning movement counts were collected at the following intersections:

- ◆ William Floyd Parkway and Moriches-Middle Island Road
- ◆ Moriches-Middle Island Road and Revilo Avenue
- ◆ Moriches-Middle Island Road and 7-Eleven Site Driveway
- ◆ Revilo Avenue and 7-Eleven Site Driveway
- ◆ William Floyd Parkway and 7-Eleven Site Driveway

Specifically, manual turning movement counts were conducted on Thursday, July 22, 2021, from 7:00 a.m. to 9:00 a.m. and from 4:00 p.m. to 7:00 p.m.

The study time periods were chosen as they are representative of the peak periods of both the adjacent roadway network and the proposed development. Please note that according to seasonal data published by New York State Department of Transportation (NYSDOT), summer months typically represent the greatest

traffic volumes along roadways near the water, such as William Floyd Parkway. As such, seasonal adjustment factors were not applied to the existing counts. The traffic volume data was collected and analyzed to identify the design peak hour in accordance with HCM and ITE guidelines. Based on the review of the count data the weekday morning peak hour occurred from 7:30 a.m. to 8:30 a.m. and the weekday evening peak hour occurred from 4:45 p.m. to 5:45 p.m. The Technical Appendix contains a summary of the turning movement count data. The 2021 Existing weekday morning and weekday evening peak-hour volumes are summarized on appended **Figure 2**.

The traffic count program also revealed the existing trip generation of the subject site. **Table I** summarizes the weekday morning and weekday evening peak hour trip generation volumes associated with the existing 2,410-square-foot 7-Eleven convenience store.

TABLE I – EXISTING SITE TRIP GENERATION

Land Use	Weekday Morning Peak Hour			Weekday Evening Peak Hour		
	Enter	Exit	Total	Enter	Exit	Total
2,410 SF 7-Eleven Convenience Store	116	116	232	90	81	171

2021 EXISTING ADJUSTED TRAFFIC VOLUMES

It is important to note that per the date of the traffic volume counts, existing traffic volumes currently observed on the roadway network may not be considered typical due to the COVID-19 pandemic. Therefore, in order to provide an accurate and conservative analysis of the adjacent roadway network, the as-counted traffic volumes observed during the study peak periods were compared to historical roadway data published by the NYSDOT from 2018. The comparison showed that the traffic volumes from the 2021 turning movement counts were generally greater than the historical counts and therefore no adjustments to the volumes were made.

2021 EXISTING LOS/CAPACITY ANALYSIS

A Level of Service and Volume/Capacity analysis was conducted for the 2021 Existing Condition during the weekday morning and weekday evening peak hours at the study intersections and existing site driveways. Under the existing condition, the signalized intersection of William Floyd Parkway & Moriches-Middle Island Road is calculated to operate at overall Level of Service C during the weekday morning and weekday evening peak hours. It is important to note that during the weekday evening peak hour, the northbound and southbound left-turn movements of William Floyd Parkway are calculated to operate with capacity constraints. This is consequence of the existing signal timing which provides less than 25% of the 90 second cycle to the

left-turn movements. Please note that this signal timing was established and is maintained by SCPDW for the purpose of promoting mobility along William Floyd Parkway.

The turning movements at the unsignalized intersection of Moriches-Middle Island Road and Revilo Avenue are calculated to operate at Level of Service B or better during the weekday morning and weekday evening peak hours. The turning movements at the unsignalized site driveways along William Floyd Parkway, Moriches-Middle Island Road, and Revilo Avenue are calculated to operate at Level of Service B or better during the study peak hours.

MOTOR VEHICLE COLLISION ANALYSIS

In order to assess the safety of the study network, the three (3) most recent years of available motor vehicle collision data prior to COVID restrictions were obtained from the NYSDOT. The study time period spans from March 1, 2017 to February 28, 2020. **Table 2** provides a summary of the manner and severity of the motor vehicle collisions reported at the study intersection.

TABLE 2 – MOTOR VEHICLE COLLISION ANALYSIS SUMMARY

Intersection	Collision Type	Number of Collisions	Collisions Resulting in Injury	Collisions Resulting in Fatalities
William Floyd Parkway & Moriches-Middle Island Road	Rear End	59	14	0
	Overtaking	18	0	0
	Right	9	3	0
	Left Turn (Against Other Car)	6	2	0
	Collision With Fixed Object	4	0	0
	Sideswipe	3	0	0
	Left Turn (With Other Car)	2	2	0
	Right Turn (Against Other Car)	2	1	0
	Right Turn (With Other Car)	1	0	0
	Head On	1	0	0
	Total	105	22	0
Moriches-Middle Island Road & Revilo Avenue	Right Angle	2	0	0
	Right Turn (Against Other Car)	1	0	0
	Rear End	1	0	0
	Total	4	0	0
Moriches-Middle Island Road btw William Floyd Parkway & Revilo Avenue	Rear End	2	0	0
	Sideswipe	1	0	0
	Overtaking	1	0	0
	Total	4	0	0
Revilo Avenue & Colin Drive	Rear End	1	0	0
	Left Turn (Against Other Car)	1	0	0
	Total	2	0	0
Network Total		115	22	0

As shown in **Table 2**, a total of 115 collisions were reported over the study network over the 36-month period. It is important to note that zero (0) fatalities occurred as a result of the reported motor vehicle collisions at the study intersection. Based on a review of the data, 59 of the 105 reported collisions at the study intersection of William Floyd Parkway and Moriches-Middle Island Road were rear end collisions which is a common collision type for a signalized intersection. Collision rates at the study intersection are not anticipated to be adversely impacted by this redevelopment project.

2023 NO-BUILD CONDITION

BACKGROUND GROWTH

The 2021 Existing Condition traffic volume data was grown to a future horizon year of 2023, which is a conservative estimate for when the proposed 7-Eleven redevelopment with fuel sales is expected to be fully constructed. In accordance with industry guidelines, the existing traffic volumes at the study intersections were increased by 1.5% annually for two (2) years. The 1.5% background growth rate was obtained from the NYSDOT Traffic Monitoring Section for the adjacent roadways and Town of Brookhaven. The 1.5% background growth rate was conservatively chosen to account for the roadway growth and the other planned development described herein.

OTHER PLANNED DEVELOPMENT PROJECTS

To evaluate the future traffic conditions, it is important to consider the potential site-generated traffic of other projects that could influence the traffic volume at the study intersections. Other planned development projects include those that are either in the entitlement process or have recently been approved for building permits in proximity to the study development. Based on research through the Town of Brookhaven Application Logs the following development is anticipated to impact traffic volumes within the study area:

- ◆ Long Island Surf Park – outdoor surf pool with 9,000-square-foot clubhouse including café/bar, locker rooms, and surf shop located along Precision Drive approximately 1.0-mile north of the subject site

Please note that the traffic volumes associated with the Long Island Surf Park project are accounted for in the background growth rate used to compute the 2023 No-Build Traffic Volumes.

2023 NO-BUILD TRAFFIC VOLUMES

The background growth rate was applied to the 2021 Existing Traffic Volumes to calculate the 2023 No-Build Traffic Volumes for the weekday morning and weekday evening peak hours. These volumes are summarized on appended **Figure 3**.

2023 NO-BUILD LOS/CAPACITY ANALYSIS

A Level of Service and Volume/Capacity analysis was also conducted for the 2023 No-Build Condition during the weekday morning and weekday evening peak hours at the study intersections and existing site driveways. The signalized intersection of William Floyd Parkway & Moriches-Middle Island Road is calculated to operate generally consistent with the findings of the 2021 Existing Condition during the weekday morning peak hour. During the weekday evening peak hour, the signalized intersection is calculated to degrade to an overall Level of Service D but only exceeds the C-to-D threshold by 2.2 seconds. The northbound and southbound left-turn movements of William Floyd Parkway are calculated to continue to operate with capacity constraints in the 2023 No-Build Condition.

The turning movements at the unsignalized intersection of Moriches-Middle Island Road and Revilo Avenue and all existing site driveways are calculated to operate generally consistent with the findings of the 2021 Existing Condition during the weekday morning and weekday evening peak hours.

2023 BUILD CONDITION

The site-generated traffic volume of the redeveloped 7-Eleven with fuel sales was estimated to identify the potential impacts of the project. For the purpose of this analysis, a complete project “build out” is assumed within two (2) years of the preparation of this study.

TRIP GENERATION

Trip generation projections for the proposed 7-Eleven redevelopment with fuel sales were prepared utilizing the ITE’s Trip Generation Manual, 11th Edition. Trip generation rates associated with Land Use 944 “Gasoline/Service Station” were cited for the 585-square-foot convenience store expansion and new 16 vehicle fueling positions. It is important to note that the ITE’s description of Land Use 944 includes trips generated by ancillary convenience store offerings and therefore is the appropriate land use to capture all trips related to the expansion and new gasoline station. **Table 3** provides the weekday morning and weekday evening peak hour trip generation volumes associated with the proposed redevelopment.

TABLE 3 – PROJECTED TRIP GENERATION

Land Use	Weekday Morning Peak Hour			Weekday Evening Peak Hour		
	Enter	Exit	Total	Enter	Exit	Total
Existing 2,410 SF 7-Eleven Convenience Store	116	116	232	90	81	171
Proposed 585 SF 7-Eleven Expansion and Gasoline/Service Station <i>ITE Land Use 944</i>	48	47	95	58	57	115
Total Projected Trip Generation	164	163	327	148	138	286

It is important to note that the property is presently operating and generating traffic to the subject site, therefore, it is reasonable to analyze the net increase in vehicular trip generation to quantify the impact of the redevelopment. **Table 4** shows the net vehicular trip generation of the existing development and proposed redevelopment during the weekday morning and weekday evening peak hours.

TABLE 4 – NET TRIP GENERATION

Land Use	Weekday Morning Peak Hour			Weekday Evening Peak Hour		
	Enter	Exit	Total	Enter	Exit	Total
Existing 2,410 SF 7-Eleven Convenience Store	116	116	232	90	81	171
Proposed 2,995 SF 7-Eleven with Gasoline Station	164	163	327	148	138	286
Net Trips	+48	+47	+95	+58	+57	+115

As stated within Chapter 10 of ITE's Trip Generation Handbook, 3rd Edition, there are instances when the total number of trips generated by a site is different from the amount of new traffic added to the street system by the generator. Gasoline stations and convenience stores are specifically located on or adjacent to busy streets to attract motorists already on the roadway. Therefore, the redeveloped 7-Eleven with fuel sales would be expected to attract a portion of its trips from the existing traffic passing the site on the way from an origin to an ultimate destination. These trips do not add new traffic to the adjacent roadway system and are referred to as pass-by trips.

Based upon the published ITE data for Land Use 944 "Gasoline/Service Station," 62% of the site-generated traffic during the weekday morning and 56% during the weekday evening is comprised of pass-by traffic. **Table 5** shows the additional site generated traffic for the redevelopment after applying the appropriate trip reductions to account for pass-by traffic.

TABLE 5 – PROJECTED TRIP GENERATION – NEW & PASS-BY TRIPS

Land Use	Weekday Morning Peak Hour			Weekday Evening Peak Hour		
	Enter	Exit	Total	Enter	Exit	Total
“New” Trips	19	18	37	27	26	53
“Pass-By” Trips	29	29	58	31	31	62
Total	48	47	95	58	57	115

As shown in **Table 5**, 37 and 53 “new” trips, respectively, are generated in the weekday morning and weekday evening peak hours. Based on Transportation Impact Analysis for Site Development published by ITE, a trip increase of less than 100 vehicle trips would likely not change the level of service of the roadway system or appreciably increase the volume-to-capacity ratio of an intersection approach. As such, the 7-Eleven redevelopment is not anticipated to significantly impact the operations of the adjacent roadway network.

At the site driveways, the calculated number of pass-by trips is shown as a negative number at the through movement as the vehicles are temporarily diverted from the through travel stream into and out of the site access point.

TRIP ASSIGNMENT/DISTRIBUTION

The trips generated by the development were distributed according to the existing travel pattern along the adjacent roadways and the access management plan of the site. Please note that the existing full-movement driveway along Moriches-Middle Island Road would be improved as a right-in/left-out/right-out driveway as part of the proposed development. Therefore, existing left-turns into the subject site were rerouted to the full-movement driveway along Revilo Avenue, as shown on appended **Figure 4**. The “New” Site-Generated Traffic Volumes are illustrated on appended **Figure 5** and the “Pass-By” Site-Generated Traffic Volumes expected to access the site are depicted on appended **Figure 6**.

2023 BUILD TRAFFIC VOLUMES

The site-generated trips were added to the 2023 No-Build Traffic Volumes to calculate the 2023 Build Traffic Volumes and are shown on appended **Figure 7**.

2023 BUILD LOS/CAPACITY ANALYSIS

A Level of Service and Volume/Capacity analysis was also conducted for the 2023 Build Condition during the weekday morning and weekday evening peak hours at the study intersections and site driveways. **Tables 6 through 15** compare the 2021 Existing Condition, 2023 No-Build Condition, and 2023 Build Condition Level of Service and delay values.

Under the 2023 Build Condition, the study intersections and existing site driveways are calculated to operate generally consistent with the 2023 No-Build Condition during the weekday morning and weekday evening peak hours. Therefore, the proposed redevelopment is not anticipated to create a significant impact on the traffic operations of the adjacent roadway network. Please note that the calculated 95th percentile queue lengths at the site driveways would be accommodated in the proposed driveway throat length without adversely impacting on-site circulation or parking maneuverability during the study peak hours.

COMPARATIVE LEVEL OF SERVICE (DELAY) TABLES

WILLIAM FLOYD PARKWAY & MORICHES-MIDDLE ISLAND ROAD

EB (Eastbound) and WB (Westbound) approaches are the Moriches-Middle Island Road approaches

NB (Northbound) and SB (Southbound) approaches are the William Floyd Parkway approaches

X (n) = Level of Service (seconds of delay)

TABLE 6 – WEEKDAY MORNING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
EB Left	C (32.0)	C (32.6)	C (33.1)
EB Through/Right	C (28.3)	C (28.7)	C (28.7)
WB Left	C (33.2)	C (33.9)	C (34.0)
WB Through	C (27.4)	C (27.7)	C (27.7)
NB Left	D (48.0)	D (48.2)	D (47.5)
NB Through	C (28.9)	C (31.8)	C (32.3)
SB Left	D (35.6)	D (36.2)	D (36.5)
SB Through	B (16.6)	B (17.0)	B (17.5)
Intersection	C (26.9)	C (28.5)	C (28.9)

TABLE 7 – WEEKDAY EVENING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
EB Left	C (31.1)	C (31.5)	C (32.0)
EB Through/Right	C (29.9)	C (30.3)	C (30.3)
WB Left	C (37.1)	D (37.8)	D (38.1)
WB Through	C (27.8)	C (28.0)	C (28.1)
NB Left	E (56.5)	E (56.4)	E (55.6)
NB Through	C (29.3)	C (30.2)	C (30.5)
SB Left	E (63.7)	F (77.9)	F (79.8)
SB Through	C (21.1)	C (22.1)	C (22.7)
Intersection	C (33.8)	D (37.2)	D (37.8)

MORICHES-MIDDLE ISLAND ROAD & REVILO AVENUE

EB (Eastbound) and WB (Westbound) approaches are the Moriches-Middle Island Road approaches

NB (Northbound) and SB (Southbound) approaches are the Revilo Avenue approaches

X (n) = Level of Service (seconds of delay)

TABLE 8 – WEEKDAY MORNING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
EB Left	--	--	--
WB Left	A (7.5)	A (7.6)	A (7.7)
NB Left/Through/Right	A (9.4)	A (9.5)	A (9.8)
SB Left/Through/Right	B (11.5)	B (11.7)	B (14.1)

TABLE 9 – WEEKDAY EVENING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
EB Left	A (7.6)	A (7.6)	A (7.6)
WB Left	A (7.9)	A (7.9)	A (8.1)
NB Left/Through/Right	B (10.9)	B (11.0)	B (11.8)
SB Left/Through/Right	B (14.5)	B (14.9)	C (18.0)

MORICHES-MIDDLE ISLAND ROAD & SITE DRIVEWAY

WB (Westbound) approach is the Moriches-Middle Island Road approaches

NB (Northbound) approach is the site driveway approach

X (n) = Level of Service (seconds of delay)

TABLE 10 – WEEKDAY MORNING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
WB Left	A (7.7)	A (7.7)	--
NB Left/Right	A (9.9)	A (9.9)	B (10.0)

TABLE 11 – WEEKDAY EVENING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
WB Left	A (7.9)	A (7.9)	--
NB Left/Right	B (10.3)	B (10.4)	B (10.8)

REVILO AVENUE & SITE DRIVEWAY/COLIN DRIVE

NB (Northbound) and SB (Southbound) approaches are the Revilo Avenue approaches

EB (Eastbound) approach is the Colin Drive approach

WB (Westbound) approach is the site driveway approach

X (n) = Level of Service (seconds of delay)

TABLE 12 – WEEKDAY MORNING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
EB Left/Through/Right	A (9.5)	A (9.6)	B (11.2)
WB Left/Through/Right	A (9.3)	A (9.3)	B (10.5)
NB Left	A (7.3)	A (7.3)	A (7.3)
SB Left	A (7.4)	A (7.4)	A (7.5)

TABLE 13 – WEEKDAY EVENING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
EB Left/Through/Right	A (9.7)	A (9.7)	B (11.1)
WB Left/Through/Right	A (9.6)	A (9.6)	B (10.9)
NB Left	A (7.4)	A (7.4)	A (7.4)
SB Left	A (7.3)	A (7.3)	A (7.5)

WILLIAM FLOYD PARKWAY & SITE DRIVEWAY

EB (Eastbound) approach is the site driveway approach

X (n) = Level of Service (seconds of delay)

TABLE 14 – WEEKDAY MORNING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
EB Right	B (11.7)	B (11.8)	B (12.1)

TABLE 15 – WEEKDAY EVENING PEAK HOUR

Lane Group	2021 Existing	2023 No-Build	2023 Build
EB Right	B (14.8)	C (15.1)	C (15.9)

SITE CIRCULATION/PARKING SUPPLY

A review was conducted of the proposed redevelopment using the Site Plan prepared by Stonefield, last revised on September 13, 2022. In completing this review, particular attention was focused on the site access, circulation, and parking supply.

Access to the site will be generally maintained and improved via one (1) right-in/left-out/right-out driveway along Moriches-Middle Island Road, one (1) full-movement driveway along Revilo Avenue, and one (1) right-in/right-out driveway along William Floyd Parkway. The 7-Eleven convenience store will be constructed in the southwestern corner of the site and the fueling canopy will be constructed in the center of the site. Parking

will be provided along the northern façade of the building, in the northern portion of the site and in the southeastern corner of the site. Two (2)-way site circulation will be provided via a minimum of 30-foot wide drive aisles. The trash enclosure will be located along the western façade of the building, accessible via a 24-foot wide drive aisle.

Regarding the parking requirements for the proposed development, the Town of Brookhaven Zoning Ordinance requires one (1) parking space per 100 square feet of convenience store space and a minimum of two (2) spaces for motor vehicle fueling stations. The site would provide 32 total parking spaces, inclusive of two (2) ADA accessible parking spaces and three (3) land banked spaces, which meets the parking requirement and would be sufficient to support this project's parking demand. The spaces would be a minimum of nine (9) feet wide by 19 feet deep in accordance with the Town of Brookhaven Zoning Ordinance and industry standards, with the exception of the two (2) ADA accessible parking spaces which would be eight (8) feet wide by 20 feet deep with an eight (8)-foot-wide access aisle in accordance with industry standards.

Additionally, it is widely recognized throughout the traffic engineering industry that vehicle fueling positions function as parking spaces for patrons of both gasoline and convenience items; patrons rarely move their vehicle from a fueling position to a striped parking space between gasoline and convenience store transactions. Therefore, 16 vehicle fueling positions are available to supplement the 32 striped parking spaces on the Site Plan, for a total of 48 vehicle positions.

CONCLUSIONS

This report was prepared to examine the potential traffic impact of the proposed 7-Eleven convenience store with fuel sales redevelopment. The analysis findings, which have been based on industry-standard guidelines, indicate that the proposed development would not have a significant impact on the traffic operations of the adjacent roadway network. The site driveways and on-site layout have been designed to provide for effective access to and from the subject property. The proposed parking supply meets the Town's requirements and would be sufficient to support this project.

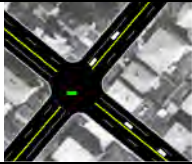
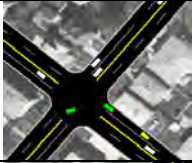




TECHNICAL APPENDIX

LEVEL OF SERVICE/AVERAGE CONTROL DELAY CRITERIA

LEVEL OF SERVICE /AVERAGE CONTROL DELAY CRITERIA

The ability of a roadway to effectively accommodate traffic demand is determined through an assessment of the volume-to-capacity ratio, delay and Level of Service of the lane group and/or intersection. The volume-to-capacity ratio is the ratio of traffic flow rate to capacity for a given transportation facility. As defined within the Highway Capacity Manual 2010 (HCM 2010), intersection delay is the total additional travel time experienced by drivers, passengers, or pedestrians as a result of control measures and interaction with other users of the facility, divided by the volume departing from the corresponding cross section of the facility. Level of service is a qualitative measure describing operational conditions within a traffic stream, based on service measures such as speed and travel time, freedom to maneuver, traffic interruptions, comfort and convenience.

For an unsignalized intersection, LOS A indicates operations with delay less than 10 seconds per vehicle, while LOS F describes operations with delay in excess of 50 seconds per vehicle. For a signalized intersection, LOS A indicates operations with delay less than 10 seconds per vehicle and LOS F denotes operations with delay in excess of 80 seconds per vehicle.

	Level Of Service (LOS)	Signalized Delay Range (average control delay in sec/veh)	Unsignalized Delay Range (average control delay in sec/veh)
	A	≤ 10	≤ 10
	B	> 10 and ≤ 20	> 10 and ≤ 15
	C	> 20 and ≤ 35	> 15 and ≤ 25
	D	> 35 and ≤ 55	> 25 and ≤ 35
	E	> 55 and ≤ 80	> 35 and ≤ 50
	F	> 80	> 50

Source: Highway Capacity Manual 2010

TURNING MOVEMENT COUNT DATA

National Data & Surveying Services

Intersection Turning Movement Count

Location: William Floyd Pkwy & Moriches Middle Island Rd
City: Shirley
Control: Signalized

Project ID: 21-370026-001
Date: 7/22/2021

Data - Total

NS/EW Streets:		William Floyd Pkwy				William Floyd Pkwy				Moriches Middle Island Rd				Moriches Middle Island Rd				
AM		NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
		1 NL	2 NT	1 NR	0 NU	2 SL	2 ST	1 SR	0 SU	1 EL	1 ET	0 ER	0 EU	1 WL	1 WT	1 WR	0 WU	
	7:00 AM	11	238	14	0	29	113	19	1	27	11	11	0	28	14	94	1	611
	7:15 AM	15	289	10	0	37	136	13	0	27	14	10	0	20	16	105	0	692
	7:30 AM	25	260	19	0	41	141	15	0	28	14	10	0	30	18	120	0	721
	7:45 AM	12	316	16	0	55	184	19	0	23	19	11	0	24	31	118	0	828
	8:00 AM	16	260	17	0	47	146	14	0	28	9	11	0	15	18	118	0	699
	8:15 AM	18	262	22	0	66	176	12	1	21	18	8	0	21	16	111	0	752
	8:30 AM	13	240	22	0	51	151	12	0	25	15	8	0	22	20	99	0	678
	8:45 AM	12	263	9	2	58	173	13	0	25	21	8	0	28	25	103	0	740
TOTAL VOLUMES :		NL 122	NT 2128	NR 129	NU 2	SL 384	ST 1220	SR 117	SU 2	EL 204	ET 121	ER 77	EU 0	WL 188	WT 158	WR 868	WU 1	TOTAL 5721
APPROACH %'s :		5.12%	89.37%	5.42%	0.08%	22.29%	70.81%	6.79%	0.12%	50.75%	30.10%	19.15%	0.00%	15.47%	13.00%	71.44%	0.08%	
PEAK HR :		07:30 AM - 08:30 AM																TOTAL
PEAK HR VOL :		71	1098	74	0	209	647	60	1	100	60	40	0	90	83	467	0	3000
PEAK HR FACTOR :		0.710	0.869	0.841	0.000	0.792	0.879	0.789	0.250	0.893	0.789	0.909	0.000	0.750	0.669	0.973	0.000	0.906
		0.903				0.889				0.943				0.925				

PM		NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
		1 NL	2 NT	1 NR	0 NU	2 SL	2 ST	1 SR	0 SU	1 EL	1 ET	0 ER	0 EU	1 WL	1 WT	1 WR	0 WU	
	4:00 PM	21	225	28	0	119	313	26	1	19	26	15	0	19	25	59	0	896
	4:15 PM	11	215	35	0	93	264	39	0	12	27	13	0	31	17	72	0	829
	4:30 PM	12	224	33	0	112	268	34	0	23	24	12	0	30	25	81	0	878
	4:45 PM	11	225	32	0	144	269	40	0	14	35	16	0	25	31	88	0	930
	5:00 PM	7	222	32	0	137	272	30	0	14	29	17	0	26	25	93	0	904
	5:15 PM	13	213	33	0	161	270	35	0	15	35	17	0	31	25	79	0	927
	5:30 PM	23	199	28	0	144	260	41	0	19	33	15	0	21	34	104	0	921
	5:45 PM	19	206	29	0	150	286	31	0	19	26	10	0	22	17	92	0	907
	6:00 PM	25	214	26	0	108	220	26	0	15	30	16	0	24	20	95	0	819
	6:15 PM	10	222	28	0	91	240	30	0	22	17	15	0	25	24	88	0	812
	6:30 PM	22	190	45	0	122	242	22	0	12	21	14	0	31	22	77	0	820
	6:45 PM	11	168	40	1	88	223	27	0	14	14	17	0	26	17	71	0	717
TOTAL VOLUMES :		NL 185	NT 2523	NR 389	NU 1	SL 1469	ST 3127	SR 381	SU 1	EL 198	ET 317	ER 177	EU 0	WL 311	WT 282	WR 999	WU 0	TOTAL 10360
APPROACH %'s :		5.97%	81.44%	12.56%	0.03%	29.51%	62.82%	7.65%	0.02%	28.61%	45.81%	25.58%	0.00%	19.54%	17.71%	62.75%	0.00%	
PEAK HR :		04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :		54	859	125	0	586	1071	146	0	62	132	65	0	103	115	364	0	3682
PEAK HR FACTOR :		0.587	0.954	0.947	0.000	0.910	0.984	0.890	0.000	0.816	0.943	0.956	0.000	0.831	0.846	0.875	0.000	0.990
		0.968				0.967				0.966				0.915				

National Data & Surveying Services

Intersection Turning Movement Count

Location: Revilo Ave & Moriches Middle Island Rd
City: Shirley
Control: 2-Way Stop(NB/SB)

Project ID: 21-370026-002
Date: 7/22/2021

Data - Total

NS/EW Streets:	Revilo Ave				Revilo Ave				Moriches Middle Island Rd				Moriches Middle Island Rd				
AM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	0 NL	1 NT	0 NR	0 NU	0 SL	1 ST	0 SR	0 SU	0 EL	1 ET	0 ER	0 EU	0 WL	1 WT	0 WR	0 WU	
7:00 AM	1	0	13	0	1	0	0	0	0	23	4	0	13	21	0	0	76
7:15 AM	3	0	16	0	0	0	0	0	0	24	2	0	7	24	0	0	76
7:30 AM	2	0	19	0	0	0	0	0	0	18	0	0	10	35	0	0	84
7:45 AM	2	0	16	0	0	0	0	0	0	25	3	0	10	30	0	0	86
8:00 AM	3	0	16	0	0	0	0	0	0	23	1	0	12	28	0	1	84
8:15 AM	1	0	12	0	1	0	0	0	0	23	1	0	10	22	0	0	70
8:30 AM	7	0	12	0	3	0	0	0	0	25	0	0	16	23	0	0	86
8:45 AM	1	0	14	0	0	0	0	0	0	27	0	0	13	28	1	0	84
TOTAL VOLUMES :	NL 20	NT 0	NR 118	NU 0	SL 5	ST 0	SR 0	SU 0	EL 0	ET 188	ER 11	EU 0	WL 91	WT 211	WR 1	WU 1	TOTAL 646
APPROACH %'s :	14.49%	0.00%	85.51%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	94.47%	5.53%	0.00%	29.93%	69.41%	0.33%	0.33%	
PEAK HR :	7:30 AM - 8:30 AM																TOTAL
PEAK HR VOL :	8	0	63	0	1	0	0	0	0	89	5	0	42	115	0	1	324
PEAK HR FACTOR :	0.667	0.000	0.829	0.000	0.250	0.000	0.000	0.000	0.000	0.890	0.417	0.000	0.875	0.821	0.000	0.250	0.942
	0.845				0.250				0.839				0.878				

PM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	0 NL	1 NT	0 NR	0 NU	0 SL	1 ST	0 SR	0 SU	0 EL	1 ET	0 ER	0 EU	0 WL	1 WT	0 WR	0 WU	
4:00 PM	4	0	13	0	0	2	0	0	0	38	4	0	17	42	1	1	122
4:15 PM	2	0	14	0	1	0	0	0	0	36	2	0	24	26	3	0	108
4:30 PM	2	0	10	0	1	0	0	0	0	30	4	0	26	32	0	1	106
4:45 PM	3	0	7	0	0	0	1	0	2	50	0	0	26	41	0	0	130
5:00 PM	3	0	19	0	0	0	0	0	0	41	3	0	20	31	0	0	117
5:15 PM	5	0	11	0	0	0	0	0	0	40	4	0	28	31	3	0	122
5:30 PM	0	0	13	0	3	0	0	0	0	46	0	0	30	54	3	0	149
5:45 PM	2	0	13	0	0	0	0	0	1	34	4	0	19	37	0	0	110
6:00 PM	2	1	14	0	1	0	0	0	0	37	5	0	21	39	0	0	120
6:15 PM	3	0	19	0	1	1	0	0	0	28	2	0	21	26	3	0	104
6:30 PM	2	0	11	0	0	0	0	0	0	30	0	0	23	33	0	0	99
6:45 PM	5	0	12	0	0	0	0	0	0	28	1	0	21	29	0	0	96
TOTAL VOLUMES :	NL 33	NT 1	NR 156	NU 0	SL 7	ST 3	SR 1	SU 0	EL 3	ET 438	ER 29	EU 0	WL 276	WT 421	WR 13	WU 2	TOTAL 1383
APPROACH %'s :	17.37%	0.53%	82.11%	0.00%	63.64%	27.27%	9.09%	0.00%	0.64%	93.19%	6.17%	0.00%	38.76%	59.13%	1.83%	0.28%	
PEAK HR :	04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :	11	0	50	0	3	0	1	0	2	177	7	0	104	157	6	0	518
PEAK HR FACTOR :	0.550	0.000	0.658	0.000	0.250	0.000	0.250	0.000	0.250	0.885	0.438	0.000	0.867	0.727	0.500	0.000	0.869
	0.693				0.333				0.894				0.767				

National Data & Surveying Services

Intersection Turning Movement Count

Location: 7-Eleven Northern Dwy & Moriches Middle Island Rd
City: Shirley
Control: No Control

Project ID: 21-370026-004
Date: 7/22/2021

Data - Total

NS/EW Streets:	7-Eleven Northern Dwy				7-Eleven Northern Dwy				Moriches Middle Island Rd				Moriches Middle Island Rd				
AM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	0 NL	1 NT	0 NR	0 NU	0 SL	0 ST	0 SR	0 SU	0 EL	1 ET	0 ER	0 EU	0 WL	1 WT	0 WR	0 WU	
7:00 AM	2	0	15	0	0	0	0	0	0	0	1	0	13	0	0	0	31
7:15 AM	1	0	11	0	0	0	0	0	0	0	0	0	13	0	0	0	25
7:30 AM	4	0	15	0	0	0	0	0	0	0	0	0	18	0	0	0	37
7:45 AM	1	0	14	0	0	0	0	0	0	0	1	0	22	0	0	0	38
8:00 AM	2	0	11	0	0	0	0	0	0	0	2	0	10	0	0	0	25
8:15 AM	1	0	12	0	0	0	0	0	0	0	3	0	15	0	0	0	31
8:30 AM	5	0	13	0	0	0	0	0	0	0	3	0	10	0	0	0	31
8:45 AM	1	0	11	0	0	0	0	0	0	0	0	0	10	0	0	0	22
TOTAL VOLUMES :	NL 17	NT 0	NR 102	NU 0	SL 0	ST 0	SR 0	SU 0	EL 0	ET 0	ER 10	EU 0	WL 111	WT 0	WR 0	WU 0	TOTAL 240
APPROACH %'s :	14.29%	0.00%	85.71%	0.00%					0.00%	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	
PEAK HR :	07:30 AM - 08:30 AM																TOTAL
PEAK HR VOL :	8	0	52	0	0	0	0	0	0	0	6	0	65	0	0	0	131
PEAK HR FACTOR :	0.500	0.000	0.867	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.739	0.000	0.000	0.000	0.862
	0.789								0.500				0.739				

PM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	0 NL	1 NT	0 NR	0 NU	0 SL	0 ST	0 SR	0 SU	0 EL	1 ET	0 ER	0 EU	0 WL	1 WT	0 WR	0 WU	
4:00 PM	1	0	9	0	0	0	0	0	0	0	1	0	15	0	0	0	26
4:15 PM	0	0	7	0	0	0	0	0	0	0	2	0	12	0	0	0	21
4:30 PM	1	0	14	0	0	0	0	0	0	0	1	0	15	0	0	0	31
4:45 PM	1	0	10	0	0	0	0	0	0	0	2	0	14	0	0	0	27
5:00 PM	2	0	6	0	0	0	0	0	0	0	4	0	12	0	0	0	24
5:15 PM	2	0	15	0	0	0	0	0	0	0	1	0	16	0	0	0	34
5:30 PM	0	0	8	0	0	0	0	0	0	0	2	0	9	0	0	0	19
5:45 PM	0	0	7	0	0	0	0	0	0	0	0	0	13	0	0	0	20
6:00 PM	1	0	9	0	0	0	0	0	0	0	0	0	10	0	0	0	20
6:15 PM	1	0	7	0	0	0	0	0	0	0	0	0	17	0	0	0	25
6:30 PM	3	0	5	0	0	0	0	0	0	0	0	0	11	0	0	0	19
6:45 PM	1	0	9	0	0	0	0	0	0	0	2	0	8	0	0	0	20
TOTAL VOLUMES :	NL 13	NT 0	NR 106	NU 0	SL 0	ST 0	SR 0	SU 0	EL 0	ET 0	ER 15	EU 0	WL 152	WT 0	WR 0	WU 0	TOTAL 286
APPROACH %'s :	10.92%	0.00%	89.08%	0.00%					0.00%	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	
PEAK HR :	4:45 PM - 5:45 PM																TOTAL
PEAK HR VOL :	5	0	39	0	0	0	0	0	0	0	9	0	51	0	0	0	104
PEAK HR FACTOR :	0.625	0.000	0.650	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.563	0.000	0.797	0.000	0.000	0.000	0.765
	0.647				0.000				0.563				0.797				

National Data & Surveying Services

Intersection Turning Movement Count

Location: Revilo Ave & Colin Dr/7-Eleven Western Dwy
City: Shirley
Control: 1-Way Stop(EB)

Project ID: 21-370026-003
Date: 7/22/2021

Data - Total

NS/EW Streets:	Revilo Ave				Revilo Ave				Colin Dr/7-Eleven Western Dwy				Colin Dr/7-Eleven Western Dwy				
AM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	0 NL	1 NT	0 NR	0 NU	0 SL	1 ST	0 SR	0 SU	0 EL	1 ET	0 ER	0 EU	0 WL	1 WT	0 WR	0 WU	
7:00 AM	0	0	3	0	2	0	0	0	0	2	0	0	0	1	3	0	11
7:15 AM	0	0	1	0	2	0	0	0	0	0	0	0	0	1	0	0	4
7:30 AM	0	0	5	0	1	0	0	0	0	2	0	0	1	3	4	0	16
7:45 AM	0	0	5	0	0	0	0	0	0	1	0	0	2	2	0	0	10
8:00 AM	0	0	2	0	3	0	0	0	0	1	0	0	0	1	1	0	8
8:15 AM	0	0	1	0	1	0	0	0	0	3	0	0	1	2	3	0	11
8:30 AM	0	0	1	0	3	0	0	0	0	0	0	0	0	2	4	0	10
8:45 AM	0	0	1	0	1	0	0	0	0	2	0	0	0	2	3	0	9
TOTAL VOLUMES :	NL 0	NT 0	NR 19	NU 0	SL 13	ST 0	SR 0	SU 0	EL 0	ET 11	ER 0	EU 0	WL 4	WT 14	WR 18	WU 0	TOTAL 79
APPROACH %'s :	0.00%	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	11.11%	38.89%	50.00%	0.00%	
PEAK HR :	07:30 AM - 08:30 AM																TOTAL
PEAK HR VOL :	0	0	13	0	5	0	0	0	0	7	0	0	4	8	8	0	45
PEAK HR FACTOR :	0.000	0.000	0.650	0.000	0.417	0.000	0.000	0.000	0.000	0.583	0.000	0.000	0.500	0.667	0.500	0.000	0.703
	0.650				0.417				0.583				0.625				
PM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	0 NL	1 NT	0 NR	0 NU	0 SL	1 ST	0 SR	0 SU	0 EL	1 ET	0 ER	0 EU	0 WL	1 WT	0 WR	0 WU	
4:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	1	3	3	0	9
4:15 PM	0	0	1	0	0	0	0	0	0	2	0	0	1	2	2	0	8
4:30 PM	0	0	1	0	1	0	0	0	0	0	0	0	1	1	0	0	4
4:45 PM	0	0	3	0	2	0	0	0	0	1	0	0	2	0	0	0	8
5:00 PM	0	0	1	0	0	0	0	0	0	2	0	0	1	1	2	0	7
5:15 PM	0	0	2	0	1	0	0	0	0	1	0	0	0	2	1	0	7
5:30 PM	0	0	1	0	0	0	0	0	0	1	0	0	0	1	0	0	3
5:45 PM	0	0	1	0	2	0	0	0	0	1	0	0	2	3	3	0	12
6:00 PM	0	0	3	0	2	0	0	0	0	1	0	0	3	1	3	0	13
6:15 PM	0	0	0	0	1	0	0	0	0	0	0	0	2	0	2	0	5
6:30 PM	0	0	2	0	0	0	0	0	0	0	0	0	1	0	2	0	5
6:45 PM	0	0	0	0	3	0	0	0	0	0	0	0	0	1	2	0	6
TOTAL VOLUMES :	NL 0	NT 0	NR 16	NU 0	SL 13	ST 0	SR 0	SU 0	EL 0	ET 9	ER 0	EU 0	WL 14	WT 15	WR 20	WU 0	TOTAL 87
APPROACH %'s :	0.00%	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	28.57%	30.61%	40.82%	0.00%	
PEAK HR :	4:45 PM - 5:45 PM																TOTAL
PEAK HR VOL :	0	0	7	0	3	0	0	0	0	5	0	0	3	4	3	0	25
PEAK HR FACTOR :	0.000	0.000	0.583	0.000	0.375	0.000	0.000	0.000	0.000	0.625	0.000	0.000	0.375	0.500	0.375	0.000	0.781
	0.583				0.375				0.625				0.625				

National Data & Surveying Services

Intersection Turning Movement Count

Location: William Floyd Pkwy & 7-Eleven Eastern Dwy
City: Shirley
Control: No Control

Project ID: 21-370026-005
Date: 7/22/2021

Data - Total

NS/EW Streets:	William Floyd Pkwy				William Floyd Pkwy				7-Eleven Eastern Dwy				7-Eleven Eastern Dwy				
AM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	0 NL	4 NT	0 NR	0 NU	0 SL	2 ST	0 SR	0 SU	0 EL	1 ET	0 ER	0 EU	0 WL	0 WT	0 WR	0 WU	
7:00 AM	0	0	0	0	0	0	6	0	0	0	11	0	0	0	0	0	17
7:15 AM	0	0	0	0	0	0	7	0	0	0	3	0	0	0	0	0	10
7:30 AM	0	0	0	0	0	0	2	0	0	0	12	0	0	0	0	0	14
7:45 AM	0	0	0	0	0	0	5	0	0	0	10	0	0	0	0	0	15
8:00 AM	0	0	0	0	0	0	4	0	0	0	6	0	0	0	0	0	10
8:15 AM	0	0	0	0	0	0	5	0	0	0	6	0	0	0	0	0	11
8:30 AM	0	0	0	0	0	0	7	0	0	0	7	0	0	0	0	0	14
8:45 AM	0	0	0	0	0	0	8	0	0	0	6	0	0	0	0	0	14
TOTAL VOLUMES :	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	TOTAL
APPROACH %'s :	0	0	0	0	0	0	44	0	0	0	61	0	0	0	0	0	105
PEAK HR :	7:30 AM - 8:30 AM																TOTAL
PEAK HR VOL :	0	0	0	0	0	0	16	0	0	0	34	0	0	0	0	0	50
PEAK HR FACTOR :	0.000	0.000	0.000	0.000	0.000	0.000	0.800	0.000	0.000	0.000	0.708	0.000	0.000	0.000	0.000	0.000	0.833
	0.000				0.800				0.708				0.000				

PM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	0 NL	4 NT	0 NR	0 NU	0 SL	2 ST	0 SR	0 SU	0 EL	1 ET	0 ER	0 EU	0 WL	0 WT	0 WR	0 WU	
4:00 PM	0	0	0	0	0	0	6	0	0	0	9	0	0	0	0	0	15
4:15 PM	0	0	0	0	0	0	8	0	0	0	11	0	0	0	0	0	19
4:30 PM	0	0	0	0	0	0	3	0	0	0	11	0	0	0	0	0	14
4:45 PM	0	0	0	0	0	0	1	0	0	0	7	0	0	0	0	0	8
5:00 PM	0	0	0	0	0	0	4	0	0	0	9	0	0	0	0	0	13
5:15 PM	0	0	0	0	0	0	9	0	0	0	8	0	0	0	0	0	17
5:30 PM	0	0	0	0	0	0	1	0	0	0	3	0	0	0	0	0	4
5:45 PM	0	0	0	0	0	0	2	0	0	0	4	0	0	0	0	0	6
6:00 PM	0	0	0	0	0	0	4	0	0	0	9	0	0	0	0	0	13
6:15 PM	0	0	0	0	0	0	1	0	0	0	6	0	0	0	0	0	7
6:30 PM	0	0	0	0	0	0	3	0	0	0	4	0	0	0	0	0	7
6:45 PM	0	0	0	0	0	0	5	0	0	0	5	0	0	0	0	0	10
TOTAL VOLUMES :	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	TOTAL
APPROACH %'s :	0	0	0	0	0	0	47	0	0	0	86	0	0	0	0	0	133
PEAK HR :	04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :	0	0	0	0	0	0	15	0	0	0	27	0	0	0	0	0	42
PEAK HR FACTOR :	0.000	0.000	0.000	0.000	0.000	0.000	0.417	0.000	0.000	0.000	0.750	0.000	0.000	0.000	0.000	0.000	0.618
	0.000				0.417				0.750				0.000				

NYSDOT TRAFFIC VOLUME COUNT DATA

STATION: 71107

New York State Department of Transportation

Traffic Count Hourly Report

ROUTE #: CR 46

ROAD NAME: WILLIAM FLOYD PARKWAY

FROM : MORICHES MIDDLE ISLAND RD

TO: RT 495I

DIRECTION: NB-SB

PLACEMENT: 447 FT S OF ROSE EXECUTIVE RD

COUNTY: SUFFOLK

TOWN: BROOKHAVEN

DATE OF COUNT: MONDAY, MAY 28, 2018

JURISDICTION: NYS DOT & SCDPW

Interval Start Time	NORTHBOUND	SOUTHBOUND	TOTAL
12:00 AM	77	250	327
1:00 AM	44	128	172
2:00 AM	39	97	136
3:00 AM	69	65	134
4:00 AM	246	104	350
5:00 AM	496	290	786
6:00 AM	1003	682	1685
7:00 AM	1216	1062	2278
8:00 AM	1098	984	2082
9:00 AM	702	800	1502
10:00 AM	723	786	1509
11:00 AM	737	919	1656
12:00 PM	755	1088	1843
1:00 PM	773	1096	1869
2:00 PM	835	1252	2087
3:00 PM	923	1470	2393
4:00 PM	794	1624	2418
5:00 PM	796	1626	2422
6:00 PM	714	1332	2046
7:00 PM	596	1094	1690
8:00 PM	502	960	1462
9:00 PM	391	808	1199
10:00 PM	264	587	851
11:00 PM	158	413	571
TOTAL	13951	19517	33468

STATION: 71017

New York State Department of Transportation

Traffic Count Hourly Report

ROUTE #: CR 46

ROAD NAME: WILLIAM FLOYD PARKWAY

FROM: NY 27 SSR

TO: MORICHES-MIDDLE

DIRECTION: NB-SB

PLACEMENT: 563 FT S OF MORICHES-MIDDLE ISLA

COUNTY: SUFFOLK

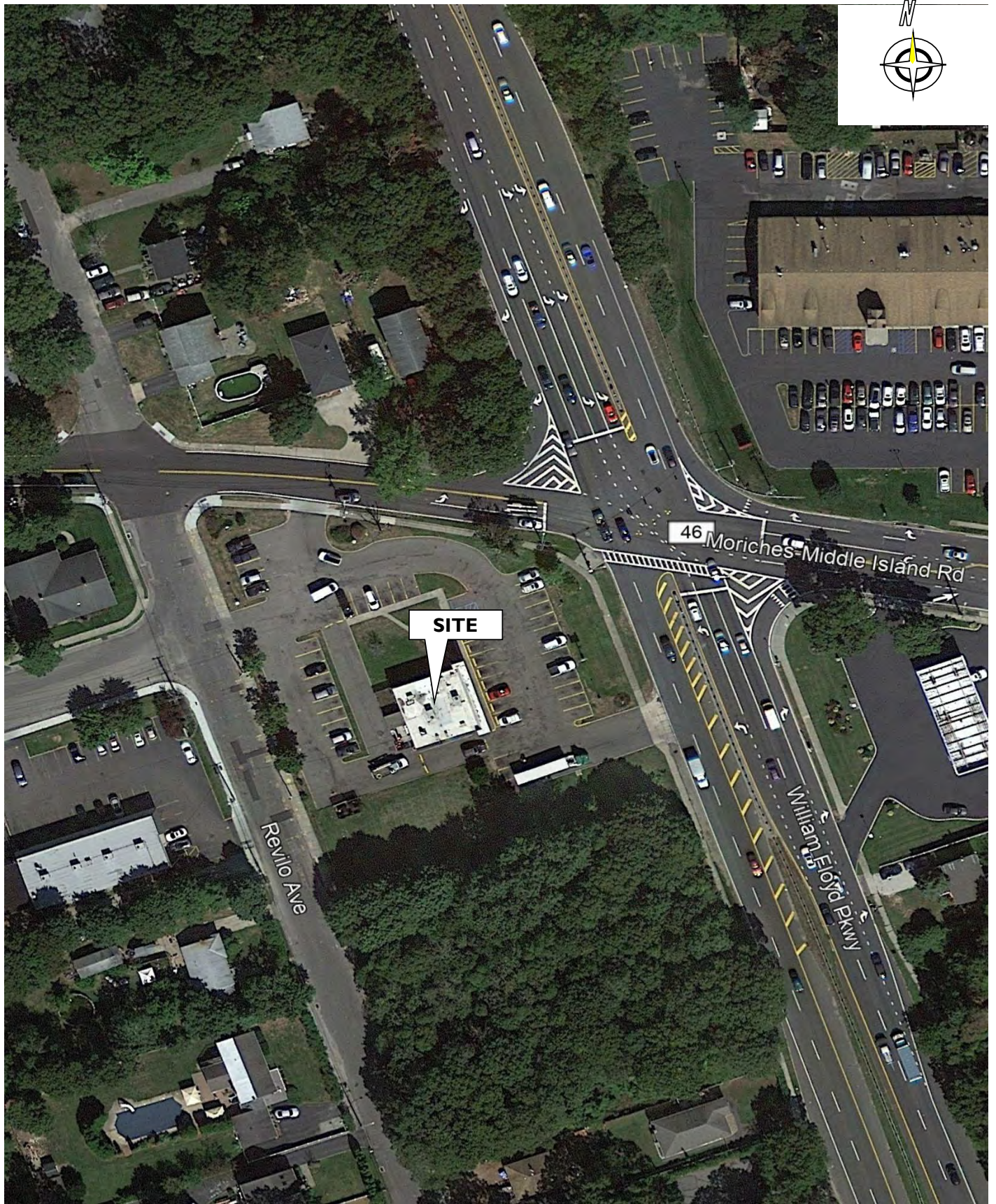
TOWN: BROOKHAVEN

DATE OF COUNT: MONDAY, MAY 28, 2018

JURISDICTION: NYSDOT & SCDPW

Interval Start Time	NORTHBOUND	SOUTHBOUND	TOTAL
12:00 AM	110	194	304
1:00 AM	68	95	163
2:00 AM	46	76	122
3:00 AM	86	56	142
4:00 AM	276	98	374
5:00 AM	533	290	823
6:00 AM	1036	602	1638
7:00 AM	1181	867	2048
8:00 AM	1007	763	1770
9:00 AM	859	708	1567
10:00 AM	792	689	1481
11:00 AM	846	773	1619
12:00 PM	924	858	1782
1:00 PM	946	896	1842
2:00 PM	1004	968	1972
3:00 PM	1104	1132	2236
4:00 PM	968	1209	2177
5:00 PM	976	1203	2179
6:00 PM	886	1048	1934
7:00 PM	787	842	1629
8:00 PM	650	724	1374
9:00 PM	538	632	1170
10:00 PM	362	464	826
11:00 PM	216	309	525
TOTAL	16201	15496	31697

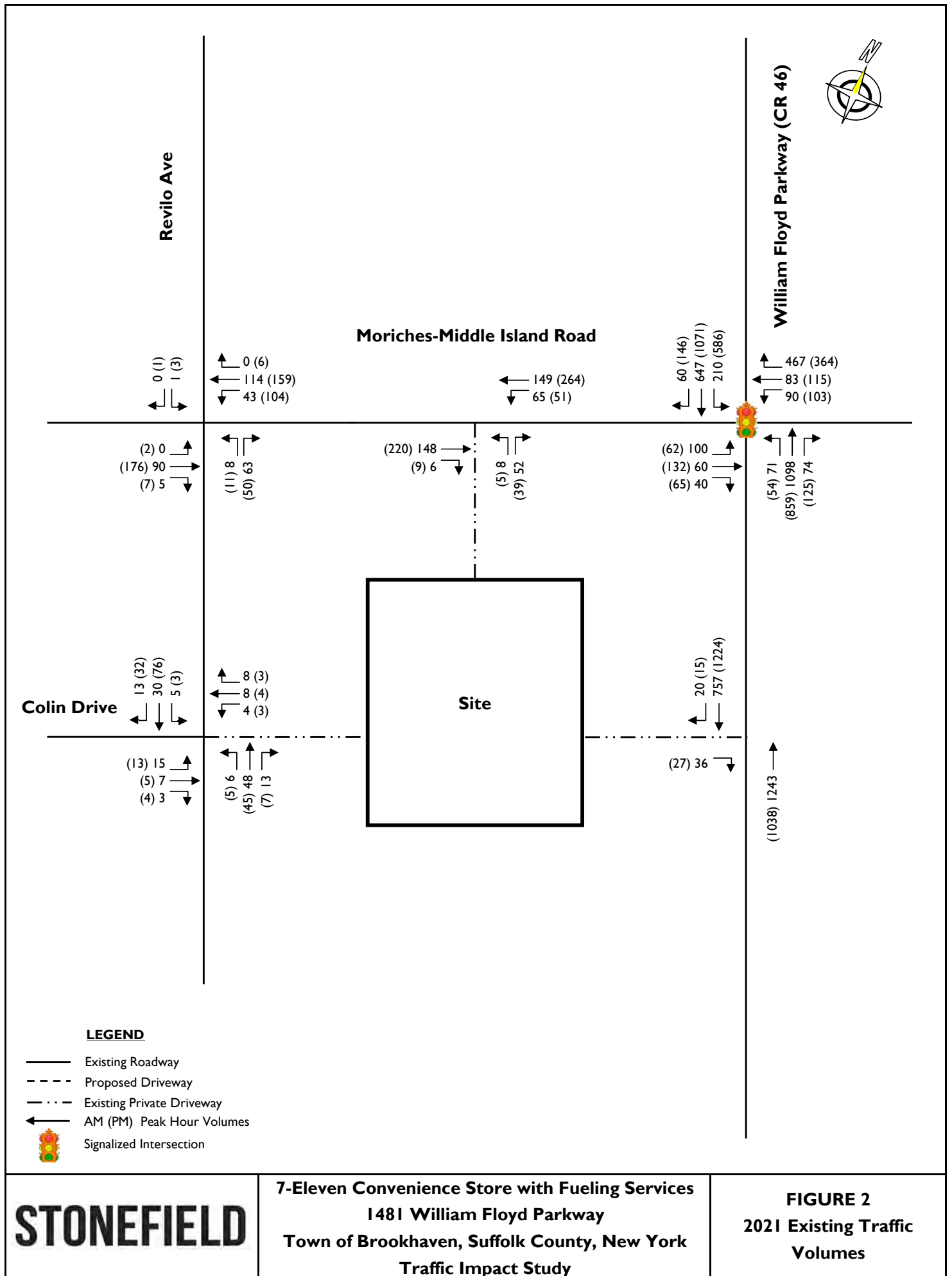
FIGURES

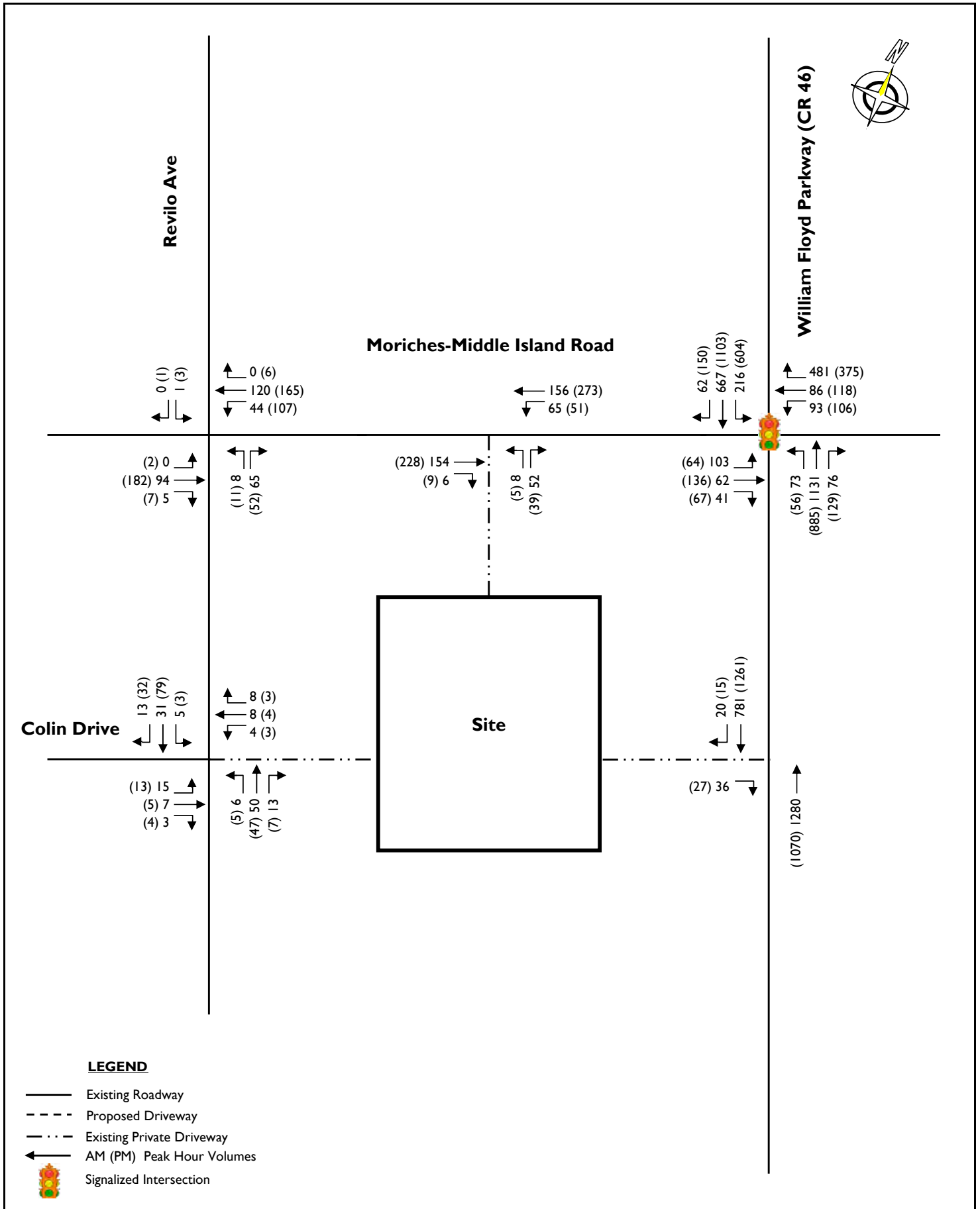


STONEFIELD

7-Eleven Convenience Store with Fueling Services
1481 William Floyd Parkway
Town of Brookhaven, Suffolk County, New York
Traffic Impact Study

FIGURE I
Site Location Map

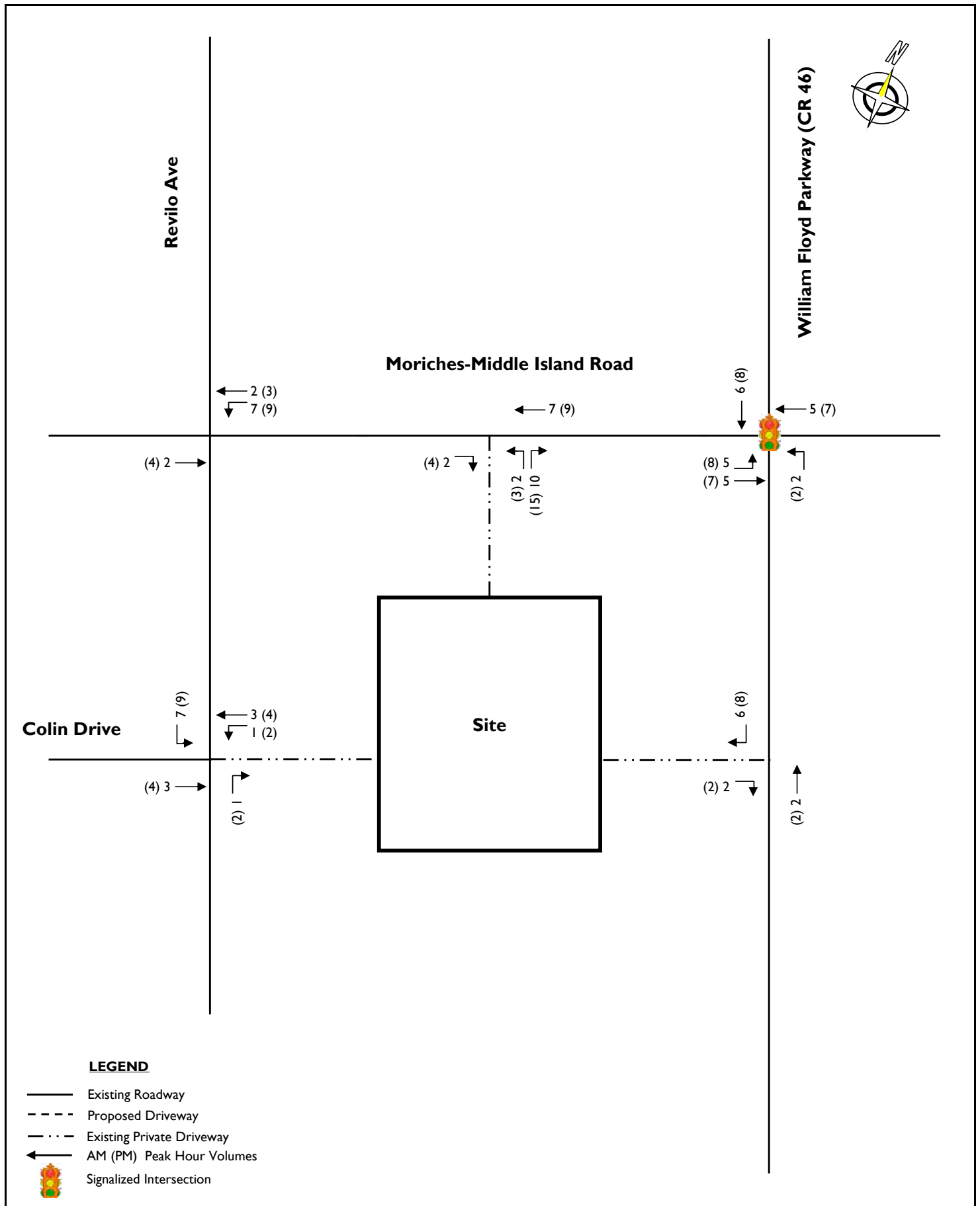




STONEFIELD

7-Eleven Convenience Store with Fueling Services
1481 William Floyd Parkway
Town of Brookhaven, Suffolk County, New York
Traffic Impact Study

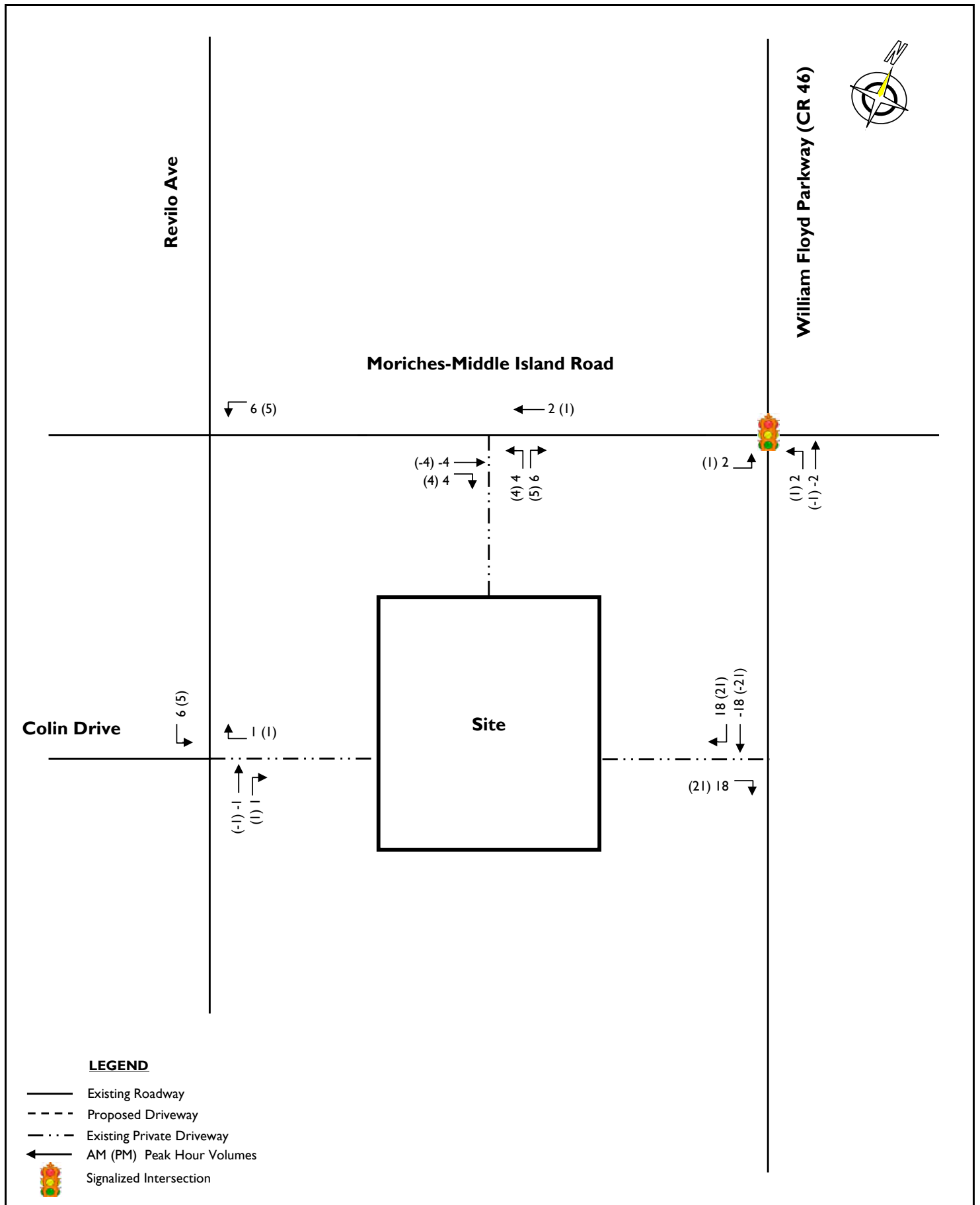
FIGURE 3
2023 No-Build Traffic
Volumes



STONEFIELD

7-Eleven Convenience Store with Fueling Services
1481 William Floyd Parkway
Town of Brookhaven, Suffolk County, New York
Traffic Impact Study

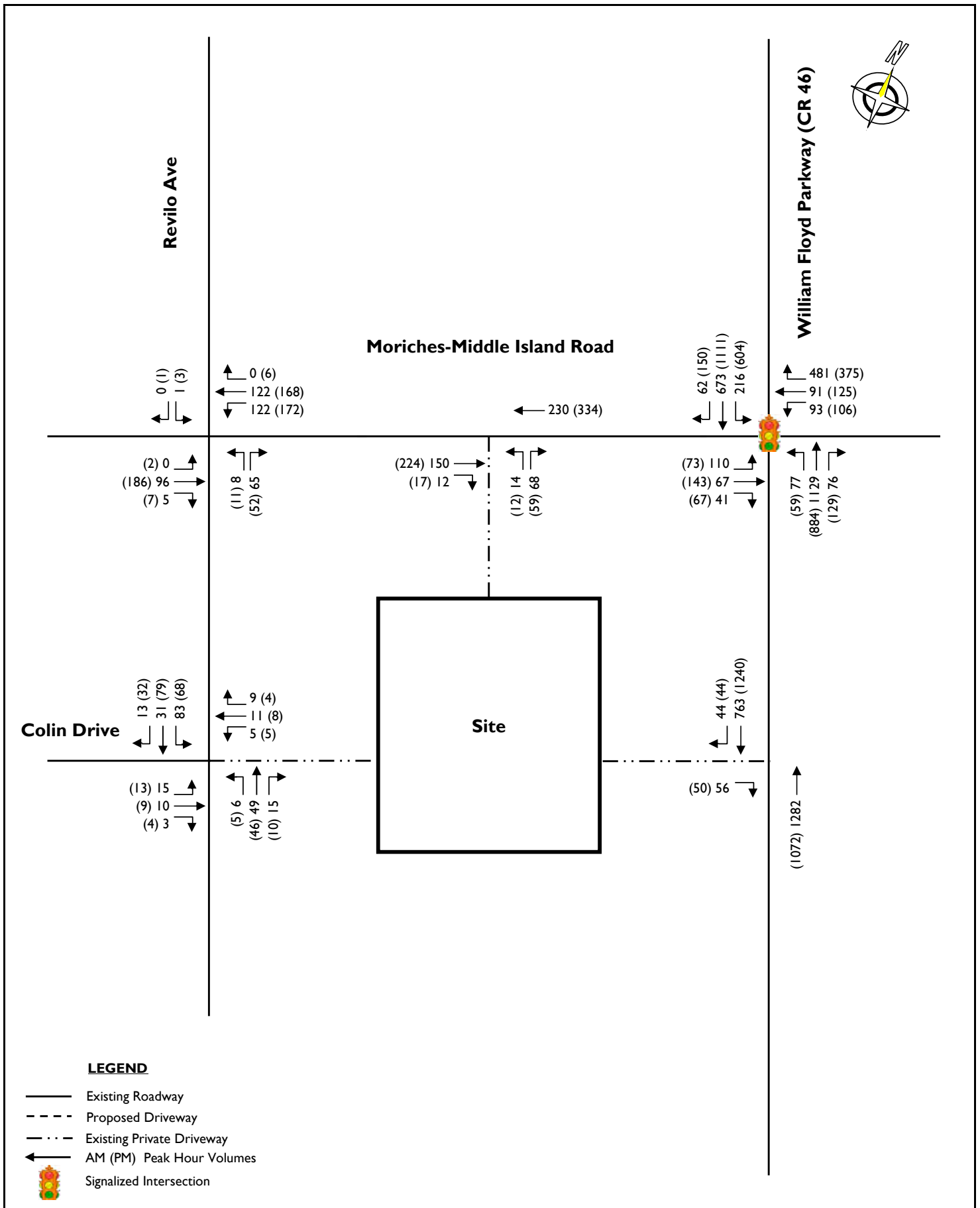
FIGURE 5
"New" Site-Generated
Traffic Volumes



STONEFIELD

7-Eleven Convenience Store with Fueling Services
1481 William Floyd Parkway
Town of Brookhaven, Suffolk County, New York
Traffic Impact Study

FIGURE 6
"Pass-By" Site-Generated
Traffic Volumes



STONEFIELD

7-Eleven Convenience Store with Fueling Services
1481 William Floyd Parkway
Town of Brookhaven, Suffolk County, New York
Traffic Impact Study
























FIGURE 7
2023 Build Traffic Volumes

HIGHWAY CAPACITY ANALYSIS DETAIL SHEETS

HCM 6th Signalized Intersection Summary

5: William Floyd Parkway & Moriches-Middle Island Road

2021 Existing
Weekday Morning Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	100	60	40	90	83	467	71	1098	74	210	647	60
Future Volume (veh/h)	100	60	40	90	83	467	71	1098	74	210	647	60
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1826	1752	1500	1781	1870	1767	1737	1826	1796	1811	1811	1596
Adj Flow Rate, veh/h	110	66	44	99	91	0	78	1207	0	231	711	0
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	5	10	27	8	2	4	11	5	7	6	6	16
Cap, veh/h	262	169	113	234	323		98	1350		326	1419	
Arrive On Green	0.17	0.17	0.17	0.17	0.17	0.00	0.06	0.39	0.00	0.10	0.41	0.00
Sat Flow, veh/h	1274	980	654	1222	1870	1498	1654	3469	1522	3346	3441	1353
Grp Volume(v), veh/h	110	0	110	99	91	0	78	1207	0	231	711	0
Grp Sat Flow(s),veh/h/ln	1274	0	1634	1222	1870	1498	1654	1735	1522	1673	1721	1353
Q Serve(g_s), s	6.1	0.0	4.5	5.9	3.2	0.0	3.5	24.4	0.0	5.0	11.4	0.0
Cycle Q Clear(g_c), s	9.3	0.0	4.5	10.3	3.2	0.0	3.5	24.4	0.0	5.0	11.4	0.0
Prop In Lane	1.00		0.40	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	262	0	282	234	323		98	1350		326	1419	
V/C Ratio(X)	0.42	0.00	0.39	0.42	0.28		0.79	0.89		0.71	0.50	
Avail Cap(c_a), veh/h	470	0	548	428	620		252	1419		559	1419	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	31.0	0.0	27.5	32.0	26.9	0.0	34.7	21.4	0.0	32.7	16.3	0.0
Incr Delay (d2), s/veh	1.1	0.0	0.9	1.2	0.5	0.0	13.2	7.5	0.0	2.8	0.3	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.9	0.0	1.8	1.8	1.4	0.0	1.7	9.9	0.0	2.0	3.9	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	32.0	0.0	28.3	33.2	27.4	0.0	48.0	28.9	0.0	35.6	16.6	0.0
LnGrp LOS	C	A	C	C	C		D	C		D	B	
Approach Vol, veh/h	220			190			1285			942		
Approach Delay, s/veh	30.2			30.4			30.0			21.2		
Approach LOS	C			C			C			C		
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	14.8	39.9		20.1	13.0	41.7		20.1				
Change Period (Y+Rc), s	* 7.5	* 11		* 7.2	8.6	10.8		7.2				
Max Green Setting (Gmax), s	* 13	* 31		* 25	11.4	27.2		24.8				
Max Q Clear Time (g_c+I1), s	7.0	26.4		11.3	5.5	13.4		12.3				
Green Ext Time (p_c), s	0.3	2.7		0.8	0.1	3.7		0.6				

Intersection Summary

HCM 6th Ctrl Delay	26.9
HCM 6th LOS	C





Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Unsignalized Delay for [NBR, WBR, SBR] is excluded from calculations of the approach delay and intersection delay.




HCM 6th TWSC
6: Revilo Avenue & Moriches-Middle Island Road

2021 Existing
Weekday Morning Peak Hour

Intersection												
Int Delay, s/veh	3.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	0	90	5	43	114	0	8	0	63	1	0	0
Future Vol, veh/h	0	90	5	43	114	0	8	0	63	1	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	18	20	7	14	2	12	2	8	0	2	2
Mvmt Flow	0	96	5	46	121	0	9	0	67	1	0	0
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	121	0	0	101	0	0	312	312	99	345	314	121
Stage 1	-	-	-	-	-	-	99	99	-	213	213	-
Stage 2	-	-	-	-	-	-	213	213	-	132	101	-
Critical Hdwy	4.12	-	-	4.17	-	-	7.22	6.52	6.28	7.1	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.22	5.52	-	6.1	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.22	5.52	-	6.1	5.52	-
Follow-up Hdwy	2.218	-	-	2.263	-	-	3.608	4.018	3.372	3.5	4.018	3.318
Pot Cap-1 Maneuver	1467	-	-	1461	-	-	621	603	941	613	601	930
Stage 1	-	-	-	-	-	-	883	813	-	794	726	-
Stage 2	-	-	-	-	-	-	767	726	-	876	811	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1467	-	-	1461	-	-	605	582	941	555	581	930
Mov Cap-2 Maneuver	-	-	-	-	-	-	605	582	-	555	581	-
Stage 1	-	-	-	-	-	-	883	813	-	794	701	-
Stage 2	-	-	-	-	-	-	741	701	-	814	811	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0			2.1			9.4			11.5		
HCM LOS							A			B		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	886	1467	-	-	1461	-	-	555				
HCM Lane V/C Ratio	0.085	-	-	-	0.031	-	-	0.002				
HCM Control Delay (s)	9.4	0	-	-	7.5	0	-	11.5				
HCM Lane LOS	A	A	-	-	A	A	-	B				
HCM 95th %tile Q(veh)	0.3	0	-	-	0.1	-	-	0				

HCM 6th TWSC
9: Site Driveway & Moriches-Middle Island Road

2021 Existing
Weekday Morning Peak Hour

Intersection						
Int Delay, s/veh	2.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	148	6	65	149	8	52
Future Vol, veh/h	148	6	65	149	8	52
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	11	2	2	13	2	2
Mvmt Flow	161	7	71	162	9	57
Major/Minor	Major1	Major2		Minor1		
Conflicting Flow All	0	0	168	0	469	165
Stage 1	-	-	-	-	165	-
Stage 2	-	-	-	-	304	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1410	-	553	879
Stage 1	-	-	-	-	864	-
Stage 2	-	-	-	-	748	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1410	-	523	879
Mov Cap-2 Maneuver	-	-	-	-	523	-
Stage 1	-	-	-	-	864	-
Stage 2	-	-	-	-	707	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		2.3		9.9	
HCM LOS					A	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	806	-	-	1410	-	
HCM Lane V/C Ratio	0.081	-	-	0.05	-	
HCM Control Delay (s)	9.9	-	-	7.7	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0.3	-	-	0.2	-	





HCM 6th TWSC
12: Revilo Avenue & Colin Drive/Site Driveway

2021 Existing
Weekday Morning Peak Hour

Intersection												
Int Delay, s/veh	3.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	15	7	3	4	8	8	6	48	13	5	30	13
Future Vol, veh/h	15	7	3	4	8	8	6	48	13	5	30	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	7	2	2	8	2
Mvmt Flow	16	8	3	4	9	9	7	52	14	5	33	14
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	132	130	40	129	130	59	47	0	0	66	0	0
Stage 1	50	50	-	73	73	-	-	-	-	-	-	-
Stage 2	82	80	-	56	57	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	840	761	1031	844	761	1007	1560	-	-	1536	-	-
Stage 1	963	853	-	937	834	-	-	-	-	-	-	-
Stage 2	926	828	-	956	847	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	821	755	1031	830	755	1007	1560	-	-	1536	-	-
Mov Cap-2 Maneuver	821	755	-	830	755	-	-	-	-	-	-	-
Stage 1	958	850	-	932	830	-	-	-	-	-	-	-
Stage 2	904	824	-	942	844	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	9.5		9.3		0.7		0.8					
HCM LOS	A		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1560	-	-	821	856	1536	-	-				
HCM Lane V/C Ratio	0.004	-	-	0.033	0.025	0.004	-	-				
HCM Control Delay (s)	7.3	0	-	9.5	9.3	7.4	0	-				
HCM Lane LOS	A	A	-	A	A	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	0.1	0	-	-				

HCM 6th TWSC
14: William Floyd Parkway & Site Driveway

2021 Existing
Weekday Morning Peak Hour

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	0	36	0	1243	757	20
Future Vol, veh/h	0	36	0	1243	757	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	20	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	5	7	2
Mvmt Flow	0	39	0	1351	823	22

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	423	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-	-
Pot Cap-1 Maneuver	0	579	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	579	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
























Approach	EB	NB	SB
HCM Control Delay, s	11.7	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT EBLn1	SBT	SBR
Capacity (veh/h)	- 579	-	-
HCM Lane V/C Ratio	- 0.068	-	-
HCM Control Delay (s)	- 11.7	-	-
HCM Lane LOS	- B	-	-
HCM 95th %tile Q(veh)	- 0.2	-	-

HCM 6th Signalized Intersection Summary

5: William Floyd Parkway & Moriches-Middle Island Road

2021 Existing
Weekday Evening Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	62	132	65	103	115	364	54	859	125	586	1071	146
Future Volume (veh/h)	62	132	65	103	115	364	54	859	125	586	1071	146
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1900	1885	1870	1870	1796	1767	1826	1870	1900	1870	1885	1796
Adj Flow Rate, veh/h	63	133	66	104	116	0	55	868	0	592	1082	0
Peak Hour Factor	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99
Percent Heavy Veh, %	0	1	2	2	7	4	5	2	0	2	1	2
Cap, veh/h	292	253	125	222	382		70	1074		608	1521	
Arrive On Green	0.21	0.21	0.21	0.21	0.21	0.00	0.04	0.30	0.00	0.18	0.42	0.00
Sat Flow, veh/h	1296	1189	590	1183	1796	1498	1739	3554	1610	3456	3582	1522
Grp Volume(v), veh/h	63	0	199	104	116	0	55	868	0	592	1082	0
Grp Sat Flow(s),veh/h/ln	1296	0	1779	1183	1796	1498	1739	1777	1610	1728	1791	1522
Q Serve(g_s), s	3.5	0.0	8.2	7.0	4.5	0.0	2.6	18.6	0.0	14.0	20.5	0.0
Cycle Q Clear(g_c), s	8.0	0.0	8.2	15.2	4.5	0.0	2.6	18.6	0.0	14.0	20.5	0.0
Prop In Lane	1.00		0.33	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	292	0	378	222	382		70	1074		608	1521	
V/C Ratio(X)	0.22	0.00	0.53	0.47	0.30		0.79	0.81		0.97	0.71	
Avail Cap(c_a), veh/h	349	0	455	269	453		283	1405		608	1521	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	30.7	0.0	28.8	35.5	27.3	0.0	39.2	26.6	0.0	33.8	19.6	0.0
Incr Delay (d2), s/veh	0.4	0.0	1.1	1.5	0.4	0.0	17.3	2.7	0.0	29.9	1.6	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.1	0.0	3.5	2.1	1.9	0.0	1.4	7.5	0.0	8.0	7.8	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	31.1	0.0	29.9	37.1	27.8	0.0	56.5	29.3	0.0	63.7	21.1	0.0
LnGrp LOS	C	A	C	D	C		E	C		E	C	
Approach Vol, veh/h	262			220			A			923		
Approach Delay, s/veh	30.2			32.2			30.9			36.2		
Approach LOS	C			C			C			D		
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	22.0	35.7		24.7	11.9	45.8		24.7				
Change Period (Y+Rc), s	* 7.5	* 11		* 7.2	8.6	10.8		7.2				
Max Green Setting (Gmax), s	* 15	* 33		* 21	13.4	29.2		20.8				
Max Q Clear Time (g_c+I1), s	16.0	20.6		10.2	4.6	22.5		17.2				
Green Ext Time (p_c), s	0.0	4.3		1.0	0.1	3.6		0.3				

Intersection Summary

HCM 6th Ctrl Delay	33.8
HCM 6th LOS	C

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Unsignalized Delay for [NBR, WBR, SBR] is excluded from calculations of the approach delay and intersection delay.




HCM 6th TWSC
6: Revilo Avenue & Moriches-Middle Island Road

2021 Existing
Weekday Evening Peak Hour

Intersection												
Int Delay, s/veh	3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	2	176	7	104	159	6	11	0	50	3	0	1
Future Vol, veh/h	2	176	7	104	159	6	11	0	50	3	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	87	87	87	87	87	87	87	87	87	87	87	87
Heavy Vehicles, %	0	1	0	2	7	0	0	0	0	0	0	0
Mvmt Flow	2	202	8	120	183	7	13	0	57	3	0	1
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	190	0	0	210	0	0	637	640	206	666	641	187
Stage 1	-	-	-	-	-	-	210	210	-	427	427	-
Stage 2	-	-	-	-	-	-	427	430	-	239	214	-
Critical Hdwy	4.1	-	-	4.12	-	-	7.1	6.5	6.2	7.1	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Follow-up Hdwy	2.2	-	-	2.218	-	-	3.5	4	3.3	3.5	4	3.3
Pot Cap-1 Maneuver	1396	-	-	1361	-	-	393	396	840	376	395	860
Stage 1	-	-	-	-	-	-	797	732	-	610	589	-
Stage 2	-	-	-	-	-	-	610	587	-	769	729	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1396	-	-	1361	-	-	362	356	840	323	355	860
Mov Cap-2 Maneuver	-	-	-	-	-	-	362	356	-	323	355	-
Stage 1	-	-	-	-	-	-	795	731	-	609	531	-
Stage 2	-	-	-	-	-	-	549	529	-	715	728	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.1			3.1			10.9			14.5		
HCM LOS							B			B		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	678	1396	-	-	1361	-	-	383				
HCM Lane V/C Ratio	0.103	0.002	-	-	0.088	-	-	0.012				
HCM Control Delay (s)	10.9	7.6	0	-	7.9	0	-	14.5				
HCM Lane LOS	B	A	A	-	A	A	-	B				
HCM 95th %tile Q(veh)	0.3	0	-	-	0.3	-	-	0				

HCM 6th TWSC
9: Site Driveway & Moriches-Middle Island Road

2021 Existing
Weekday Evening Peak Hour

Intersection						
Int Delay, s/veh	1.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	220	9	51	264	5	39
Future Vol, veh/h	220	9	51	264	5	39
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	1	2	2	5	2	2
Mvmt Flow	239	10	55	287	5	42
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	249	0	641	244
Stage 1	-	-	-	-	244	-
Stage 2	-	-	-	-	397	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1317	-	439	795
Stage 1	-	-	-	-	797	-
Stage 2	-	-	-	-	679	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1317	-	417	795
Mov Cap-2 Maneuver	-	-	-	-	417	-
Stage 1	-	-	-	-	797	-
Stage 2	-	-	-	-	645	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		1.3		10.3	
HCM LOS					B	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	721	-	-	1317	-	
HCM Lane V/C Ratio	0.066	-	-	0.042	-	
HCM Control Delay (s)	10.3	-	-	7.9	0	
HCM Lane LOS	B	-	-	A	A	
HCM 95th %tile Q(veh)	0.2	-	-	0.1	-	

HCM 6th TWSC
12: Revilo Avenue & Colin Drive/Site Driveway

2021 Existing
Weekday Evening Peak Hour

Intersection												
Int Delay, s/veh	1.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	13	5	4	3	4	3	5	45	7	3	76	32
Future Vol, veh/h	13	5	4	3	4	3	5	45	7	3	76	32
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	2	0	2	2	2	0	0	2	2	1	3
Mvmt Flow	14	5	4	3	4	3	5	49	8	3	83	35
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	174	174	101	174	187	53	118	0	0	57	0	0
Stage 1	107	107	-	63	63	-	-	-	-	-	-	-
Stage 2	67	67	-	111	124	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.52	6.2	7.12	6.52	6.22	4.1	-	-	4.12	-	-
Critical Hdwy Stg 1	6.1	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4.018	3.3	3.518	4.018	3.318	2.2	-	-	2.218	-	-
Pot Cap-1 Maneuver	793	719	960	789	708	1014	1483	-	-	1547	-	-
Stage 1	903	807	-	948	842	-	-	-	-	-	-	-
Stage 2	948	839	-	894	793	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	783	715	960	778	704	1014	1483	-	-	1547	-	-
Mov Cap-2 Maneuver	783	715	-	778	704	-	-	-	-	-	-	-
Stage 1	900	805	-	945	839	-	-	-	-	-	-	-
Stage 2	937	836	-	882	791	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	9.7		9.6		0.7		0.2					
HCM LOS	A		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1483	-	-	792	800	1547	-	-				
HCM Lane V/C Ratio	0.004	-	-	0.03	0.014	0.002	-	-				
HCM Control Delay (s)	7.4	0	-	9.7	9.6	7.3	0	-				
HCM Lane LOS	A	A	-	A	A	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	0	0	-	-				

HCM 6th TWSC
14: William Floyd Parkway & Site Driveway

2021 Existing
Weekday Evening Peak Hour

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		↗		↑↑↑	↑↑	
Traffic Vol, veh/h	0	27	0	1038	1224	15
Future Vol, veh/h	0	27	0	1038	1224	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	20	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	1	2
Mvmt Flow	0	29	0	1128	1330	16

Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	- 673	- 0	- 0
Stage 1	- -	- -	- -
Stage 2	- -	- -	- -
Critical Hdwy	- 6.94	- -	- -
Critical Hdwy Stg 1	- -	- -	- -
Critical Hdwy Stg 2	- -	- -	- -
Follow-up Hdwy	- 3.32	- -	- -
Pot Cap-1 Maneuver	0 398	0 -	- -
Stage 1	0 -	0 -	- -
Stage 2	0 -	0 -	- -
Platoon blocked, %		- -	- -
Mov Cap-1 Maneuver	- 398	- -	- -
Mov Cap-2 Maneuver	- -	- -	- -
Stage 1	- -	- -	- -
Stage 2	- -	- -	- -
























Approach	EB	NB	SB
HCM Control Delay, s	14.8	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT EBLn1	SBT	SBR
Capacity (veh/h)	- 398	- -	- -
HCM Lane V/C Ratio	- 0.074	- -	- -
HCM Control Delay (s)	- 14.8	- -	- -
HCM Lane LOS	- B	- -	- -
HCM 95th %tile Q(veh)	- 0.2	- -	- -

HCM 6th Signalized Intersection Summary

5: William Floyd Parkway & Moriches-Middle Island Road

2023 No-Build
Weekday Morning Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	103	62	41	93	86	481	73	1131	76	216	667	62
Future Volume (veh/h)	103	62	41	93	86	481	73	1131	76	216	667	62
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1826	1752	1500	1781	1870	1767	1737	1826	1796	1811	1811	1596
Adj Flow Rate, veh/h	113	68	45	102	95	0	80	1243	0	237	733	0
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	5	10	27	8	2	4	11	5	7	6	6	16
Cap, veh/h	262	173	115	235	330		101	1355		331	1424	
Arrive On Green	0.18	0.18	0.18	0.18	0.18	0.00	0.06	0.39	0.00	0.10	0.41	0.00
Sat Flow, veh/h	1270	984	651	1219	1870	1498	1654	3469	1522	3346	3441	1353
Grp Volume(v), veh/h	113	0	113	102	95	0	80	1243	0	237	733	0
Grp Sat Flow(s),veh/h/ln	1270	0	1635	1219	1870	1498	1654	1735	1522	1673	1721	1353
Q Serve(g_s), s	6.5	0.0	4.7	6.2	3.4	0.0	3.6	25.9	0.0	5.2	12.1	0.0
Cycle Q Clear(g_c), s	9.8	0.0	4.7	10.8	3.4	0.0	3.6	25.9	0.0	5.2	12.1	0.0
Prop In Lane	1.00		0.40	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	262	0	288	235	330		101	1355		331	1424	
V/C Ratio(X)	0.43	0.00	0.39	0.43	0.29		0.79	0.92		0.72	0.51	
Avail Cap(c_a), veh/h	456	0	538	416	608		247	1392		548	1424	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	31.5	0.0	27.8	32.6	27.3	0.0	35.3	22.1	0.0	33.3	16.6	0.0
Incr Delay (d2), s/veh	1.1	0.0	0.9	1.3	0.5	0.0	12.9	9.7	0.0	2.9	0.3	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.0	0.0	1.8	1.8	1.5	0.0	1.7	10.9	0.0	2.1	4.2	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	32.6	0.0	28.7	33.9	27.7	0.0	48.2	31.8	0.0	36.2	17.0	0.0
LnGrp LOS	C	A	C	C	C		D	C		D	B	
Approach Vol, veh/h	226			197			1323			970		
Approach Delay, s/veh	30.7			30.9			32.8			21.7		
Approach LOS	C			C			C			C		
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	15.0	40.6		20.6	13.3	42.4		20.6				
Change Period (Y+Rc), s	* 7.5	* 11		* 7.2	8.6	10.8		7.2				
Max Green Setting (Gmax), s	* 13	* 31		* 25	11.4	27.2		24.8				
Max Q Clear Time (g_c+I1), s	7.2	27.9		11.8	5.6	14.1		12.8				
Green Ext Time (p_c), s	0.3	1.8		0.8	0.1	3.8		0.6				

Intersection Summary

HCM 6th Ctrl Delay	28.5
HCM 6th LOS	C





Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Unsignalized Delay for [NBR, WBR, SBR] is excluded from calculations of the approach delay and intersection delay.




HCM 6th TWSC
6: Revilo Avenue & Moriches-Middle Island Road

2023 No-Build
Weekday Morning Peak Hour

Intersection												
Int Delay, s/veh	3.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	0	94	5	44	120	0	8	0	65	1	0	0
Future Vol, veh/h	0	94	5	44	120	0	8	0	65	1	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	18	20	7	14	2	12	2	8	0	2	2
Mvmt Flow	0	100	5	47	128	0	9	0	69	1	0	0
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	128	0	0	105	0	0	325	325	103	359	327	128
Stage 1	-	-	-	-	-	-	103	103	-	222	222	-
Stage 2	-	-	-	-	-	-	222	222	-	137	105	-
Critical Hdwy	4.12	-	-	4.17	-	-	7.22	6.52	6.28	7.1	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.22	5.52	-	6.1	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.22	5.52	-	6.1	5.52	-
Follow-up Hdwy	2.218	-	-	2.263	-	-	3.608	4.018	3.372	3.5	4.018	3.318
Pot Cap-1 Maneuver	1458	-	-	1456	-	-	609	593	936	600	591	922
Stage 1	-	-	-	-	-	-	879	810	-	785	720	-
Stage 2	-	-	-	-	-	-	758	720	-	871	808	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1458	-	-	1456	-	-	593	572	936	541	570	922
Mov Cap-2 Maneuver	-	-	-	-	-	-	593	572	-	541	570	-
Stage 1	-	-	-	-	-	-	879	810	-	785	695	-
Stage 2	-	-	-	-	-	-	731	695	-	807	808	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0			2			9.5			11.7		
HCM LOS							A			B		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	880	1458	-	-	1456	-	-	541				
HCM Lane V/C Ratio	0.088	-	-	-	0.032	-	-	0.002				
HCM Control Delay (s)	9.5	0	-	-	7.6	0	-	11.7				
HCM Lane LOS	A	A	-	-	A	A	-	B				
HCM 95th %tile Q(veh)	0.3	0	-	-	0.1	-	-	0				

HCM 6th TWSC
9: Site Driveway & Moriches-Middle Island Road

2023 No-Build
Weekday Morning Peak Hour

Intersection						
Int Delay, s/veh	2.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	154	6	65	156	8	52
Future Vol, veh/h	154	6	65	156	8	52
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	11	2	2	13	2	2
Mvmt Flow	167	7	71	170	9	57
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	174	0	483	171
Stage 1	-	-	-	-	171	-
Stage 2	-	-	-	-	312	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1403	-	542	873
Stage 1	-	-	-	-	859	-
Stage 2	-	-	-	-	742	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1403	-	512	873
Mov Cap-2 Maneuver	-	-	-	-	512	-
Stage 1	-	-	-	-	859	-
Stage 2	-	-	-	-	700	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		2.3		9.9	
HCM LOS					A	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	798	-	-	1403	-	
HCM Lane V/C Ratio	0.082	-	-	0.05	-	
HCM Control Delay (s)	9.9	-	-	7.7	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0.3	-	-	0.2	-	




HCM 6th TWSC
12: Revilo Avenue & Colin Drive/Site Driveway

2023 No-Build
Weekday Morning Peak Hour

Intersection												
Int Delay, s/veh	3.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	15	7	3	4	8	8	6	50	13	5	31	13
Future Vol, veh/h	15	7	3	4	8	8	6	50	13	5	31	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	7	2	2	8	2
Mvmt Flow	16	8	3	4	9	9	7	54	14	5	34	14
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	135	133	41	132	133	61	48	0	0	68	0	0
Stage 1	51	51	-	75	75	-	-	-	-	-	-	-
Stage 2	84	82	-	57	58	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	836	758	1030	840	758	1004	1559	-	-	1533	-	-
Stage 1	962	852	-	934	833	-	-	-	-	-	-	-
Stage 2	924	827	-	955	847	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	817	752	1030	826	752	1004	1559	-	-	1533	-	-
Mov Cap-2 Maneuver	817	752	-	826	752	-	-	-	-	-	-	-
Stage 1	957	849	-	929	829	-	-	-	-	-	-	-
Stage 2	902	823	-	941	844	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	9.6		9.3			0.6			0.8			
HCM LOS	A		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1559	-	-	818	853	1533	-	-				
HCM Lane V/C Ratio	0.004	-	-	0.033	0.025	0.004	-	-				
HCM Control Delay (s)	7.3	0	-	9.6	9.3	7.4	0	-				
HCM Lane LOS	A	A	-	A	A	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	0.1	0	-	-				

HCM 6th TWSC
14: William Floyd Parkway & Site Driveway
























2023 No-Build
Weekday Morning Peak Hour

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	0	36	0	1280	781	20
Future Vol, veh/h	0	36	0	1280	781	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	20	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	5	7	2
Mvmt Flow	0	39	0	1391	849	22
Major/Minor	Minor2	Major1		Major2		
Conflicting Flow All	-	436	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-	-
Pot Cap-1 Maneuver	0	568	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	568	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB	NB		SB		
HCM Control Delay, s	11.8	0		0		
HCM LOS	B					
Minor Lane/Major Mvmt	NBT EBLn1		SBT	SBR		
Capacity (veh/h)	- 568		-	-		
HCM Lane V/C Ratio	- 0.069		-	-		
HCM Control Delay (s)	- 11.8		-	-		
HCM Lane LOS	- B		-	-		
HCM 95th %tile Q(veh)	- 0.2		-	-		

HCM 6th Signalized Intersection Summary

5: William Floyd Parkway & Moriches-Middle Island Road

2023 No-Build
Weekday Evening Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	64	136	67	106	118	375	56	885	129	604	1103	150
Future Volume (veh/h)	64	136	67	106	118	375	56	885	129	604	1103	150
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1900	1885	1870	1870	1796	1767	1826	1870	1900	1870	1885	1796
Adj Flow Rate, veh/h	65	137	68	107	119	0	57	894	0	610	1114	0
Peak Hour Factor	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99
Percent Heavy Veh, %	0	1	2	2	7	4	5	2	0	2	1	2
Cap, veh/h	295	258	128	221	390		73	1091		596	1521	
Arrive On Green	0.22	0.22	0.22	0.22	0.22	0.00	0.04	0.31	0.00	0.17	0.42	0.00
Sat Flow, veh/h	1293	1189	590	1177	1796	1498	1739	3554	1610	3456	3582	1522
Grp Volume(v), veh/h	65	0	205	107	119	0	57	894	0	610	1114	0
Grp Sat Flow(s),veh/h/ln	1293	0	1779	1177	1796	1498	1739	1777	1610	1728	1791	1522
Q Serve(g_s), s	3.7	0.0	8.6	7.4	4.7	0.0	2.7	19.6	0.0	14.5	21.8	0.0
Cycle Q Clear(g_c), s	8.4	0.0	8.6	16.0	4.7	0.0	2.7	19.6	0.0	14.5	21.8	0.0
Prop In Lane	1.00		0.33	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	295	0	386	221	390		73	1091		596	1521	
V/C Ratio(X)	0.22	0.00	0.53	0.48	0.30		0.78	0.82		1.02	0.73	
Avail Cap(c_a), veh/h	338	0	446	257	444		277	1378		596	1521	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	31.1	0.0	29.1	36.2	27.6	0.0	39.9	27.0	0.0	34.8	20.2	0.0
Incr Delay (d2), s/veh	0.4	0.0	1.1	1.6	0.4	0.0	16.5	3.2	0.0	43.1	1.9	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.2	0.0	3.7	2.2	2.0	0.0	1.4	8.0	0.0	9.2	8.4	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	31.5	0.0	30.3	37.8	28.0	0.0	56.4	30.2	0.0	77.9	22.1	0.0
LnGrp LOS	C	A	C	D	C		E	C		F	C	
Approach Vol, veh/h	270			226			A			951		
Approach Delay, s/veh	30.5			32.7			31.8			41.8		
Approach LOS	C			C			C			D		
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	22.0	36.6		25.5	12.1	46.5		25.5				
Change Period (Y+Rc), s	* 7.5	* 11		* 7.2	8.6	10.8		7.2				
Max Green Setting (Gmax), s	* 15	* 33		* 21	13.4	29.2		20.8				
Max Q Clear Time (g_c+I1), s	16.5	21.6		10.6	4.7	23.8		18.0				
Green Ext Time (p_c), s	0.0	4.2		1.0	0.1	3.1		0.3				

Intersection Summary

HCM 6th Ctrl Delay	37.2
HCM 6th LOS	D





Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Unsignalized Delay for [NBR, WBR, SBR] is excluded from calculations of the approach delay and intersection delay.




HCM 6th TWSC
6: Revilo Avenue & Moriches-Middle Island Road

2023 No-Build
Weekday Evening Peak Hour

Intersection												
Int Delay, s/veh	3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	2	182	7	107	165	6	11	0	52	3	0	1
Future Vol, veh/h	2	182	7	107	165	6	11	0	52	3	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	87	87	87	87	87	87	87	87	87	87	87	87
Heavy Vehicles, %	0	1	0	2	7	0	0	0	0	0	0	0
Mvmt Flow	2	209	8	123	190	7	13	0	60	3	0	1
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	197	0	0	217	0	0	657	660	213	687	661	194
Stage 1	-	-	-	-	-	-	217	217	-	440	440	-
Stage 2	-	-	-	-	-	-	440	443	-	247	221	-
Critical Hdwy	4.1	-	-	4.12	-	-	7.1	6.5	6.2	7.1	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Follow-up Hdwy	2.2	-	-	2.218	-	-	3.5	4	3.3	3.5	4	3.3
Pot Cap-1 Maneuver	1388	-	-	1353	-	-	381	386	832	364	385	853
Stage 1	-	-	-	-	-	-	790	727	-	600	581	-
Stage 2	-	-	-	-	-	-	600	579	-	761	724	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1388	-	-	1353	-	-	350	346	832	311	345	853
Mov Cap-2 Maneuver	-	-	-	-	-	-	350	346	-	311	345	-
Stage 1	-	-	-	-	-	-	788	726	-	599	522	-
Stage 2	-	-	-	-	-	-	538	520	-	705	723	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.1			3.1			11			14.9		
HCM LOS							B			B		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	671	1388	-	-	1353	-	-	370				
HCM Lane V/C Ratio	0.108	0.002	-	-	0.091	-	-	0.012				
HCM Control Delay (s)	11	7.6	0	-	7.9	0	-	14.9				
HCM Lane LOS	B	A	A	-	A	A	-	B				
HCM 95th %tile Q(veh)	0.4	0	-	-	0.3	-	-	0				





HCM 6th TWSC
9: Site Driveway & Moriches-Middle Island Road

2023 No-Build
Weekday Evening Peak Hour

Intersection						
Int Delay, s/veh	1.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	228	9	51	273	5	39
Future Vol, veh/h	228	9	51	273	5	39
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	1	2	2	5	2	2
Mvmt Flow	248	10	55	297	5	42
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	258	0	660	253
Stage 1	-	-	-	-	253	-
Stage 2	-	-	-	-	407	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1307	-	428	786
Stage 1	-	-	-	-	789	-
Stage 2	-	-	-	-	672	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1307	-	407	786
Mov Cap-2 Maneuver	-	-	-	-	407	-
Stage 1	-	-	-	-	789	-
Stage 2	-	-	-	-	638	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		1.2		10.4	
HCM LOS					B	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	711	-	-	1307	-	
HCM Lane V/C Ratio	0.067	-	-	0.042	-	
HCM Control Delay (s)	10.4	-	-	7.9	0	
HCM Lane LOS	B	-	-	A	A	
HCM 95th %tile Q(veh)	0.2	-	-	0.1	-	

HCM 6th TWSC
12: Revilo Avenue & Colin Drive/Site Driveway

2023 No-Build
Weekday Evening Peak Hour

Intersection												
Int Delay, s/veh	1.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	13	5	4	3	4	3	5	47	7	3	79	32
Future Vol, veh/h	13	5	4	3	4	3	5	47	7	3	79	32
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	2	0	2	2	2	0	0	2	2	1	3
Mvmt Flow	14	5	4	3	4	3	5	51	8	3	86	35
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	179	179	104	179	192	55	121	0	0	59	0	0
Stage 1	110	110	-	65	65	-	-	-	-	-	-	-
Stage 2	69	69	-	114	127	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.52	6.2	7.12	6.52	6.22	4.1	-	-	4.12	-	-
Critical Hdwy Stg 1	6.1	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4.018	3.3	3.518	4.018	3.318	2.2	-	-	2.218	-	-
Pot Cap-1 Maneuver	787	715	956	783	703	1012	1479	-	-	1545	-	-
Stage 1	900	804	-	946	841	-	-	-	-	-	-	-
Stage 2	946	837	-	891	791	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	778	711	956	772	699	1012	1479	-	-	1545	-	-
Mov Cap-2 Maneuver	778	711	-	772	699	-	-	-	-	-	-	-
Stage 1	897	802	-	943	838	-	-	-	-	-	-	-
Stage 2	935	834	-	879	789	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	9.7		9.6		0.6		0.2					
HCM LOS	A		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1479	-	-	788	795	1545	-	-				
HCM Lane V/C Ratio	0.004	-	-	0.03	0.014	0.002	-	-				
HCM Control Delay (s)	7.4	0	-	9.7	9.6	7.3	0	-				
HCM Lane LOS	A	A	-	A	A	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.1	0	0	-	-				

HCM 6th TWSC
14: William Floyd Parkway & Site Driveway


























2023 No-Build
Weekday Evening Peak Hour

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		↗		↑↑↑	↑↑	
Traffic Vol, veh/h	0	27	0	1070	1261	15
Future Vol, veh/h	0	27	0	1070	1261	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	20	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	1	2
Mvmt Flow	0	29	0	1163	1371	16
Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	694	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-	-
Pot Cap-1 Maneuver	0	385	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	385	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB	NB	SB			
HCM Control Delay, s	15.1	0	0			
HCM LOS	C					
Minor Lane/Major Mvmt	NBT	EBLn1	SBT	SBR		
Capacity (veh/h)	-	385	-	-		
HCM Lane V/C Ratio	-	0.076	-	-		
HCM Control Delay (s)	-	15.1	-	-		
HCM Lane LOS	-	C	-	-		
HCM 95th %tile Q(veh)	-	0.2	-	-		

HCM 6th Signalized Intersection Summary

5: William Floyd Parkway & Moriches-Middle Island Road

2023 Build
Weekday Morning Peak Hour

													
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations										 	 		
Traffic Volume (veh/h)	110	67	41	93	91	481	77	1129	76	216	673	62	
Future Volume (veh/h)	110	67	41	93	91	481	77	1129	76	216	673	62	
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0	
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Work Zone On Approach	No			No			No			No			
Adj Sat Flow, veh/h/ln	1826	1752	1500	1781	1870	1767	1737	1826	1796	1811	1811	1596	
Adj Flow Rate, veh/h	121	74	45	102	100	0	85	1241	0	237	740	0	
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	
Percent Heavy Veh, %	5	10	27	8	2	4	11	5	7	6	6	16	
Cap, veh/h	262	183	111	234	336		107	1350		330	1406		
Arrive On Green	0.18	0.18	0.18	0.18	0.18	0.00	0.06	0.39	0.00	0.10	0.41	0.00	
Sat Flow, veh/h	1264	1020	620	1212	1870	1498	1654	3469	1522	3346	3441	1353	
Grp Volume(v), veh/h	121	0	119	102	100	0	85	1241	0	237	740	0	
Grp Sat Flow(s),veh/h/ln	1264	0	1640	1212	1870	1498	1654	1735	1522	1673	1721	1353	
Q Serve(g_s), s	7.0	0.0	4.9	6.2	3.6	0.0	3.9	26.1	0.0	5.3	12.4	0.0	
Cycle Q Clear(g_c), s	10.6	0.0	4.9	11.2	3.6	0.0	3.9	26.1	0.0	5.3	12.4	0.0	
Prop In Lane	1.00		0.38	1.00		1.00	1.00		1.00	1.00		1.00	
Lane Grp Cap(c), veh/h	262	0	295	234	336		107	1350		330	1406		
V/C Ratio(X)	0.46	0.00	0.40	0.44	0.30		0.79	0.92		0.72	0.53		
Avail Cap(c_a), veh/h	449	0	537	408	605		246	1384		545	1406		
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	
Uniform Delay (d), s/veh	31.8	0.0	27.8	32.7	27.3	0.0	35.3	22.3	0.0	33.5	17.1	0.0	
Incr Delay (d2), s/veh	1.3	0.0	0.9	1.3	0.5	0.0	12.2	10.0	0.0	2.9	0.4	0.0	
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
%ile BackOfQ(50%),veh/ln	2.2	0.0	1.9	1.9	1.6	0.0	1.8	11.0	0.0	2.1	4.3	0.0	
Unsig. Movement Delay, s/veh													
LnGrp Delay(d),s/veh	33.1	0.0	28.7	34.0	27.7	0.0	47.5	32.3	0.0	36.5	17.5	0.0	
LnGrp LOS	C	A	C	C	C		D	C		D	B		
Approach Vol, veh/h	240				202		A	1326		A	977		A
Approach Delay, s/veh	30.9				30.9			33.2			22.1		
Approach LOS	C				C			C			C		
Timer - Assigned Phs	1	2		4	5	6		8					
Phs Duration (G+Y+Rc), s	15.1	40.6		21.0	13.6	42.1		21.0					
Change Period (Y+Rc), s	* 7.5	* 11		* 7.2	8.6	10.8		7.2					
Max Green Setting (Gmax), s	* 13	* 31		* 25	11.4	27.2		24.8					
Max Q Clear Time (g_c+I1), s	7.3	28.1		12.6	5.9	14.4		13.2					
Green Ext Time (p_c), s	0.3	1.8		0.8	0.1	3.8		0.6					

Intersection Summary

HCM 6th Ctrl Delay	28.9
HCM 6th LOS	C

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Unsignalized Delay for [NBR, WBR, SBR] is excluded from calculations of the approach delay and intersection delay.





HCM 6th TWSC
6: Revilo Avenue & Moriches-Middle Island Road

2023 Build
Weekday Morning Peak Hour

Intersection												
Int Delay, s/veh	4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	96	5	122	122	0	8	0	65	1	0	0
Future Vol, veh/h	0	96	5	122	122	0	8	0	65	1	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	18	20	7	14	2	12	2	8	0	2	2
Mvmt Flow	0	102	5	130	130	0	9	0	69	1	0	0
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	130	0	0	107	0	0	495	495	105	529	497	130
Stage 1	-	-	-	-	-	-	105	105	-	390	390	-
Stage 2	-	-	-	-	-	-	390	390	-	139	107	-
Critical Hdwy	4.12	-	-	4.17	-	-	7.22	6.52	6.28	7.1	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.22	5.52	-	6.1	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.22	5.52	-	6.1	5.52	-
Follow-up Hdwy	2.218	-	-	2.263	-	-	3.608	4.018	3.372	3.5	4.018	3.318
Pot Cap-1 Maneuver	1455	-	-	1453	-	-	469	476	933	463	475	920
Stage 1	-	-	-	-	-	-	877	808	-	638	608	-
Stage 2	-	-	-	-	-	-	614	608	-	869	807	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1455	-	-	1453	-	-	435	430	933	397	429	920
Mov Cap-2 Maneuver	-	-	-	-	-	-	435	430	-	397	429	-
Stage 1	-	-	-	-	-	-	877	808	-	638	550	-
Stage 2	-	-	-	-	-	-	555	550	-	805	807	-
Approach	EB		WB				NB		SB			
HCM Control Delay, s	0		3.9				9.8		14.1			
HCM LOS							A		B			
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	829	1455	-	-	1453	-	-	397				
HCM Lane V/C Ratio	0.094	-	-	-	0.089	-	-	0.003				
HCM Control Delay (s)	9.8	0	-	-	7.7	0	-	14.1				
HCM Lane LOS	A	A	-	-	A	A	-	B				
HCM 95th %tile Q(veh)	0.3	0	-	-	0.3	-	-	0				

HCM 6th TWSC
9: Site Driveway & Moriches-Middle Island Road

2023 Build
Weekday Morning Peak Hour

Intersection						
Int Delay, s/veh	1.7					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	150	12	0	230	14	68
Future Vol, veh/h	150	12	0	230	14	68
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	11	2	2	13	2	2
Mvmt Flow	163	13	0	250	15	74
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	-	-	420	170
Stage 1	-	-	-	-	170	-
Stage 2	-	-	-	-	250	-
Critical Hdwy	-	-	-	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	-	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	0	-	590	874
Stage 1	-	-	0	-	860	-
Stage 2	-	-	0	-	792	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	-	-	590	874
Mov Cap-2 Maneuver	-	-	-	-	590	-
Stage 1	-	-	-	-	860	-
Stage 2	-	-	-	-	792	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		0		10	
HCM LOS					B	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT		
Capacity (veh/h)	808	-	-	-		
HCM Lane V/C Ratio	0.11	-	-	-		
HCM Control Delay (s)	10	-	-	-		
HCM Lane LOS	B	-	-	-		
HCM 95th %tile Q(veh)	0.4	-	-	-		

HCM 6th TWSC
12: Revilo Avenue & Colin Drive/Site Driveway

2023 Build
Weekday Morning Peak Hour

Intersection												
Int Delay, s/veh	5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	15	10	3	5	11	9	6	49	15	83	31	13
Future Vol, veh/h	15	10	3	5	11	9	6	49	15	83	31	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	7	2	2	8	2
Mvmt Flow	16	11	3	5	12	10	7	53	16	90	34	14

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	307	304	41	303	303	61	48	0	0	69	0	0
Stage 1	221	221	-	75	75	-	-	-	-	-	-	-
Stage 2	86	83	-	228	228	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	645	609	1030	649	610	1004	1559	-	-	1532	-	-
Stage 1	781	720	-	934	833	-	-	-	-	-	-	-
Stage 2	922	826	-	775	715	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	597	569	1030	606	570	1004	1559	-	-	1532	-	-
Mov Cap-2 Maneuver	597	569	-	606	570	-	-	-	-	-	-	-
Stage 1	777	677	-	929	829	-	-	-	-	-	-	-
Stage 2	895	822	-	715	672	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	11.2		10.5		0.6		4.9	
HCM LOS	B		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1559	-	-	614	685	1532	-
HCM Lane V/C Ratio	0.004	-	-	0.05	0.04	0.059	-
HCM Control Delay (s)	7.3	0	-	11.2	10.5	7.5	0
HCM Lane LOS	A	A	-	B	B	A	A
HCM 95th %tile Q(veh)	0	-	-	0.2	0.1	0.2	-

HCM 6th TWSC
14: William Floyd Parkway & Site Driveway
























2023 Build
Weekday Morning Peak Hour

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		↗		↑↑↑	↑↑	
Traffic Vol, veh/h	0	56	0	1282	763	44
Future Vol, veh/h	0	56	0	1282	763	44
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	20	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	5	7	2
Mvmt Flow	0	61	0	1393	829	48
Major/Minor	Minor2	Major1		Major2		
Conflicting Flow All	-	439	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-	-
Pot Cap-1 Maneuver	0	566	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	566	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	EB	NB		SB		
HCM Control Delay, s	12.1	0		0		
HCM LOS	B					
Minor Lane/Major Mvmt	NBT EBLn1		SBT	SBR		
Capacity (veh/h)	- 566		-	-		
HCM Lane V/C Ratio	- 0.108		-	-		
HCM Control Delay (s)	- 12.1		-	-		
HCM Lane LOS	- B		-	-		
HCM 95th %tile Q(veh)	- 0.4		-	-		

HCM 6th Signalized Intersection Summary

5: William Floyd Parkway & Moriches-Middle Island Road

2023 Build
Weekday Evening Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	73	143	67	106	125	375	59	884	129	604	1111	150
Future Volume (veh/h)	73	143	67	106	125	375	59	884	129	604	1111	150
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1900	1885	1870	1870	1796	1767	1826	1870	1900	1870	1885	1796
Adj Flow Rate, veh/h	74	144	68	107	126	0	60	893	0	610	1122	0
Peak Hour Factor	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99
Percent Heavy Veh, %	0	1	2	2	7	4	5	2	0	2	1	2
Cap, veh/h	293	267	126	220	396		77	1089		593	1506	
Arrive On Green	0.22	0.22	0.22	0.22	0.22	0.00	0.04	0.31	0.00	0.17	0.42	0.00
Sat Flow, veh/h	1285	1211	572	1170	1796	1498	1739	3554	1610	3456	3582	1522
Grp Volume(v), veh/h	74	0	212	107	126	0	60	893	0	610	1122	0
Grp Sat Flow(s),veh/h/ln	1285	0	1782	1170	1796	1498	1739	1777	1610	1728	1791	1522
Q Serve(g_s), s	4.3	0.0	8.9	7.5	5.0	0.0	2.9	19.7	0.0	14.5	22.4	0.0
Cycle Q Clear(g_c), s	9.3	0.0	8.9	16.4	5.0	0.0	2.9	19.7	0.0	14.5	22.4	0.0
Prop In Lane	1.00		0.32	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	293	0	393	220	396		77	1089		593	1506	
V/C Ratio(X)	0.25	0.00	0.54	0.49	0.32		0.78	0.82		1.03	0.74	
Avail Cap(c_a), veh/h	330	0	445	250	442		276	1370		593	1506	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	31.5	0.0	29.1	36.4	27.6	0.0	40.0	27.2	0.0	35.0	20.7	0.0
Incr Delay (d2), s/veh	0.4	0.0	1.1	1.7	0.5	0.0	15.6	3.3	0.0	44.7	2.1	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.4	0.0	3.8	2.2	2.1	0.0	1.5	8.1	0.0	9.4	8.7	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	32.0	0.0	30.3	38.1	28.1	0.0	55.6	30.5	0.0	79.8	22.7	0.0
LnGrp LOS	C	A	C	D	C		E	C		F	C	
Approach Vol, veh/h	286				233		A		953		A	
Approach Delay, s/veh	30.7				32.7				32.1		42.8	
Approach LOS	C				C				C		D	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	22.0	36.7		25.9	12.3	46.4		25.9				
Change Period (Y+Rc), s	* 7.5	* 11		* 7.2	8.6	10.8		7.2				
Max Green Setting (Gmax), s	* 15	* 33		* 21	13.4	29.2		20.8				
Max Q Clear Time (g_c+I1), s	16.5	21.7		11.3	4.9	24.4		18.4				
Green Ext Time (p_c), s	0.0	4.2		1.0	0.1	2.9		0.2				

Intersection Summary

HCM 6th Ctrl Delay 37.8
HCM 6th LOS D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Unsignalized Delay for [NBR, WBR, SBR] is excluded from calculations of the approach delay and intersection delay.




HCM 6th TWSC
6: Revilo Avenue & Moriches-Middle Island Road

2023 Build
Weekday Evening Peak Hour

Intersection												
Int Delay, s/veh	3.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	2	186	7	172	168	6	11	0	52	3	0	1
Future Vol, veh/h	2	186	7	172	168	6	11	0	52	3	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	87	87	87	87	87	87	87	87	87	87	87	87
Heavy Vehicles, %	0	1	0	2	7	0	0	0	0	0	0	0
Mvmt Flow	2	214	8	198	193	7	13	0	60	3	0	1
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	200	0	0	222	0	0	815	818	218	845	819	197
Stage 1	-	-	-	-	-	-	222	222	-	593	593	-
Stage 2	-	-	-	-	-	-	593	596	-	252	226	-
Critical Hdwy	4.1	-	-	4.12	-	-	7.1	6.5	6.2	7.1	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Follow-up Hdwy	2.2	-	-	2.218	-	-	3.5	4	3.3	3.5	4	3.3
Pot Cap-1 Maneuver	1384	-	-	1347	-	-	298	313	827	285	312	849
Stage 1	-	-	-	-	-	-	785	723	-	496	497	-
Stage 2	-	-	-	-	-	-	496	495	-	757	721	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	1384	-	-	1347	-	-	259	260	827	230	260	849
Mov Cap-2 Maneuver	-	-	-	-	-	-	259	260	-	230	260	-
Stage 1	-	-	-	-	-	-	783	722	-	495	414	-
Stage 2	-	-	-	-	-	-	413	413	-	701	720	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.1			4			11.8			18		
HCM LOS							B			C		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1				
Capacity (veh/h)	598	1384	-	-	1347	-	-	281				
HCM Lane V/C Ratio	0.121	0.002	-	-	0.147	-	-	0.016				
HCM Control Delay (s)	11.8	7.6	0	-	8.1	0	-	18				
HCM Lane LOS	B	A	A	-	A	A	-	C				
HCM 95th %tile Q(veh)	0.4	0	-	-	0.5	-	-	0.1				

HCM 6th TWSC
9: Site Driveway & Moriches-Middle Island Road

2023 Build
Weekday Evening Peak Hour

Intersection						
Int Delay, s/veh	1.2					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	224	17	0	334	12	59
Future Vol, veh/h	224	17	0	334	12	59
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	1	2	2	5	2	2
Mvmt Flow	243	18	0	363	13	64
Major/Minor	Major1	Major2		Minor1		
Conflicting Flow All	0	0	-	-	615	252
Stage 1	-	-	-	-	252	-
Stage 2	-	-	-	-	363	-
Critical Hdwy	-	-	-	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	-	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	0	-	455	787
Stage 1	-	-	0	-	790	-
Stage 2	-	-	0	-	704	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	-	-	455	787
Mov Cap-2 Maneuver	-	-	-	-	455	-
Stage 1	-	-	-	-	790	-
Stage 2	-	-	-	-	704	-
Approach	EB	WB		NB		
HCM Control Delay, s	0	0		10.8		
HCM LOS	B					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBT		
Capacity (veh/h)	701	-	-	-		
HCM Lane V/C Ratio	0.11	-	-	-		
HCM Control Delay (s)	10.8	-	-	-		
HCM Lane LOS	B	-	-	-		
HCM 95th %tile Q(veh)	0.4	-	-	-		

HCM 6th TWSC
12: Revilo Avenue & Colin Drive/Site Driveway

2023 Build
Weekday Evening Peak Hour

Intersection												
Int Delay, s/veh	3.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	13	9	4	5	8	4	5	46	10	68	79	32
Future Vol, veh/h	13	9	4	5	8	4	5	46	10	68	79	32
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	2	0	2	2	2	0	0	2	2	1	3
Mvmt Flow	14	10	4	5	9	4	5	50	11	74	86	35

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	324	323	104	325	335	56	121	0	0	61	0	0
Stage 1	252	252	-	66	66	-	-	-	-	-	-	-
Stage 2	72	71	-	259	269	-	-	-	-	-	-	-
Critical Hdwy	7.1	6.52	6.2	7.12	6.52	6.22	4.1	-	-	4.12	-	-
Critical Hdwy Stg 1	6.1	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.1	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4.018	3.3	3.518	4.018	3.318	2.2	-	-	2.218	-	-
Pot Cap-1 Maneuver	633	595	956	628	585	1011	1479	-	-	1542	-	-
Stage 1	757	698	-	945	840	-	-	-	-	-	-	-
Stage 2	943	836	-	746	687	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	596	562	956	590	552	1011	1479	-	-	1542	-	-
Mov Cap-2 Maneuver	596	562	-	590	552	-	-	-	-	-	-	-
Stage 1	754	662	-	941	837	-	-	-	-	-	-	-
Stage 2	925	833	-	694	651	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	11.1		10.9		0.6		2.8	
HCM LOS	B		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1479	-	-	619	631	1542	-
HCM Lane V/C Ratio	0.004	-	-	0.046	0.029	0.048	-
HCM Control Delay (s)	7.4	0	-	11.1	10.9	7.5	0
HCM Lane LOS	A	A	-	B	B	A	A
HCM 95th %tile Q(veh)	0	-	-	0.1	0.1	0.2	-

HCM 6th TWSC
14: William Floyd Parkway & Site Driveway

2023 Build
Weekday Evening Peak Hour

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations		↗		↑↑↑	↑↑	
Traffic Vol, veh/h	0	50	0	1072	1240	44
Future Vol, veh/h	0	50	0	1072	1240	44
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	20	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	1	2
Mvmt Flow	0	54	0	1165	1348	48

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	698	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-	-
Pot Cap-1 Maneuver	0	383	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	383	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	15.9	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT EBLn1	SBT	SBR
Capacity (veh/h)	- 383	-	-
HCM Lane V/C Ratio	- 0.142	-	-
HCM Control Delay (s)	- 15.9	-	-
HCM Lane LOS	- C	-	-
HCM 95th %tile Q(veh)	- 0.5	-	-

PLANS

241 CIRCLE 120188X. 18112 7.EVEN. 1481 WILLIAM FLOYD PARKWAY SHIRLEY NY 11967NGCAPD1K.18112.V3DNG

SURVEYOR'S DESCRIPTION - LOT 41.1, BLOCK 3

ALL THAT CERTAIN LOT, TRACT OR PARCEL OF LAND, WITH THE BUILDINGS AND IMPROVEMENTS THEREON ERECTED, SITUATE, LYING AND BEING IN THE HAMLET OF SHIRLEY, TOWN OF BROOKHAVEN, COUNTY OF SUFFOLK, STATE OF NEW JERSEY AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT, SAID POINT ON THE WESTERLY LINE OF WILLIAM FLOYD PARKWAY (150 FEET WIDE), SAID POINT ALSO BEING DISTANT 200 FEET SOUTHERLY FROM THE INTERSECTION OF SAID WESTERLY LINE OF WILLIAM PARKWAY AND THE SOUTHERLY LINE OF YORKICHES MIDDLE ISLAND ROAD (VARIABLE WIDTH), SAID POINT HAVING COORDINATES N: 245,664.46, E: 1,292,492.15 IN THE NEW JERSEY STATE PLANE COORDINATE SYSTEM, THENCE

1. ALONG SAID WESTERLY LINE OF WILLIAM FLOYD PARKWAY, SOUTH 20 DEGREES 26 MINUTES 03 SECONDS EAST, 200.00 FEET TO A POINT; THENCE
2. ALONG THE DIVIDING LINE OF LOT 41.1 AND LOTS 43 AND 42, BLOCK 3, SOUTH 69 DEGREES 43 MINUTES 32 SECONDS WEST, 221.55 FEET TO A POINT, SAID POINT BEING ON THE EASTERLY LINE OF REVILLO AVENUE (50 FEET WIDE); THENCE
3. ALONG SAID EASTERLY LINE OF REVILLO AVENUE, NORTH 20 DEGREES 16 MINUTES 27 SECONDS WEST, 200.00 FEET TO A POINT; THENCE
4. ALONG THE DIVIDING LINE OF LOT 41.1 AND 41.3, BLOCK 3, NORTH 69 DEGREES 43 MINUTES 33 SECONDS EAST, 220.99 FEET TO THE POINT AND PLACE OF BEGINNING.

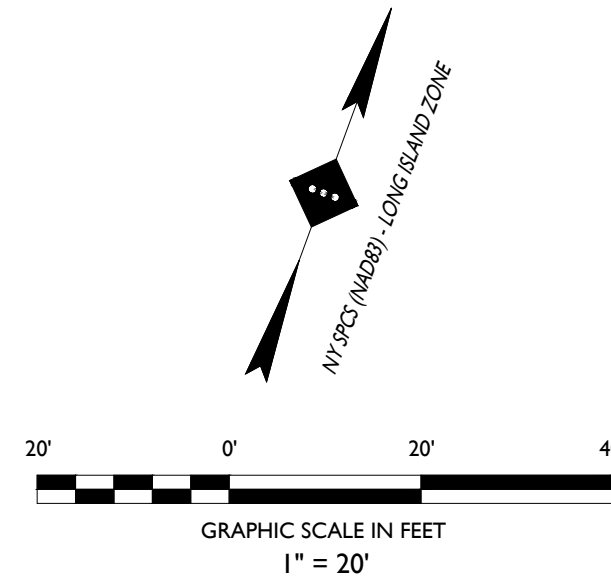
HAVING AN AREA OF 44,253 SQUARE FEET, 1.016 ACRES MORE OR LESS.

FLOOD NOTE

BY GRAPHIC PLOTTING ONLY, THIS PROPERTY IS IN ZONE X (UNSHADED)(AREA OF MINIMAL FLOOD HAZARD) OF THE FLOOD INSURANCE RATE MAP, COMMUNITY NO. 365334 MAP NUMBER 36103C0710H WHICH BEARS AN EFFECTIVE DATE OF SEPTEMBER 25, 2009.

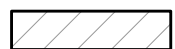


SOURCE: GOOGLE MAPS VICINITY MAP NOT TO SCALE



SYMBOL

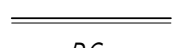
DESCRIPTION



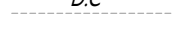
BUILDING



CONCRETE SIDEWALK / MAT



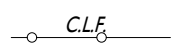
ASPHALT / CONCRETE CURB



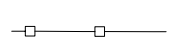
DEPRESSED CURB



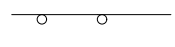
PROPERTY LINE



ADJACENT PROPERTY LINE



CHAIN LINK FENCE



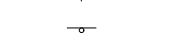
BOARD-ON-BOARD FENCE



GUIDE RAIL



MAST ARM LIGHT POLE



POLE



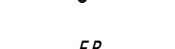
LIGHT POLE



SIGNS



DRAIN



INLET



MAN HOLE



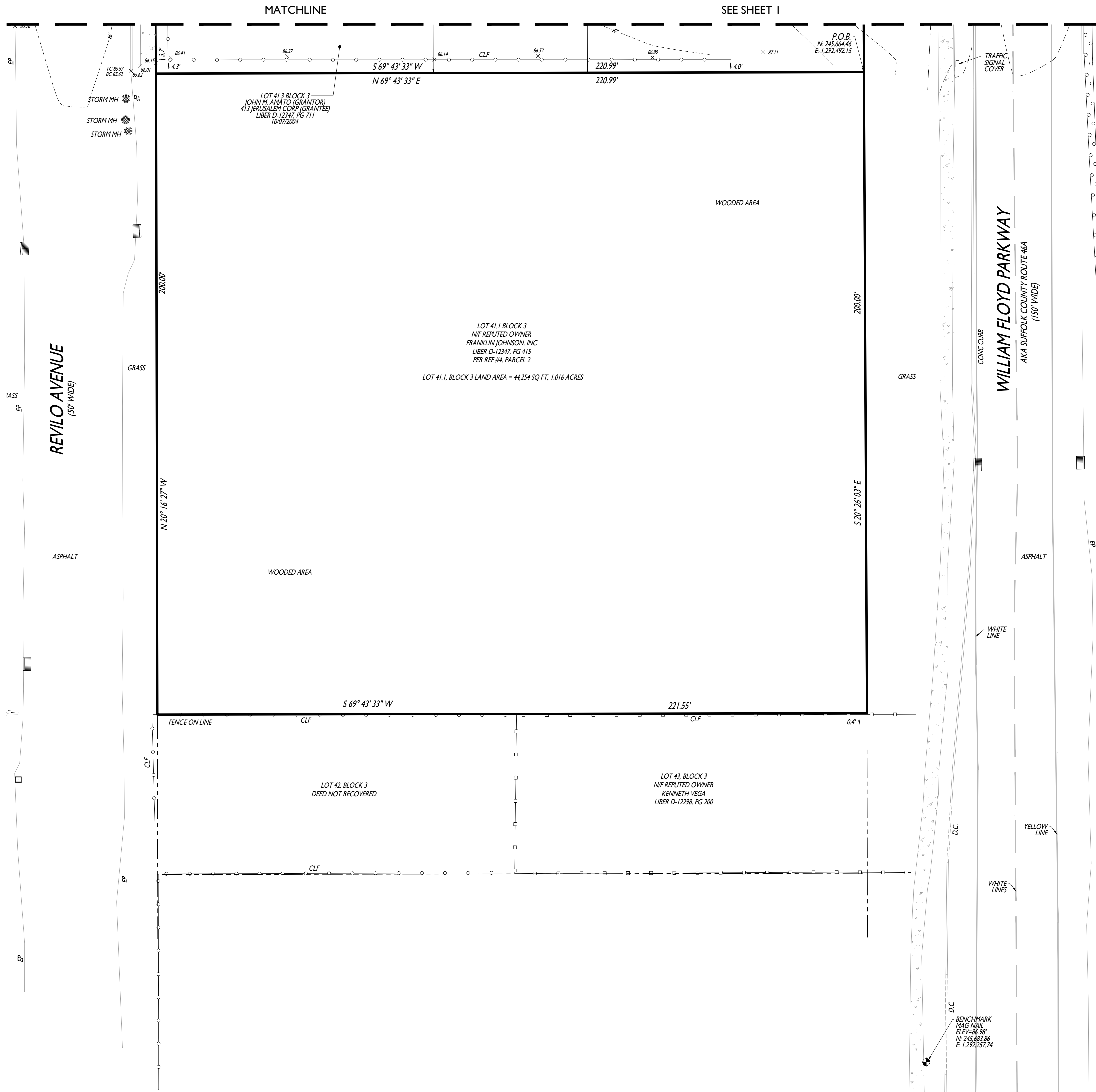
BOLLARDS



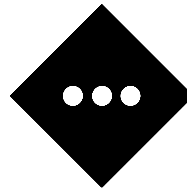
EDGE OF PAVEMENT



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BOUNDARY & PARTIAL TOPOGRAPHIC SURVEY

MAP OF SURVEY OF:

1481 WILLIAM FLOYD PARKWAY - SEC 642, BLK 3, LOTS 41.3, 41.4 & 41.5
NO ADDRESS - SEC 642, BLK 3, LOT 41.1
HAMLET OF SHIRLEY, TOWN OF BROOKHAVEN
COUNTY OF SUFFOLK, STATE OF NEW YORK

ISSUE	DATE	BY	FIELD DATE	FIELD CREW	DESCRIPTION
3	4/22/2022	DJS	4/21/2022	AW	ADD LOT 41.1, BLK 3 BOUNDARY
2	9/02/2021	AG	-	-	ADD P.O.B. FOR DESCRIPTION
1	5/25/2021	DJS	05/04/2021	AW	FOR ISSUE

SUBJECT TO RESTRICTIONS, EASEMENTS OF RECORD AND OTHER FACTS WHICH A CURRENT TITLE SEARCH MIGHT DISCLOSE.



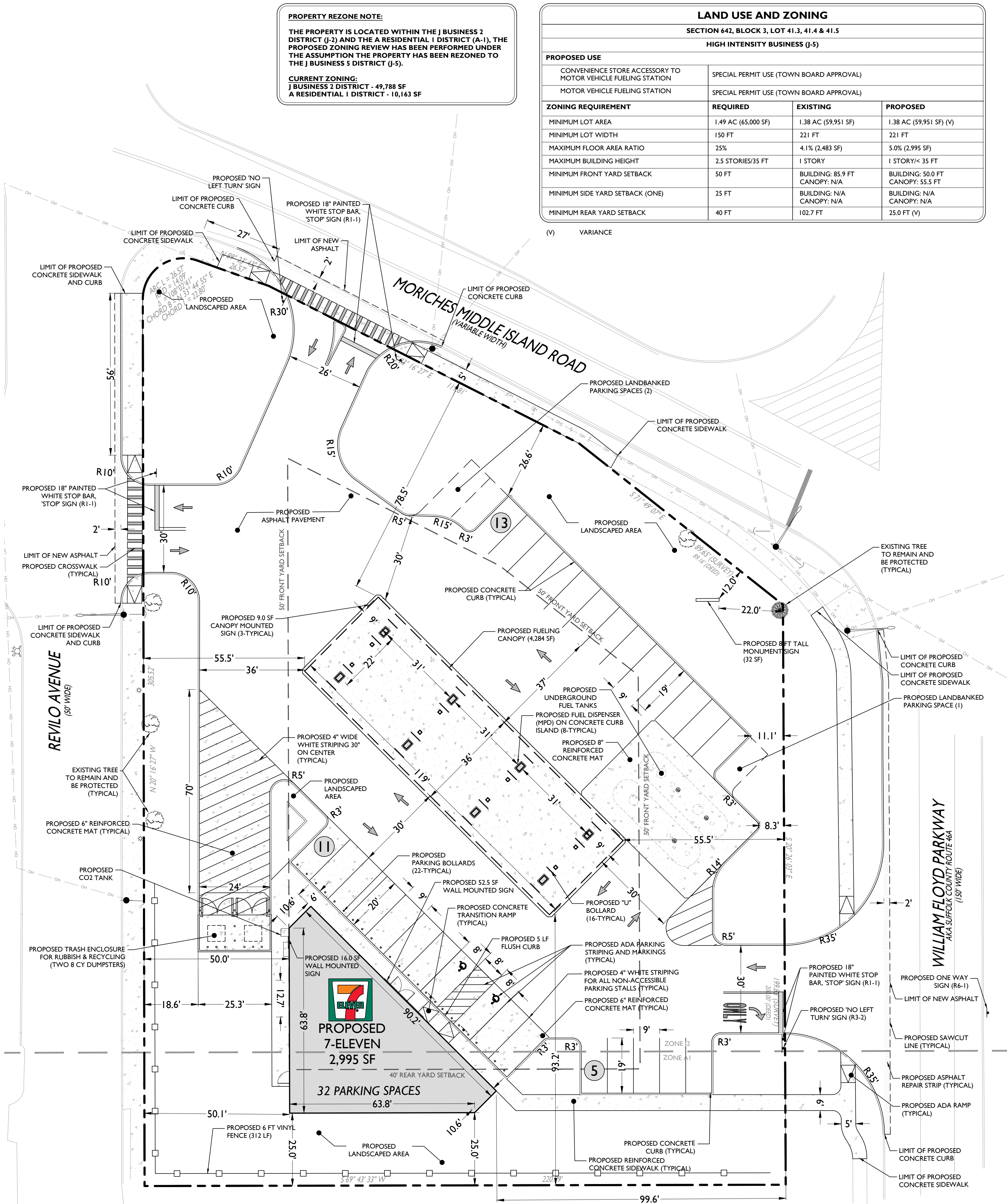
THOMAS F. MILLER
NEW YORK LAND SURVEYOR No. 050484
LICENSED PROFESSIONAL LAND SURVEYOR
NEW YORK CERTIFICATE OF AUTHORIZATION No. 15155

SCALE: 1"=20'

PROJECT ID: K-18112

SHEET: 2 OF 2

K:\2024\843.1\7-ELEVEN - 488 WILLIAM FLOYD PARKWAY, SHIRLEY, NY\CD\NOT FOR CONSTRUCTION



OFF-STREET PARKING REQUIREMENTS		
CODE SECTION	REQUIRED	PROPOSED
§85-852	MINIMUM PARKING REQUIREMENT: CONVENIENCE STORE: 1 SPACE PER 100 SF OF GFA (1 SPACE / 100 SF) * (2,995 SF) = 30 SPACES MOTOR VEHICLE FUELING STATION: MINIMUM OF 2 STALLS TOTAL = 32 SPACES	32 SPACES (*)
§85-850	LOADING SPACE REQUIREMENT: 1 SPACE FOR BUILDING FLOOR AREA UNDER 8,000 SF	1 SPACE
§85-854.B	MINIMUM PARKING SPACE DIMENSIONS: LENGTH = 19 FT WIDTH = 9 FT	LENGTH = 19 FT WIDTH = 9 FT
§85-854.E	MINIMUM MANEUVERING AISLE WIDTH: TBD BY PLANNING BOARD	30 FT

(*) THREE (3) PARKING SPACES CURRENTLY SHOWN AS PROPOSED LANDBANKED PARKING

SIGNAGE REQUIREMENTS		
CODE SECTION	REQUIRED	PROPOSED
§ 57A-6.F	CANOPY SIGN REQUIREMENTS: MAXIMUM SIGN AREA = 1 SF PER LF OF CANOPY WIDTH (MAX. 12 SF) NORTH FACING = 119 SF EAST/WEST FACING = 32 SF	NORTH = 9 SF EAST/WEST = 9 SF EACH
§ 57A-6.E	BUILDING SIGN REQUIREMENTS: MAXIMUM SIGNS PER WALL = 1 SIGN MAXIMUM AREA OF LARGEST WALL SIGN = 2 SF PER LF (MAX. 40 SF/1)	NORTH FACADE 1 SIGN - 52.5 SF (V) WEST FACADE 1 SIGN - 16 SF
§ 57A-6.C	GROUND SIGN REQUIREMENTS: MAXIMUM NUMBER = 1 SIGN MAXIMUM SIGN AREA = 24 SF IN AREA PER SIGN FACE MAXIMUM HEIGHT = 12 FT IN HEIGHT ABOVE GROUND LEVEL	1 SIGN AREA = 32 SF EACH FACE (V) HEIGHT = 8 FT
§ 57A-12	SETBACK REQUIREMENTS: MINIMUM SIDE YARD SETBACK = 10 FT MINIMUM FRONT YARD SETBACK PER SPEED LIMIT < 30 MPH = 12 FT 31 - 40 MPH = 17 FT 41 - 55 MPH = 22 FT SIGN SHALL NOT BE LOCATED WITHIN THE TRIANGLE FORMED BY THE TWO PROPERTY LINES AND AN INTERSECTING LINE 30' FROM THE PROP. CORNER.	N/A MIDDLE ISLAND ROAD = 12.0 FT N/A WILLIAM FLOYD PKWY = 22.0 FT COMPLIES

(*) OTHER PERMITTED WALL SIGNS MAY NOT EXCEED 32 SF

(V) VARIANCE

MOTOR VEHICLE FUELING / ACCESSORY CONVENIENCE STORE REQUIREMENTS		
CODE SECTION	REQUIRED	PROPOSED
§85-467.B.1	APPLICANT MUST DEMONSTRATE A PLAN WHEREBY VEHICLES WISHING TO RECEIVE FUELING SERVICES CAN DO SO WITHOUT INCONVENIENCE.	COMPLIES
§85-467.B.2	ALL CONVENIENCE STORES SHALL HAVE PUBLIC RESTROOMS ATTACHED THERETO.	COMPLIES
§85-467.B.3	A MAXIMUM OF 750 SF OF BUILDING AREA SHALL BE DEVOTED TO THE RETAIL SALE AND DISPLAY AREA. TOTAL BUILDING AREA SHALL NOT EXCEED 1,500 SF (EXCEPT FOR APPROVED WAIVER)*	RETAIL & DISPLAY = ±1,850 SF BUILDING = 2,995 SF
§85-467.B.4	THE PLANNING BOARD SHALL APPROVE ALL SIGNS DISPLAYED AT THE SITE IN CONNECTION WITH THE CONVENIENCE STORE. PORTABLE AND MOBILE SIGNS ARE PROHIBITED.	WILL COMPLY
§85-467.B.5	OUTSIDE DISPLAY SHALL BE PROHIBITED.	WILL COMPLY
§85-467.B.6	A DUMPSTER ENCLOSURE FOR SEPARATE RUBBISH AND CARDBOARD RECYCLING SHALL BE PROVIDED. ENCLOSURE SHALL MEASURE 8 FT IN HEIGHT AND WALLS SHALL COMPLEMENT THE BUILDING FACADE.	COMPLIES
§85-467.B.7	BUFFERS AND PLANTINGS IN ACCORDANCE WITH THE LAND DEVELOPMENT STANDARDS, EXCEPT THAT THE REAR YARD SETBACK SHALL BE 40 FT. MORICHES-MIDDLE ISLAND ROAD = 25 FT REQUIRED	REAR YARD SETBACK: 25 FT (V) MMI ROAD = 26.6 FT
§85-467.B.8	A MINIMUM 6 FT TALL SOLID (OPAQUE) FENCE ALONG ANY PROPERTY LINE ADJACENT TO A RESIDENTIAL DISTRICT IS REQUIRED.	DOES NOT COMPLY (V)
§85-467.B.9	WASTE RECEPTACLES FOR CUSTOMER USE SHALL BE PROVIDED AND MAINTAINED ON SITE.	WILL COMPLY
§85-467.G.1	USE SHALL BE LIMITED TO RETAIL SALE OF MOTOR FUELS, LUBRICANTS AND OTHER MOTOR VEHICLE SUPPLIES.	COMPLIES
§85-467.G.2	NO REPAIR WORK SHALL BE PERFORMED IN THE OPEN. ALL REPAIR WORK, EXCLUDING EMERGENCY SERVICE, SHALL BE CONDUCTED ONLY BETWEEN THE HOURS OF 7:00 AM AND 9:00 PM.	N/A
§85-467.G.3	THE OVERNIGHT STORAGE OF REGISTERED VEHICLES SHALL ONLY BE PERMITTED WITH THE REVIEW AND APPROVAL OF THE TOWN BOARD AND THE ISSUANCE OF A SPECIAL PERMIT.	WILL COMPLY
§85-467.G.4	A FUELING STATION SHALL BE PROHIBITED WITHIN 500 FT OF ANY LOT LINE BOUNDING AN AREA OF PUBLIC ASSEMBLY.	COMPLIES
§85-467.G.5	THE PLANNING BOARD SHALL APPROVE ALL SIGNS DISPLAYED AT THE SITE IN CONNECTION WITH THE MOTOR VEHICLE FUELING STATION. PORTABLE AND MOBILE SIGNS ARE PROHIBITED.	WILL COMPLY
§85-467.G.6	OUTSIDE DISPLAY SHALL ONLY BE PERMITTED WITH THE REVIEW AND APPROVAL OF THE TOWN BOARD AND THE ISSUANCE OF A SPECIAL PERMIT.	WILL COMPLY
§85-467.G.7(a)	A MINIMUM OF ONE (1) PUBLIC FUEL PUMP ISLAND SHALL BE FULL SERVICE FROM 8:00AM THROUGH 8:00 PM ANY DAYS ON WHICH THE PUBLIC FUELING STATION IS OPEN FOR BUSINESS	COMPLIES
§85-467.G.8	CURB CUT REQUIREMENTS: MAXIMUM ONE CURB CUT PER 75 FT OF ROAD FRONTAGE 768 LF FRONTAGE * (1 PER 75 FT) = 10 CURB CUTS MINIMUM CURB CUT SEPARATION FROM INTERSECTION OR RESIDENTIAL PROPERTY = 25 FT MINIMUM WIDTH = 20 FT MAXIMUM WIDTH = 35 FT MINIMUM SEPARATION = 25 FT	3 CURB CUTS 27 FT COMPLIES COMPLIES N/A
§85-467.G.9	SCREENING, BUFFERS AND FENCING SHALL BE PROVIDED IN ACCORDANCE WITH THE LAND DEVELOPMENT STANDARDS.	DOES NOT COMPLY (V)

(V) VARIANCE
(*) THE BUILDING AREA OF THE ACCESSORY CONVENIENCE STORE CAN BE IN EXCESS OF 1,500SF THROUGH THE GRANT OF A TOWN BOARD SPECIAL PERMIT WAIVER AND THE REDEMPTION OF PINE BARRENS CREDITS. THE MAXIMUM PERMITTED BUILDING AREA OF THE CONVENIENCE STORE SHALL NOT EXCEED 3,000SF.

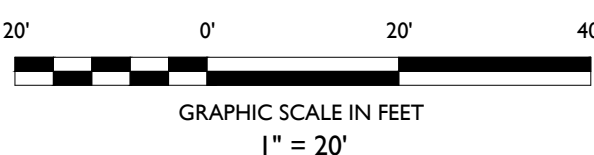
LAND DEVELOPMENT STANDARDS		
CODE SECTION	REQUIRED	PROPOSED
§85-843.A.1-4	MINIMUM PERCENT OF LANDSCAPING = 20% MINIMUM PERCENT OF LANDSCAPING IN FRONT YARD = 50% OF TOTAL STREET TREE SPACING = 30 FT MINIMUM LANDSCAPE WIDTH (STREET FRONTAGES) = 15 FT	38.72% (23,215 SF) 144.79% (17,361 SF) COMPLIES (SEE LANDSCAPE PLAN) 8.3 FT (W.M. FLOYD PKWY) (V)
§85-843.A.7	ALL PARKING AREAS SHALL BE SCREENED FROM VIEW WITH A HEDGE, BERM AND/OR DECORATIVE WALL OR FENCE.	COMPLIES
§85-843.B.1	MINIMUM RESIDENTIAL BUFFER = 25FT	25 FT

(V) VARIANCE
(*) THREE (3) PARKING SPACES CURRENTLY SHOWN AS PROPOSED LANDBANKED PARKING

SYMBOL	DESCRIPTION
---	PROPERTY LINE
- - - - -	SETBACK LINE
=====	PROPOSED CURB
=====	PROPOSED FLUSH CURB
○ ○ ○	PROPOSED SIGNS / BOLLARDS
■	PROPOSED BUILDING
□	PROPOSED CONCRETE
▬	PROPOSED RETAINING WALL
□	PROPOSED VINYL FENCE
⊕ ⊕ ⊕	PROPOSED TREES
⊗ ⊗ ⊗	PROPOSED SHRUBS
▨	PROPOSED GROUND COVER PLANTS

GENERAL NOTES

- THE CONTRACTOR SHALL VERIFY AND FAMILIARIZE THEMSELVES WITH THE EXISTING SITE CONDITIONS AND THE PROPOSED SCOPE OF WORK (INCLUDING DIMENSIONS, LAYOUT, ETC.) PRIOR TO INITIATING THE IMPROVEMENTS IDENTIFIED WITHIN THESE DOCUMENTS. SHOULD ANY DISCREPANCY BE FOUND BETWEEN THE EXISTING SITE CONDITIONS AND THE PROPOSED WORK, THE CONTRACTOR SHALL NOTIFY STONEFIELD ENGINEERING & DESIGN, LLC PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND ENSURE THAT ALL REQUIRED APPROVALS HAVE BEEN OBTAINED PRIOR TO THE START OF CONSTRUCTION. COPIES OF ALL REQUIRED PERMITS AND APPROVALS SHALL BE KEPT ON SITE AT ALL TIMES DURING CONSTRUCTION.
- ALL CONTRACTORS WILL, TO THE FULLEST EXTENT PERMITTED BY LAW, INDEMNIFY AND HOLD HARMLESS STONEFIELD ENGINEERING & DESIGN, LLC, AND ITS SUB-CONSULTANTS FROM AND AGAINST ANY DAMAGES AND LIABILITIES INCLUDING ATTORNEY'S FEES ARISING OUT OF CLAIMS BY EMPLOYEES OF THE CONTRACTOR IN ADDITION TO CLAIMS CONNECTED TO THE PROJECT AS A RESULT OF NOT CARRYING THE PROPER INSURANCE FOR WORKERS COMPENSATION, LIABILITY INSURANCE, AND LIMITS OF COMMERCIAL GENERAL LIABILITY INSURANCE.
- THE CONTRACTOR SHALL NOT DEVIATE FROM THE PROPOSED IMPROVEMENTS IDENTIFIED WITHIN THIS PLAN SET UNLESS APPROVAL IS PROVIDED IN WRITING BY STONEFIELD ENGINEERING & DESIGN, LLC.
- THE CONTRACTOR IS RESPONSIBLE TO DETERMINE THE MEANS AND METHODS OF CONSTRUCTION.
- THE CONTRACTOR SHALL NOT PERFORM ANY WORK OR CAUSE DISTURBANCE ON A PRIVATE PROPERTY NOT CONTROLLED BY THE PERSON OR ENTITY WHO HAS AUTHORIZED THE WORK WITHOUT PRIOR WRITTEN CONSENT FROM THE OWNER OF THE PRIVATE PROPERTY.
- THE CONTRACTOR IS RESPONSIBLE TO RESTORE ANY DAMAGED OR UNDERMINED STRUCTURE OR SITE FEATURE THAT IS IDENTIFIED TO REMAIN ON THE PLAN SET. ALL REPAIRS SHALL USE NEW MATERIALS TO RESTORE THE FEATURE TO ITS EXISTING CONDITION AT THE CONTRACTOR'S EXPENSE.
- CONTRACTOR IS RESPONSIBLE TO PROVIDE THE APPROPRIATE SHOP DRAWINGS, PRODUCT DATA, AND OTHER REQUIRED SUBMITTALS FOR REVIEW. STONEFIELD ENGINEERING & DESIGN, LLC, WILL REVIEW THE SUBMITTALS IN ACCORDANCE WITH THE DESIGN INTENT AS REFLECTED WITHIN THE PLAN SET.
- THE CONTRACTOR IS RESPONSIBLE FOR TRAFFIC CONTROL IN ACCORDANCE WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- THE CONTRACTOR IS REQUIRED TO PERFORM ALL WORK IN THE PUBLIC RIGHT-OF-WAY IN ACCORDANCE WITH THE APPROPRIATE GOVERNING AUTHORITY AND SHALL BE RESPONSIBLE FOR THE PROCUREMENT OF STREET OPENING PERMITS.
- THE CONTRACTOR IS REQUIRED TO RETAIN AN OSHA CERTIFIED SAFETY INSPECTOR TO BE PRESENT ON SITE AT ALL TIMES DURING CONSTRUCTION & DEMOLITION ACTIVITIES.
- SHOULD AN EMPLOYEE OF STONEFIELD ENGINEERING & DESIGN, LLC, BE PRESENT ON SITE AT ANY TIME DURING CONSTRUCTION, IT DOES NOT RELIEVE THE CONTRACTOR OF ANY OF THE RESPONSIBILITIES AND REQUIREMENTS LISTED IN THE NOTES WITHIN THIS PLAN SET.



PLANNING DIVISION RESUBMISSION
PLANNING DIVISION RESUBMISSION
UPDATED PER TOWN AND COUNTY COMMENTS
ISSUED FOR REVIEW

BR
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SITE PLANS

7-ELEVEN, INC
PROPOSED CONVENIENCE STORE
WITH FUEL SALES

SECTION 642, BLOCK 3, LOT 41.3, 41.4 & 41.5
1481 WILLIAM FLOYD PARKWAY
HAMLET OF SHIRLEY, TOWN OF BROOKHAVEN
SUFFOLK COUNTY, NEW YORK

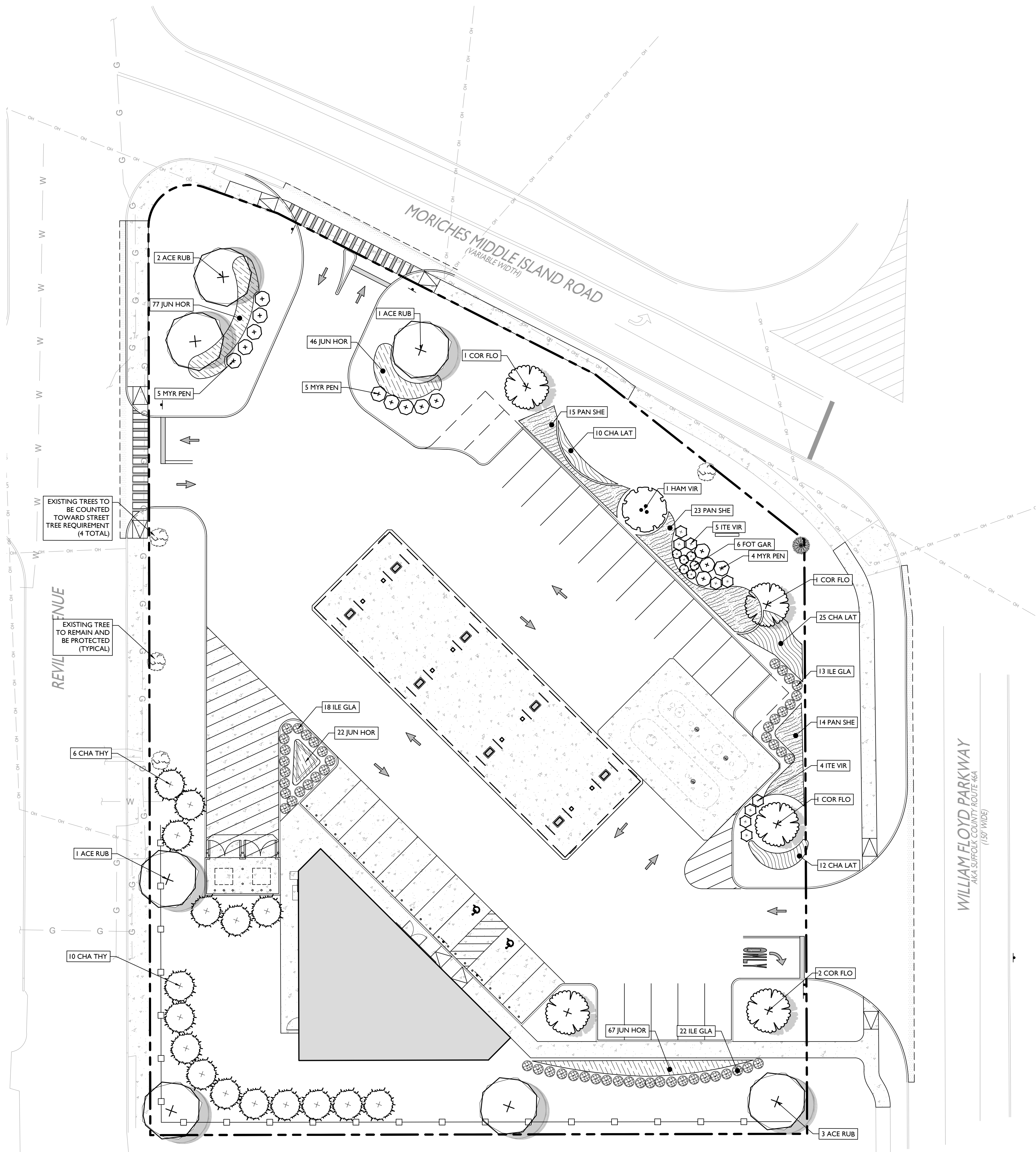
STATE OF NEW YORK
ZACHARY E. SHAPLIN, P.E.
NEW YORK LICENSE No. 99748
LICENSED PROFESSIONAL ENGINEER

STONEFIELD
engineering & design

SCALE: 1" = 20' PROJECT ID: K-18112

TITLE: SITE PLAN

DRAWING: C-4



PLANT SCHEDULE						
DECIDUOUS TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	ACE RUB	7	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY RED MAPLE	4" - 4.5" CAL	B&B
	HAM VIR	1	HAMAMELIS VIRGINIANA	COMMON WITCH HAZEL	4" - 4.5" CAL	B&B
	COR FLO	5	CORNUS FLORIDA	DOGWOOD	4" - 4.5" CAL	B&B
EVERGREEN TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	CHA THY	16	CHAMAECYPARIS THYOIDES	ATLANTIC WHITE CEDAR	2" - 2.5" CAL	B&B
SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	FOT GAR	6	FOTHERGILLA GARDENII	DWARF FOTHERGILLA	24" - 30"	POT
	ITE VIR	9	ITEA VIRGINICA	VIRGINIA WILLOW	24" - 30"	POT
	MYR PEN	14	MYRICA PENSYLVANICA	NORTHERN BAYBERRY	24" - 30"	POT
EVERGREEN SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
	ILE GLA	53	ILEX GLABRA	INKBERRY HOLLY	24" - 30"	POT
SHRUB AREAS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SPACING	CONTAINER
	CHA LAT	47	CHASMANTHIUM LATIFOLIUM	NORTHERN SEA OATS	36" o.c.	POT
	JUN HOR	212	JUNIPERUS HORIZONTALIS	CREeping JUNIPER	24" o.c.	POT
	PAN SHE	52	PANICUM VIRGATUM 'SHENANDOAH'	SWITCH GRASS	48" o.c.	POT

NOTE: IF ANY DISCREPANCIES OCCUR BETWEEN AMOUNTS SHOWN ON THE LANDSCAPE PLAN AND WITHIN THE PLANT LIST, THE PLAN SHALL DICTATE.

LANDSCAPING AND BUFFER REQUIREMENTS		
CODE SECTION	REQUIRED	PROPOSED
§ 85-843 A.(1)	LANDSCAPE REQUIREMENTS MINIMUM 20% OF SITE SHALL BE MAINTAINED AS LANDSCAPED SITE AREA = 59,951 SQ FT (59,951 SQ FT) (0.20) = 11,990 SQ FT	COMPLIES 23,215 SQ FT PROPOSED
§ 85-843 A.(2)	MINIMUM 50% OF REQUIRED LANDSCAPED AREA SHALL BE LOCATED WITHIN THE FRONT YARD REQUIRED LANDSCAPED AREA = 11,990 SQ FT (11,990 SQ FT) (0.50) = 5,995 SQ FT	COMPLIES 17,361 SQ FT PROPOSED
§ 85-843 A.(3)	STREET TREES 1 TREE FOR EVERY 30 LF OF FRONTAGE WILLIAM FLOYD PARKWAY = 90 FT (90 FT) (1 TREE / 30 LF OF FRONTAGE) = 3 TREES MORICHES MIDDLE ISLAND ROAD = 173 FT (173 FT) (1 TREE / 30 LF OF FRONTAGE) = 5 TREES REVILLO AVENUE = 255 FT (255 FT) (1 TREE / 30 LF OF FRONTAGE) = 9 TREES	3 PROPOSED 5 PROPOSED 9 PROPOSED
§ 85-843 A.(4)	SCREENING REQUIREMENTS MINIMUM 15 FT LANDSCAPED AREA ALONG ALL STREET FRONTAGES	DOES NOT COMPLY
§ 85-843 B.(1)	MINIMUM PERIMETER BUFFER AREA OF 25 FT ADJACENT TO ANY RESIDENTIAL ZONE	DOES NOT COMPLY



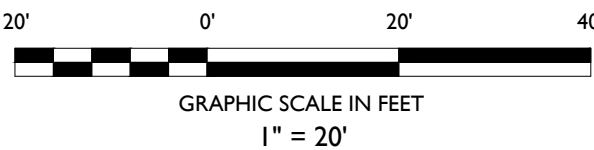
Know what's below
Call before you dig.

IRRIGATION NOTE:

IRRIGATION CONTRACTOR TO PROVIDE A DESIGN FOR AN IRRIGATION SYSTEM SEPARATING PLANTING BEDS FROM LAWN AREA. PRIOR TO CONSTRUCTION, DESIGN IS TO BE SUBMITTED TO THE PROJECT LANDSCAPE DESIGNER FOR REVIEW AND APPROVAL. WHERE POSSIBLE, DRIP IRRIGATION AND OTHER WATER CONSERVATION TECHNIQUES SUCH AS RAIN SENSORS SHALL BE IMPLEMENTED. CONTRACTOR TO VERIFY MAXIMUM ON SITE DYNAMIC WATER PRESSURE AVAILABLE MEASURED IN PSI. PRESSURE REDUCING DEVICES OR BOOSTER PUMPS SHALL BE PROVIDED TO MEET SYSTEM PRESSURE REQUIREMENTS. DESIGN TO SHOW ALL VALVES, PIPING, HEADS, BACKFLOW PREVENTION, METERS, CONTROLLERS, AND SLEEVES WITHIN HARDSCAPE AREAS.

LANDSCAPING NOTES

- THE CONTRACTOR SHALL RESTORE ALL DISTURBED GRASS AND LANDSCAPED AREAS TO MATCH EXISTING CONDITIONS UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET.
- THE CONTRACTOR SHALL RESTORE ALL DISTURBED LAWN AREAS WITH A MINIMUM 4 INCH LAYER OF TOPSOIL AND SEED.
- THE CONTRACTOR SHALL RESTORE MULCH AREAS WITH A MINIMUM 3 INCH LAYER OF MULCH.
- THE MAXIMUM SLOPE ALLOWABLE IN LANDSCAPE RESTORATION AREAS SHALL BE 3 FEET HORIZONTAL TO 1 FOOT VERTICAL (3:1 SLOPE) UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET.
- THE CONTRACTOR IS REQUIRED TO LOCATE ALL SPRINKLER HEADS IN AREA OF DISTURBANCE PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL RELOCATE SPRINKLER HEADS AND LINES IN ACCORDANCE WITH OWNER'S DIRECTION WITHIN AREAS OF DISTURBANCE.
- THE CONTRACTOR SHALL ENSURE THAT ALL DISTURBED LANDSCAPED AREAS ARE GRADED TO MEET FLUSH AT THE ELEVATION OF WALKWAYS AND TOP OF CURB ELEVATIONS EXCEPT UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET. NO ABRUPT CHANGES IN GRADE ARE PERMITTED IN DISTURBED LANDSCAPING AREAS.



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SCALE: 1" = 20' PROJECT ID: K-18112

TITLE:

LANDSCAPING PLAN

DRAWING:

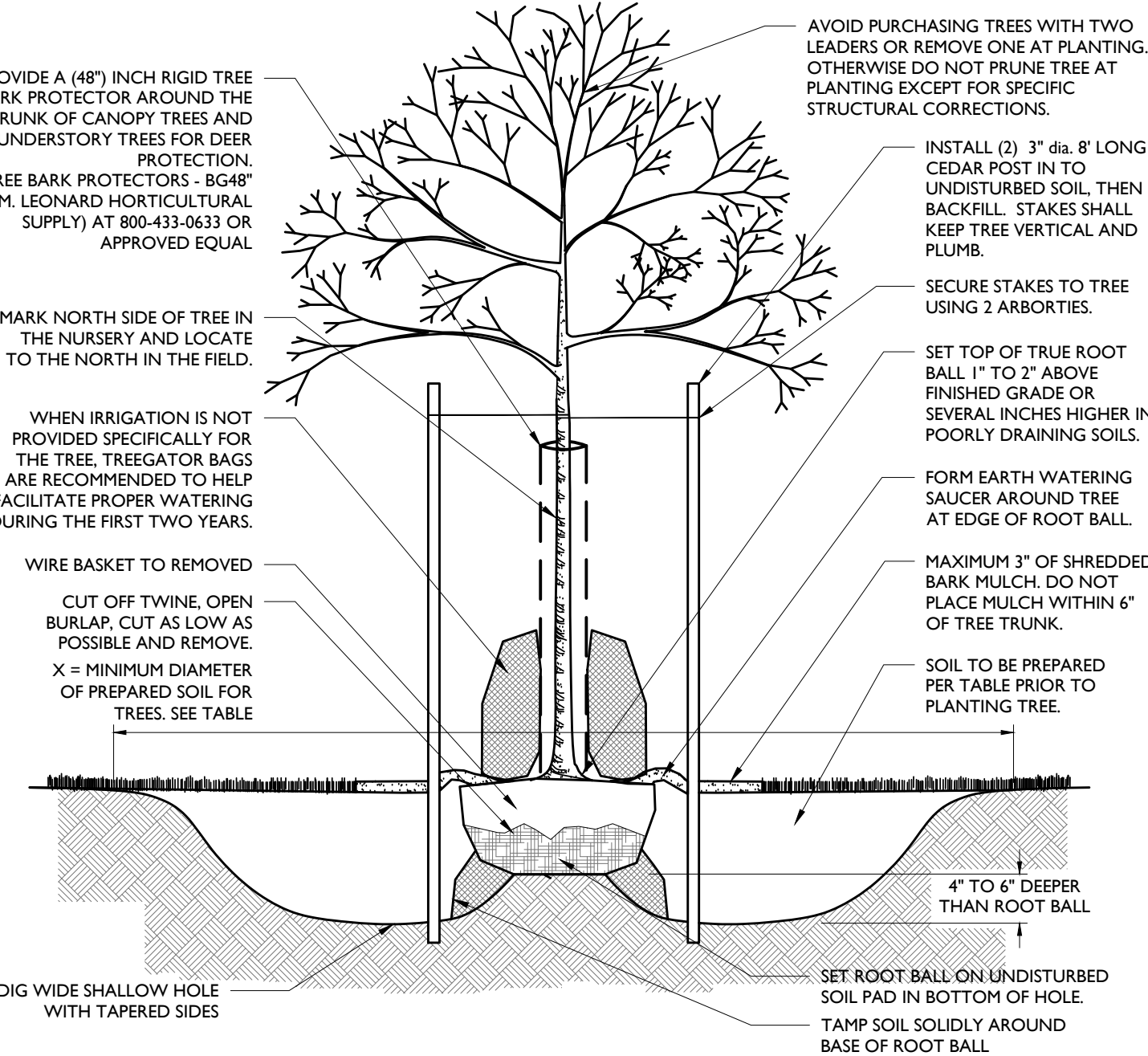
C-10

DATE	BY	DESCRIPTION
04/09/2022	BR	PLANNING DIVISION RESUBMISSION
05/22/2022	BR	PLANNING DIVISION RESUBMISSION
11/09/2021	BR	UPDATED PER TOWN AND COUNTY COMMENTS
08/07/2021	BR	ISSUED FOR REVIEW

NOT APPROVED FOR CONSTRUCTION

NOTES:

- FOR CONTAINER-GROWN TREES, USE FINGERS OR SMALL HAND TOOLS TO PULL THE ROOTS OUT OF THE OUTER LAYER OF POTTING SOIL, THEN CUT OR PULL APART ANY ROOTS CIRCLING THE PERIMETER OF THE CONTAINER.
- THOROUGHLY SOAK THE TREE ROOT BALL AND ADJACENT PREPARED SOIL SEVERAL TIMES DURING THE FIRST MONTH AFTER PLANTING AND REGULARLY THROUGHOUT THE FOLLOWING TWO SUMMERS.
- SOIL AMENDMENTS:
 - MODIFY HEAVY CLAY OR SILT SOILS (MORE THAN 40% CLAY OR SILT) BY ADDING COMPOSTED PINE BARK (UP TO 30% BY VOLUME) OR GYPSUM
 - MODIFY EXTREMELY SANDY SOILS (MORE THAN 85% SAND) BY ADDING ORGANIC MATTER AND/OR DRY, SHREDDED CLAY LOAM UP TO 30% OF THE TOTAL MIX

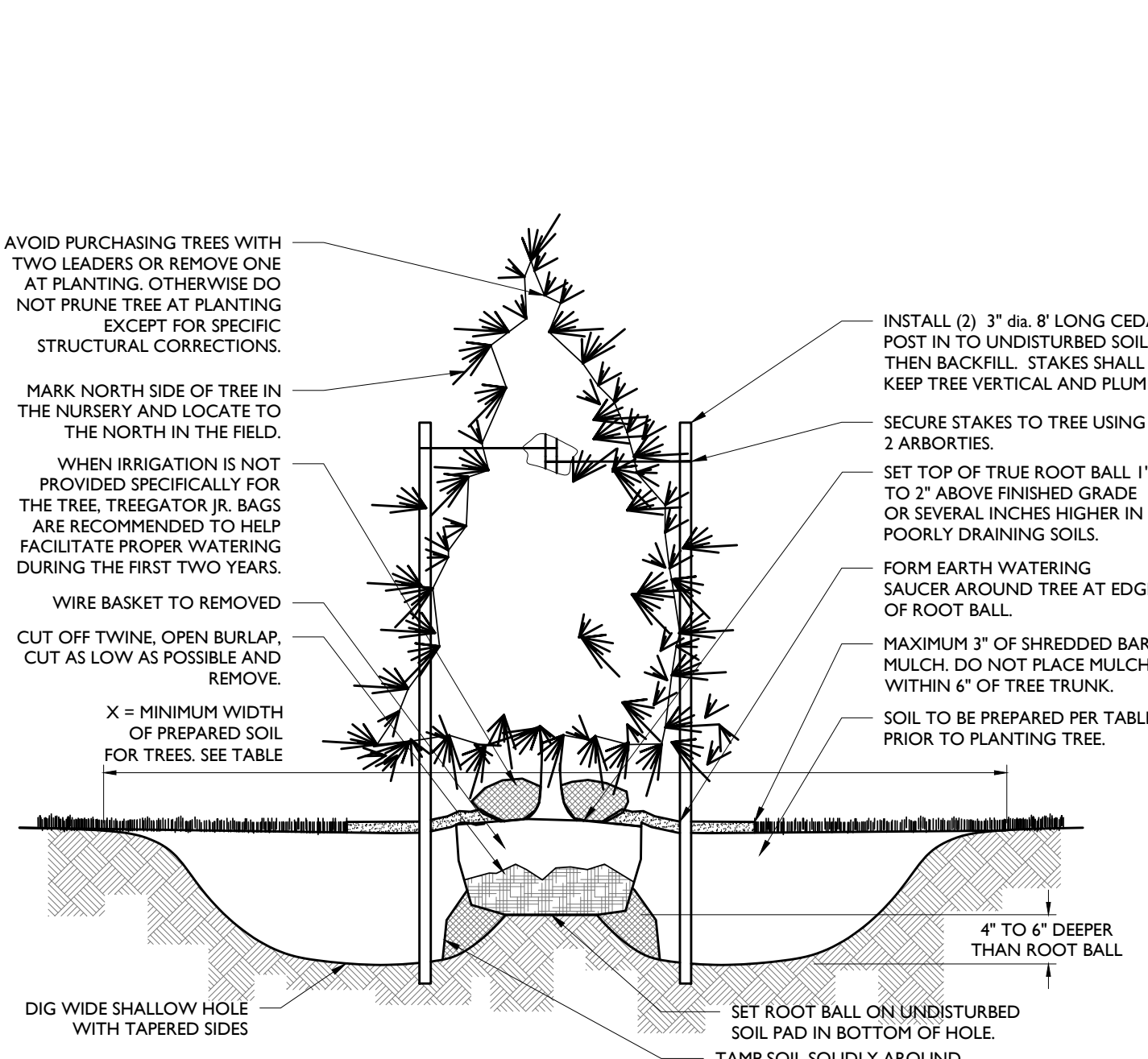


DECIDUOUS TREE PLANTING DETAIL

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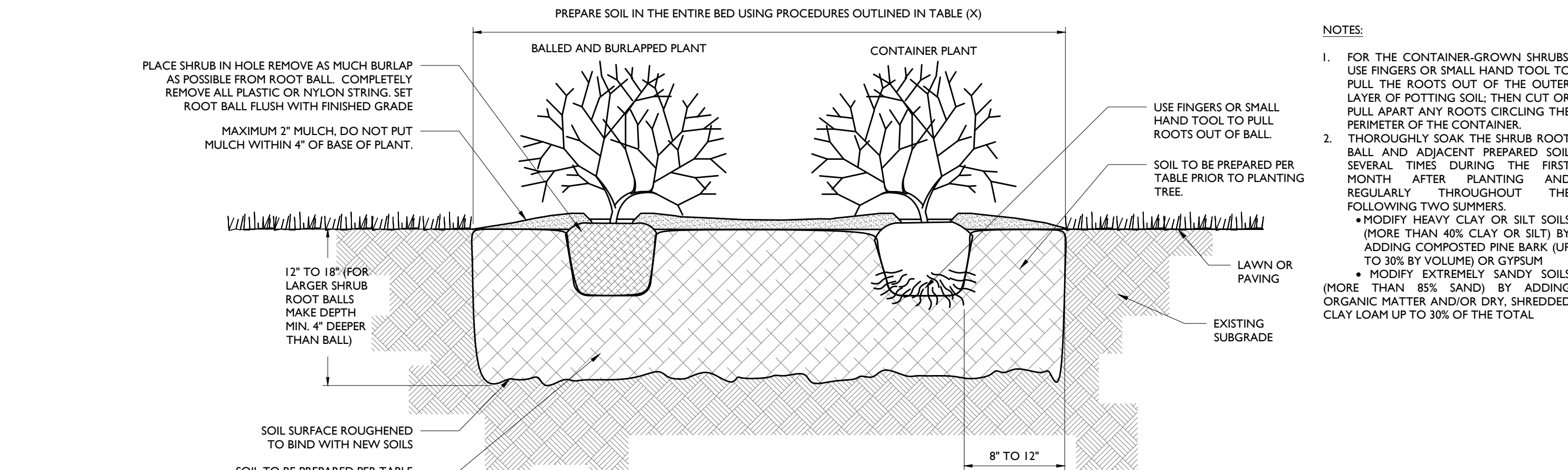
NOTES:

- FOR CONTAINER-GROWN TREES, USE FINGERS OR SMALL HAND TOOLS TO PULL THE ROOTS OUT OF THE OUTER LAYER OF POTTING SOIL, THEN CUT OR PULL APART ANY ROOTS CIRCLING THE PERIMETER OF THE CONTAINER.
- THOROUGHLY SOAK THE TREE ROOT BALL AND ADJACENT PREPARED SOIL SEVERAL TIMES DURING THE FIRST MONTH AFTER PLANTING AND REGULARLY THROUGHOUT THE FOLLOWING TWO SUMMERS.
- SOIL AMENDMENTS:
 - MODIFY HEAVY CLAY OR SILT SOILS (MORE THAN 40% CLAY OR SILT) BY ADDING COMPOSTED PINE BARK (UP TO 30% BY VOLUME) OR GYPSUM
 - MODIFY EXTREMELY SANDY SOILS (MORE THAN 85% SAND) BY ADDING ORGANIC MATTER AND/OR DRY, SHREDDED CLAY LOAM UP TO 30% OF THE TOTAL MIX



CONIFEROUS TREE PLANTING DETAIL

NOT TO SCALE

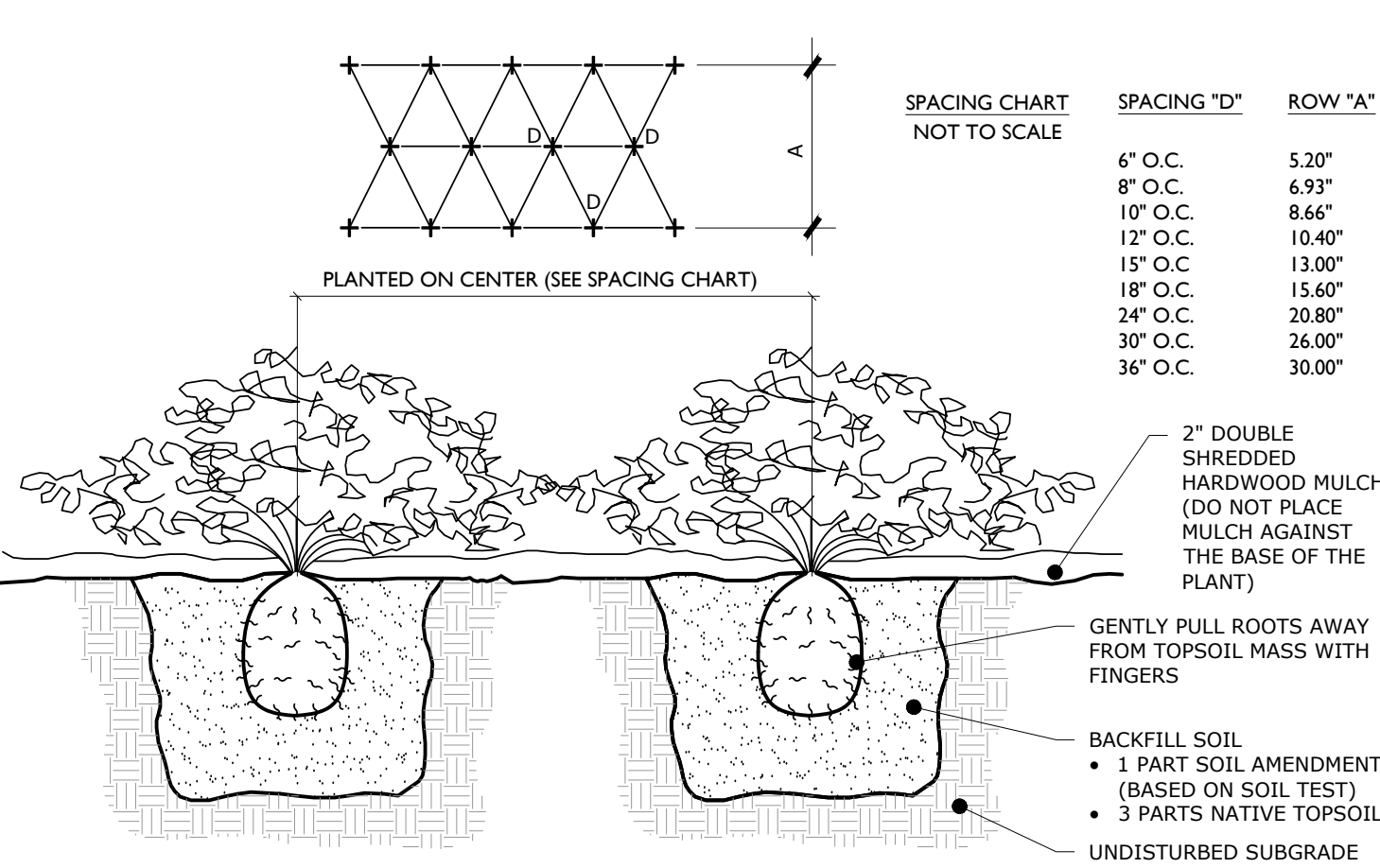


DECIDUOUS AND EVERGREEN SHRUB PLANTING DETAIL

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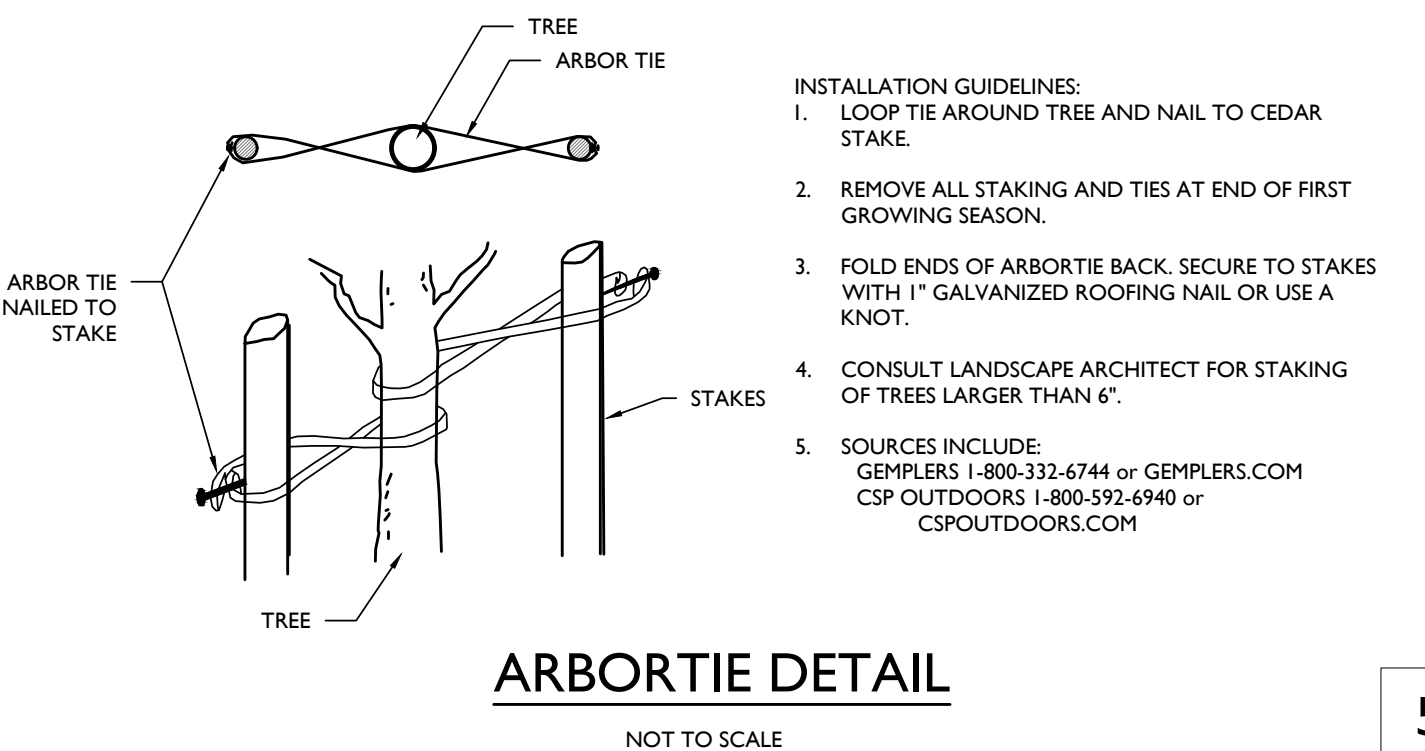
NOTES:

- THOROUGHLY SOAK THE GROUND COVER ROOT BALL AND ADJACENT PREPARED SOIL SEVERAL TIMES DURING THE FIRST MONTH AFTER PLANTING AND REGULARLY THROUGHOUT THE FOLLOWING TWO SUMMERS.
- SOIL AMENDMENTS:
 - MODIFY HEAVY CLAY OR SILT SOILS (MORE THAN 40% CLAY OR SILT) BY ADDING COMPOSTED PINE BARK (UP TO 30% BY VOLUME) OR GYPSUM
 - MODIFY EXTREMELY SANDY SOILS (MORE THAN 85% SAND) BY ADDING ORGANIC MATTER AND/OR DRY, SHREDDED CLAY LOAM UP TO 30% OF THE TOTAL MIX
- ALL GROUND COVER AREAS SHALL BE TREATED WITH A PRE-EMERGENT PER MANUFACTURER'S SPECIFICATIONS



GROUND COVER/PERENNIAL/ANNUAL PLANTING DETAIL

NOT TO SCALE



ARBORTIE DETAIL

NOT TO SCALE

GENERAL LANDSCAPING NOTES:

- THE LANDSCAPE CONTRACTOR SHALL FURNISH ALL MATERIALS AND PERFORM ALL WORK IN ACCORDANCE WITH THESE SPECIFICATIONS, APPROVED OR FINAL DRAWINGS, AND INSTRUCTIONS PROVIDED BY THE PROJECT LANDSCAPE DESIGNER, MUNICIPAL OFFICIALS, OR OWNER/OWNER'S REPRESENTATIVE. ALL WORK COMPLETED AND MATERIALS FURNISHED AND INSTALLED SHALL BE IN STRICT ACCORDANCE WITH THE INTENTION OF THE SPECIFICATIONS, DRAWINGS, AND INSTRUCTIONS AND EXECUTED WITH THE STANDARD LEVEL OF CARE FOR THE LANDSCAPE INDUSTRY.
- WORK MUST BE CARRIED OUT ONLY DURING WEATHER CONDITIONS FAVORABLE TO LANDSCAPE CONSTRUCTION AND TO THE HEALTH AND WELFARE OF PLANTS. THE SUITABILITY OF SUCH WEATHER CONDITIONS SHALL BE DETERMINED BY THE PROJECT LANDSCAPE DESIGNER OR GOVERNING MUNICIPAL OFFICIAL.
- IT IS THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR, BEFORE ORDERING OR PURCHASING MATERIALS, TO PROVIDE SAMPLES OF THOSE MATERIALS TO THE PROJECT LANDSCAPE DESIGNER OR GOVERNING MUNICIPAL OFFICIAL FOR APPROVAL, IF SO REQUESTED.
- IF SAMPLES ARE REQUESTED, THE LANDSCAPE CONTRACTOR IS TO SUBMIT CERTIFICATION TAGS FROM TREES, SHRUBS AND SEED VERIFYING TYPE AND PURITY.
- UNLESS OTHERWISE AUTHORIZED BY THE PROJECT LANDSCAPE DESIGNER OR GOVERNING MUNICIPAL OFFICIAL, THE LANDSCAPE CONTRACTOR SHALL PROVIDE NOTICE AT LEAST FORTY-EIGHT HOURS (48 HRS.) IN ADVANCE OF THE ANTICIPATED DELIVERY DATE OF ANY PLANT MATERIALS TO THE PROJECT SITE. A LEGIBLE COPY OF THE INVOICE, SHOWING VARIETIES AND SIZES OF MATERIALS INCLUDED FOR EACH SHIPMENT SHALL BE FURNISHED TO THE PROJECT LANDSCAPE DESIGNER, OR GOVERNING MUNICIPAL OFFICIAL.
- THE PROJECT LANDSCAPE DESIGNER OR GOVERNING MUNICIPAL OFFICIAL RESERVES THE RIGHT TO INSPECT AND REJECT PLANTS AT ANY TIME AND AT ANY PLACE.

PROTECTION OF EXISTING VEGETATION NOTES:

- BEFORE COMMENCING WORK, ALL EXISTING VEGETATION WHICH COULD BE IMPACTED AS A RESULT OF THE PROPOSED CONSTRUCTION ACTIVITIES MUST BE PROTECTED FROM DAMAGE BY THE INSTALLATION OF TREE PROTECTION FENCING. FENCING SHALL BE LOCATED AT THE DRIPLINE OR LIMIT OF DISTURBANCE AS DEPICTED WITHIN THE APPROVED OR FINAL PLAN SET, ESTABLISHING THE TREE PROTECTION ZONE. FENCE INSTALLATION SHALL BE IN ACCORDANCE WITH THE PROVIDED "TREE PROTECTION FENCE DETAIL." NO WORK MAY BEGIN UNTIL THIS REQUIREMENT IS FULFILLED. THE FENCING SHALL BE INSPECTED REGULARLY BY THE LANDSCAPE CONTRACTOR AND MAINTAINED UNTIL ALL CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED.
- IN ORDER TO AVOID DAMAGE TO ROOTS, BARK OR LOWER BRANCHES, NO VEHICLE, EQUIPMENT, DEBRIS, OR OTHER MATERIALS SHALL BE DRIVEN, PARKED OR PLACED WITHIN THE TREE PROTECTION ZONE. ALL ON-SITE CONTRACTORS SHALL USE ANY AND ALL PRECAUTIONARY MEASURES WHEN PERFORMING WORK AROUND TREES, WALKS, PAVEMENTS, UTILITIES, AND ANY OTHER FEATURES EITHER EXISTING OR PREVIOUSLY INSTALLED UNDER THIS CONTRACT.
- IN RARE INSTANCES WHERE EXCAVATING, GRADING OR GRADING IS REQUIRED WITHIN THE DRIPLINE OF TREES TO REMAIN, THE WORK SHALL BE PERFORMED AS FOLLOWS:
 - TRENCHING: WHEN TRENCHING OCCURS AROUND TREES TO REMAIN, THE TREE ROOTS SHALL NOT BE CUT, BUT THE TRENCH SHALL BE TUNNELED UNDER OR AROUND THE ROOTS BY CAREFUL HAND DIGGING AND WITHOUT INJURY TO THE ROOTS. NO ROOTS, LIMBS, OR WOODS ARE TO HAVE ANY PAINT OR MATERIAL APPLIED TO ANY SURFACE.
 - RAISING GRADES: WHEN THE GRADE AT AN EXISTING TREE IS BELOW THE NEW FINISHED GRADE, AND FILL NOT EXCEEDING 6 INCHES (6") IS REQUIRED, CLEAN, WASHED GRAVEL FROM ONE TO TWO INCHES (1" - 2") IN SIZE SHALL BE PLACED DIRECTLY AROUND THE TREE TRUNK. THE GRAVEL SHALL EXTEND OUT FROM THE TRUNK ON ALL SIDES A MINIMUM OF 18 INCHES (18") FROM THE TRUNK. THE FINISHED GRADE AT THE TREE TRUNK SHALL BE AT LEAST 6 INCHES (6") ABOVE THE FINISHED GRADE. IF THE GRAVEL BEFORE ANY EARTH FILL IS PLACED, NEW EARTH FILL SHALL NOT BE LEFT IN CONTACT WITH THE TRUNK OF ANY TREE REQUIRING FILL. WHERE FILL EXCEEDING 6 INCHES (6") IS REQUIRED, A DRY LAID TREE WELL SHALL BE CONSTRUCTED. IF APPLICABLE, TREE WELL INSTALLATION SHALL BE IN ACCORDANCE WITH THE PROVIDED "TREE WELL DETAIL."
 - LOWERING GRADES: EXISTING TREES LOCATED IN AREAS WHERE THE NEW FINISHED GRADE IS TO BE LOWERED, SHALL HAVE RE-GRADING WORK DONE BY HAND TO THE INDICATED ELEVATION, NO GREATER THAN SIX INCHES (6"). ROOTS SHALL BE CUT CLEANLY THREE INCHES (3") BELOW FINISHED GRADE UNDER THE DIRECTION OF A LICENSED ARBORIST. WHERE CUT EXCEEDING 6 INCHES (6") IS REQUIRED, A DRY LAID RETAINING WALL SHALL BE CONSTRUCTED. IF APPLICABLE, THE RETAINING WALL INSTALLATION SHALL BE IN ACCORDANCE WITH THE PROVIDED "TREE RETAINING WALL DETAIL."

SOIL PREPARATION AND MULCH NOTES:

- LANDSCAPE CONTRACTOR SHALL OBTAIN A SOIL TEST OF THE IN-SITU TOPSOIL BY A CERTIFIED SOIL LABORATORY PRIOR TO PLANTING. LANDSCAPE CONTRACTOR SHALL ALLOW FOR A TWO WEEK TURNAROUND TIME FROM SUBMITTAL OF SAMPLE TO NOTIFICATION OF RESULTS.
- BASED ON SOIL TEST RESULTS, ADJUST THE RATES OF LIME AND FERTILIZER THAT SHALL BE MIXED INTO THE TOP SIX INCHES (6") OF TOPSOIL. THE LIME AND FERTILIZER RATES PROVIDED WITHIN THE "SEED SPECIFICATION" OR "SOD SPECIFICATION" IS APPROXIMATE AND FOR BIDDING PURPOSES ONLY. IF ADDITIONAL AMENDMENTS ARE NECESSARY, ADJUST THE TOPSOIL AS FOLLOWS:
 - MODIFY HEAVY CLAY OR SILT SOILS (MORE THAN 40% CLAY OR SILT) BY ADDING COMPOSTED PINE BARK (UP TO 30% BY VOLUME) OR GYPSUM.
 - MODIFY EXTREMELY SANDY SOILS (MORE THAN 85%) BY ADDING ORGANIC MATTER AND/OR DRY, SHREDDED CLAY LOAM UP TO 30% OF THE TOTAL MIX.
- TOPSOIL SHALL BE FERTILE, FRIABLE, NATURAL TOPSOIL OF LOAMING CHARACTER, WITHOUT ADMIXTURE OF SUBSOIL MATERIAL, OBTAINED FROM A WELL-DRAINED AREA, FREE FROM ALL CLAY, LUMPS, COARSE SANDS, STONES, PLANTS, ROOTS, STICKS, AND OTHER FOREIGN MATERIAL, GREATER THAN ONE INCH (1").
- TOPSOIL SHALL HAVE A PH RANGE OF 5.0-7.0 AND SHALL NOT CONTAIN LESS THAN 6% ORGANIC MATTER BY WEIGHT.
- OBTAIN TOPSOIL ONLY FROM LOCAL SOURCES OR FROM AREAS HAVING SIMILAR SOIL CHARACTERISTICS TO THAT FOUND AT THE PROJECT SITE.
- CONTRACTOR SHALL PROVIDE A SIX INCH (6") DEEP LAYER OF TOPSOIL IN ALL PLANTING AREAS. TOPSOIL SHALL BE SPREAD OVER A PREPARED SURFACE IN A UNIFORM LAYER TO ACHIEVE THE DESIRED COMPACTED THICKNESS. THE SPREADING OF TOPSOIL SHALL NOT BE CONDUCTED UNDER MUDDY OR FROZEN SOIL CONDITIONS.
- UNLESS OTHERWISE NOTED IN THE CONTRACT, THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION OF TOPSOIL AND THE ESTABLISHMENT OF FINE GRADING WITHIN THE DISTURBED AREA OF THE SITE.
- LANDSCAPE CONTRACTOR SHALL VERIFY THAT THE SUB-GRADE ELEVATION MEETS THE FINISHED GRADE ELEVATION (LESS THE REQUIRED TOPSOIL), IN ACCORDANCE WITH THE APPROVED OR FINAL GRADING PLAN.
- ALL LAWN AND PLANTING AREAS SHALL BE GRADED TO A SMOOTH, EVEN AND UNIFORM PLANE WITH NO ABRUPT CHANGE OF SURFACE AS DEPICTED WITHIN THE APPROVED OR FINAL CONSTRUCTION SET UNLESS OTHERWISE DIRECTED BY THE PROJECT LANDSCAPE DESIGNER OR MUNICIPAL OFFICIAL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER SURFACE AND SUBSURFACE PLANT BED DRAINAGE PRIOR TO THE INSTALLATION OF PLANTINGS. IF POOR DRAINAGE CONDITIONS EXIST, CORRECTIVE ACTION SHALL BE TAKEN PRIOR TO INSTALLATION. ALL PLANTING AND LAWN AREAS SHALL BE GRADED AND MAINTAINED TO ALLOW A FREE FLOW OF SURFACE WATER.
- DOUBLE SHREDDED HARDWOOD MULCH OR APPROVED EQUAL, SHALL BE USED AS A THREE INCH (3") TOP DRESSING IN ALL SHRUB PLANTING BEDS AND AROUND ALL TREES PLANTED BY LANDSCAPE CONTRACTOR, GROUND COVER, PERENNIAL AND ANNUAL PLANTING BEDS SHALL BE MULCHED WITH A TWO INCH (2") TOP DRESSING. SINGLE TREES OR SHRUBS SHALL BE MULCHED TO AVOID CONTACT WITH TRUNK OR PLANT STEM. MULCH SHALL BE OF SUFFICIENT CHARACTER AS NOT TO BE EASILY DISPLACED BY WIND OR WATER RUNOFF.
- WHENEVER POSSIBLE, THE SOIL PREPARATION AREA SHALL BE CONNECTED FROM PLANTING TO PLANTING.
- SOIL SHALL BE LOOSENEED WITH A BACKHOE OR OTHER LARGE COARSE-TILING EQUIPMENT UNLESS THE SOIL IS FROZEN OR EXCESSIVELY WET. TILING THAT PRODUCES LARGE, COARSE CHUNKS OF SOIL IS PREFERABLE TO TILING THAT RESULTS IN FINE GRAINS UNIFORM IN TEXTURE. AFTER THE AREA IS LOOSENEED IT SHALL NOT BE DRIVEN OVER BY ANY VEHICLE.
- APPLY PRE-EMERGENT WEED CONTROL TO ALL PLANT BEDS PRIOR TO MULCHING. ENSURE COMPATIBILITY BETWEEN PRODUCT AND PLANT MATERIAL.
- ALL PLANTING SOIL SHALL BE AMENDED WITH THE FOLLOWING:

- MYCRO® TREE SAVER - A DRY GRANULAR MYCORRHIZAL FUNGI INOCULANT THAT IS MIXED IN THE BACKFILL WHEN PLANTING TREES AND SHRUBS. IT CONTAINS SPORES OF BOTH ECTOMYCORRHIZAL AND VA MYCORRHIZAL FUNGI (VAM), BENEFICIAL RHIZOSPHERE BACTERIA, TERRA-SORB SUPERABSORBENT HYDROGEL TO REDUCE WATER LEACHING, AND SELECTED ORGANIC MICROBIAL NUTRIENTS.
- DIRECTIONS FOR USE: USE 3 OZ PER EACH FOOT DIAMETER OF THE ROOT BALL, OR 3 OZ PER INCH CALIPER, MIX INTO THE BACKFILL WHEN TRANSPLANTING TREES AND SHRUBS. MIX PRODUCT IN A RING-SHAPED VOLUME OF SOIL AROUND THE UPPER PORTION OF THE ROOT BALL, EXTENDING FROM THE SOIL SURFACE TO A DEPTH OF ABOUT 8 INCHES, AND EXTENDING OUT FROM THE ROOT BALL ABOUT 8 INCHES INTO THE BACKFILL. ADD WATER TO SOIL SATURATION.
- MYCRO® TREE SAVER IS EFFECTIVE FOR ALL TREE AND SHRUB SPECIES EXCEPT RHODODENDRONS, AZALEAS, AND MOUNTAIN LAUREL, WHICH REQUIRE ERICOID MYCORRHIZAE.
- SOIL PH: THE FUNGI IN THIS PRODUCT WERE CHOSEN BASED ON THEIR ABILITY TO SURVIVE AND COLONIZE PLANT ROOTS IN A PH RANGE OF 3 TO 9.
- FUNGICIDES: THE USE OF CERTAIN FUNGICIDES CAN HAVE A DETRIMENTAL EFFECT ON THE INOCULATION PROGRAM. SOIL APPLICATION OF ANY FUNGICIDE IS NOT RECOMMENDED FOR TWO WEEKS AFTER APPLICATION.
- OTHER PESTICIDES: HERBICIDES AND INSECTICIDES DO NOT NORMALLY INTERFERE WITH MYCORRHIZAL FUNGAL DEVELOPMENT, BUT MAY INHIBIT THE GROWTH OF SOME TREE AND SHRUB SPECIES IF NOT USED PROPERLY.

IRRIGATION DURING ESTABLISHMENT		
SIZE AT PLANTING	IRRIGATION FOR VITALITY	IRRIGATION FOR SURVIVAL
< 2" CALIPER	DAILY FOR TWO WEEKS, EVERY OTHER DAY FOR TWO MONTHS, WEEKLY UNTIL ESTABLISHED	TWO TO THREE TIMES WEEKLY FOR TWO TO THREE MONTHS
2"-4 CALIPER	DAILY FOR ONE MONTH, EVERY OTHER DAY FOR THREE MONTHS, WEEKLY UNTIL ESTABLISHED	TWO TO THREE TIMES WEEKLY FOR THREE TO FOUR MONTHS
4" > CALIPER	DAILY FOR SIX WEEKS, EVERY OTHER DAY FOR FIVE MONTHS, WEEKLY UNTIL ESTABLISHED	TWICE WEEKLY FOR FOUR TO FIVE MONTHS

TABLE NOTES:

- AT EACH IRRIGATION, APPLY TWO TO THREE GALLONS PER INCH TRUNK CALIPER TO THE ROOT BALL SURFACE. APPLY IT IN A MANNER SO ALL WATER SOAKS THE ENTIRE ROOT BALL. DO NOT WATER IF ROOT BALL IS WET/SATURATED ON THE IRRIGATION DAY.
- WHEN IRRIGATING FOR VITALITY, DELETE DAILY IRRIGATION WHEN PLANTING IN WINTER OR WHEN PLANTING IN COOL CLIMATES. ESTABLISHMENT TAKES THREE TO FOUR MONTHS PER INCH TRUNK CALIPER, NEVER APPLY IRRIGATION IF THE SOIL IS SATURATED.
- WHEN IRRIGATION FOR SURVIVAL, TREES TAKE MUCH LONGER TO ESTABLISH THAN REGULARLY IRRIGATED TREES. IRRIGATION MAY BE REQUIRED IN THE NORMAL HOT, DRY PORTIONS OF THE FOLLOWING YEAR.

PLANT QUALITY AND HANDLING NOTES:

- ALL PLANT MATERIAL SHALL CONFORM TO THE AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z601-2004) OR LATEST REVISION AS PUBLISHED BY THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION.
- IN ALL CASES, BOTANICAL NAMES LISTED WITHIN THE APPROVED OR FINAL PLANT LIST SHALL TAKE PRECEDENCE OVER COMMON NAMES.
- ALL PLANTS SHALL BE OF SELECTED SPECIMEN QUALITY, EXCEPTIONALLY HEAVY, TIGHTLY KNIT, SO TRAINED OR FAVORED IN THEIR DEVELOPMENT AND APPEARANCE AS TO BE SUPERIOR IN FORM, NUMBER OF BRANCHES, COMPACTNESS AND SYMMETRY. ALL PLANTS SHALL HAVE A NORMAL HABIT OR SOUND, HEALTHY, VIGOROUS PLANTS WITH WELL DEVELOPED ROOT SYSTEM. PLANTS SHALL BE FREE OF DISEASE, INSECT PESTS, EGGS OR LARVAE.
- PLANTS SHALL NOT BE PRUNED BEFORE DELIVERY. TREES WITH ABRASION OF THE BARK, SUNSCALDS, DISFIGURING KNOTS OR FRESH CUTS OF LIMBS OVER ONE AND ONE-FOURTH INCHES (1-1/4") WHICH HAVE NOT COMPLETELY CALLOUSED SHALL BE REJECTED.
- ALL PLANTS SHALL BE TYPICAL OF THEIR SPECIES OR VARIETY AND SHALL HAVE A NORMAL HABIT OF GROWTH AND BE LEGIBLY TAGGED WITH THE PROPER NAME AND SIZE.
- THE ROOT SYSTEM OF EACH PLANT SHALL BE WELL PROVIDED WITH FIBROUS ROOTS. ALL PARTS SHALL BE SOUND, HEALTHY, VIGOROUS, WELL-BRANCHED AND DENSELY FOLIATED WHEN IN LEAF.
- ALL PLANTS DESIGNATED BALL AND BURLAP (B&B) MUST BE MOVED WITH THE ROOT SYSTEM AS SOLID UNITS WITH BALLS OF EARTH FIRMLY WRAPPED WITH BURLAP. THE DIAMETER AND DEPTH OF THE BALLS OF EARTH MUST BE SUFFICIENT TO ENCOMPASS THE FIBROUS ROOT FEEDING SYSTEMS NECESSARY FOR THE HEALTHY DEVELOPMENT OF THE PLANT. NO PLANT SHALL BE ACCEPTED WHEN THE BALL OF EARTH SURROUNDING ITS ROOTS HAS BEEN BADLY CRACKED OR BROKEN PREPARATORY TO OR DURING THE PROCESS OF PLANTING. THE BALLS SHALL REMAIN INTACT DURING ALL OPERATIONS. ALL PLANTS THAT CANNOT BE PLANTED AT ONCE MUST BE HEeled-IN BY SETTING IN THE GROUND AND COVERING THE BALLS WITH SOIL OR MULCH AND THEN WATERING. HEMP BURLAP AND TWINE IS PREFERABLE TO TREATED. IF TREATED BURLAP IS USED, ALL TWINE IS TO BE CUT FROM AROUND THE TRUNK AND ALL BURLAP IS TO BE REMOVED.
- PLANTS TRANSPORTED TO THE PROJECT IN OPEN VEHICLES SHALL BE COVERED WITH TARP OR OTHER SUITABLE COVERS SECURELY FASTENED TO THE BODY OF THE VEHICLE TO PREVENT INJURY TO THE PLANTS. CLOSED VEHICLES SHALL BE ADEQUATELY VENTILATED TO PREVENT OVERHEATING OF THE PLANTS. EVIDENCE OF INADEQUATE PROTECTION FOLLOWING DIGGING, CARELESSNESS WHILE IN TRANSIT, OR IMPROPER HANDLING OR STORAGE SHALL BE CAUSE FOR REJECTION OF PLANT MATERIAL. ALL PLANTS SHALL BE KEPT MOIST, FRESH, AND PROTECTED. SUCH PROTECTION SHALL ENCOMPASS THE ENTIRE PERIOD DURING WHICH THE PLANTS ARE IN TRANSIT, BEING HANDLED, OR ARE IN TEMPORARY STORAGE.
- ALL PLANT MATERIAL SHALL BE INSTALLED IN ACCORDANCE WITH THE CORRESPONDING LANDSCAPE PLAN AND PLANTING DETAILS.
- LANDSCAPE CONTRACTOR SHALL MAKE BEST EFFORT TO INSTALL PLANTINGS ON THE SAME DAY AS DELIVERY. IF PLANTS ARE NOT PLANTED IMMEDIATELY ON SITE, PROPER CARE SHALL BE TAKEN TO PLACE THE PLANTINGS IN PARTIAL SHADE WHEN POSSIBLE. THE ROOT BALL SHALL BE KEPT MOIST AT ALL TIME AND COVERED WITH MOISTENED MULCH OR AGED WOODCHIPS. PROPER IRRIGATION SHALL BE SUPPLIED SO AS TO NOT ALLOW THE ROOT BALL TO DRY OUT. PLANTINGS SHALL BE UNTIED AND PROPER SPACING SHALL BE ALLOTTED FOR AIR CIRCULATION AND TO PREVENT DISEASE, WILTING, AND LEAF LOSS. PLANTS THAT REMAIN UNPLANTED FOR A PERIOD OF TIME GREATER THAN THREE (3) DAYS SHALL BE HEaled IN WITH TOPSOIL OR MULCH AND WATERED AS REQUIRED TO PRESERVE ROOT MOISTURE.
- NO PLANT MATERIAL SHALL BE PLANTED IN MUDDY OR FROZEN SOIL.
- PLANTS WITH INJURED ROOTS OR BRANCHES SHALL BE PRUNED PRIOR TO PLANTING UTILIZING CLEAN, SHARP TOOLS. ONLY DISEASED OR INJURED PLANTS SHALL BE REMOVED.
- IF ROCK OR OTHER UNDERGROUND OBSTRUCTION IS ENCOUNTERED, THE LANDSCAPE DESIGNER RESERVES THE RIGHT TO REMOVE IT OR ENCASE PLANTING IN ITS OWN PROTECTION.
- IF PLANTS ARE PROPOSED WITHIN SIGHT TRIANGLES, TREES SHALL BE LIMBED AND MAINTAINED TO A HEIGHT OF EIGHT FEET (8') ABOVE GRADE, AND SHRUBS, GROUND COVER, PERENNIALS, AND ANNUALS SHALL BE MAINTAINED TO A HEIGHT NOT TO EXCEED TWO FEET (2') ABOVE GRADE UNLESS OTHERWISE NOTED OR SPECIFIED BY THE GOVERNING MUNICIPALITY OR AGENCY.
- INSTALLATION SHALL OCCUR DURING THE FOLLOWING SEASONS:
 - PLANTS (MARCH 15 - DECEMBER 15)
 - LAWNS (MARCH 15 - JUNE 15 OR SEPTEMBER 1 - DECEMBER 1)
- THE FOLLOWING TREES ARE SUSCEPTIBLE TO TRANSPORT SHOCK AND SHALL NOT BE PLANTED DURING THE FALL SEASON (STARTING SEPTEMBER 15):
 - ABIES CONCOLOR
 - ACER BUEKERIANUM
 - ACER FREESIANUM
 - ACER RUBRUM
 - ACER SACCHARINUM
 - BETULA VARIETIES
 - CARPINUS VARIETIES
 - CEDRUS DEODARA
 - CELTIS VARIETIES
 - CERIDOPHYLLUM VARIETIES
 - CERCIS CANADENSIS
 - CORNUS VARIETIES
 - CRATAEGUS VARIETIES
 - CUPRESSOCYPARIS LEYLANDII
 - FAGUS VARIETIES
 - HALESIA VARIETIES
 - ILEX X FOSTERI
 - ILEX NELLE STEVENS
 - LIQUIDAMBAR VARIETIES
 - JUNIPERUS VIRGINIANA
 - KOELERUTERA PANICULATA
 - LYCODYMBAR VARIETIES
 - LIRODENDRON VARIETIES
 - MALLUS IN LEAF
 - NYSSA SYLVATICA
 - OSTRYA VIRGINIANA
 - PINUS NIGRA
 - CUPRESSOCYPARIS LEYLANDII
 - PLATANUS VARIETIES
 - POPULUS VARIETIES
 - PRUNUS VARIETIES
 - PYRUS VARIETIES
 - QUERCUS VARIETIES (NOT Q. PALUSTRIS)
 - SALIX WEEPING VARIETIES
 - SORBUS VARIETIES
 - TAXODIUM VARIETIES
 - TAXUS S. REPANDENS
 - TILIA TOMENTOSA VARIETIES
 - ULMUS PARVIFOLIA VARIETIES
 - ZELKOVA VARIETIES

- IF A PROPOSED PLANT IS UNAVAILABLE OR ON THE FALL DIGGING HAZARD LIST, AN EQUIVALENT SPECIES OF THE SAME SIZE MAY BE REQUESTED FOR SUBSTITUTION OF THE ORIGINAL PLANT. ALL SUBSTITUTIONS SHALL BE APPROVED BY THE PROJECT LANDSCAPE DESIGNER OR MUNICIPAL OFFICIAL PRIOR TO ORDERING AND INSTALLATION.
- DURING THE COURSE OF CONSTRUCTION/PLANT INSTALLATION, EXCESS AND WASTE MATERIALS SHALL BE CONTINUOUSLY AND PROMPTLY REMOVED AT THE END OF EACH WORK DAY. ALL DEBRIS, MATERIALS, AND TOOLS SHALL BE PROPERLY STORED, STOCKPILED OR DISPOSED OF AND ALL PAVED AREAS SHALL BE CLEANED.
- THE LANDSCAPE CONTRACTOR SHALL DISPOSE OF ALL RUBBISH AND EXCESS SOIL AT HIS EXPENSE TO AN OFF-SITE LOCATION AS APPROVED BY THE LOCAL MUNICIPALITY.
- 90-DAY MAINTENANCE PERIOD SHALL BEGIN IMMEDIATELY AFTER ALL PLANTS HAVE BEEN SATISFACTORILY INSTALLED.
- MAINTENANCE SHALL INCLUDE BUT NOT BE LIMITED TO, REPLACING MULCH THAT HAS BEEN DISPLACED BY EROSION OR OTHER MEANS, REPAIRING AND RESHAPING WATER RINGS OR SAUCERS, MAINTAINING STAKES AND GUYS IF ORIGINALLY REQUIRED, WATERING WHEN NEEDED OR DIRECTED, WEEDING, PRUNING, SPRAYING, FERTILIZING, MOWING THE LAWN, AND PERFORMING ANY OTHER WORK REQUIRED TO KEEP THE PLANTS IN A HEALTHY CONDITION.
- HOW ALL GRASS AREAS AT REGULAR INTERVALS TO KEEP THE GRASS HEIGHT FROM EXCEEDING THREE INCHES (3"). MOWING SHALL BE PERFORMED ONLY WHEN GRASS IS DRY. MOWER BLADE SHALL BE SET TO REMOVE NO MORE THAN ONE THIRD (1/3) OF THE GRASS LENGTH. WHEN THE AMOUNT OF GRASS IS HEAVY, IT SHALL BE REMOVED TO PREVENT DESTRUCTION OF THE UNDERLYING TURF. MOW GRASS AREAS IN SUCH A MANNER AS TO PREVENT CLIPPINGS FROM BLOWING ON PAVED AREAS, AND SIDEWALKS. CLEANUP AFTER MOWING SHALL INCLUDE SWEEPING OR BLOWING OF PAVED AREAS AND SIDEWALKS TO CLEAR THEM FROM MOWING DEBRIS.
- GRASSED AREAS DAMAGED DURING THE PROCESS OF THE WORK SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR, WHO SHALL RESTORE THE DISTURBED AREAS TO A CONDITION SATISFACTORY TO THE PROJECT LANDSCAPE DESIGNER, MUNICIPAL OFFICIAL, OR OWNER/OWNER'S REPRESENTATIVE. THIS MAY INCLUDE FILLING TO GRADE, FERTILIZING, SEEDING, AND MULCHING.
- SHOULD THE OWNER REQUIRE MAINTENANCE BEYOND THE STANDARD 90-DAY MAINTENANCE PERIOD, A SEPARATE CONTRACT SHALL BE ESTABLISHED.
- LANDSCAPE CONTRACTOR SHALL WATER NEW PLANTINGS FROM TIME OF INSTALL AND THROUGHOUT REQUIRED 90-DAY MAINTENANCE PERIOD UNTIL PLANTS ARE ESTABLISHED. IF ON-SITE WATER IS NOT AVAILABLE AT THE PROJECT LOCATION, THE LANDSCAPE CONTRACTOR SHALL FURNISH IT BY MEANS OR A WATERING TRUCK OR OTHER ACCEPTABLE MANNER.
- THE QUANTITY OF WATER APPLIED AT ONE TIME SHALL BE SUFFICIENT TO PENETRATE THE SOIL TO A MINIMUM OF EIGHT INCHES (8") IN SHRUB BEDS AND SIX INCHES (6") IN TURF AREAS AT A RATE WHICH WILL PREVENT SATURATION OF THE SOIL.
- IF AN AUTOMATIC IRRIGATION SYSTEM HAS BEEN INSTALLED, IT CAN BE USED FOR WATERING PLANT MATERIAL. HOWEVER, FAILURE OF THE SYSTEM DOES NOT ELIMINATE THE LANDSCAPE CONTRACTOR'S RESPONSIBILITY OF PLANT HEALTH AND ESTABLISHMENT.

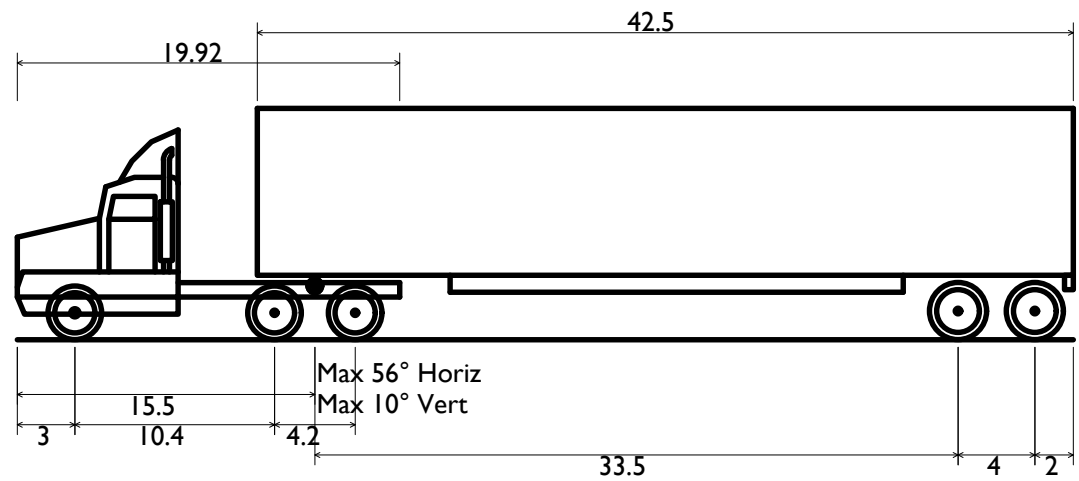
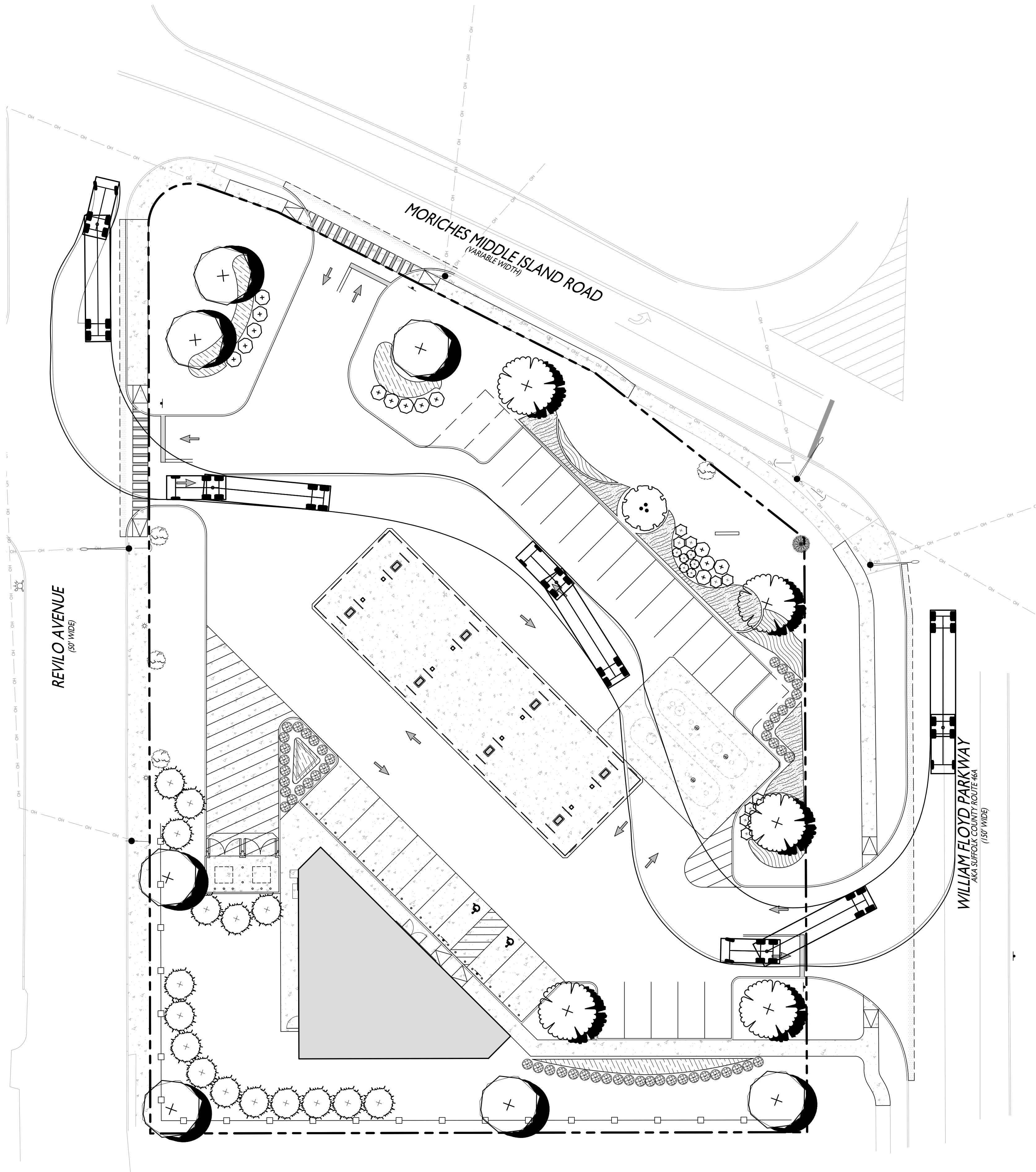
PLANT MATERIAL GUARANTEE NOTES:

- THE LANDSCAPE CONTRACTOR SHALL GUARANTEE ALL PLANT MATERIAL FOR A PERIOD OF ONE YEAR (1 YR.) FROM APPROVAL OF LANDSCAPE INSTALLATION BY THE PROJECT LANDSCAPE DESIGNER, MUNICIPAL OFFICIAL, OR OWNER/OWNER'S REPRESENTATIVE.
- THE LANDSCAPE CONTRACTOR SHALL REMOVE AND REPLACE DYING, DEAD, OR DEFECTIVE PLANT MATERIAL AT HIS EXPENSE. THE LANDSCAPE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR ANY DAMAGES CAUSED BY HIS COMPANY'S OPERATIONS.
- ALL REPLACEMENT PLANTS SHALL BE OF THE SAME SPECIES AND SIZE AS SPECIFIED ON THE APPROVED OR FINAL PLANT LIST. REPLACEMENTS RESULTING FROM REMOVAL, LOSS, OR DAMAGE DUE TO OCCUPANCY OF THE PROJECT IN ANY PART: VANDALISM, PHYSICAL DAMAGE BY ANIMALS, VEHICLES, ETC., AND LOSSES DUE TO CURTALMENT OF WATER BY LOCAL AUTHORITIES SHALL BE APPROVED AND PAID FOR BY THE OWNER.
- THE CONTRACTOR SHALL INSTRUCT THE OWNER AS TO THE PROPER CARE AND MAINTENANCE OF ALL PLANTINGS.

LAWN (SEED OR SOD) NOTES:

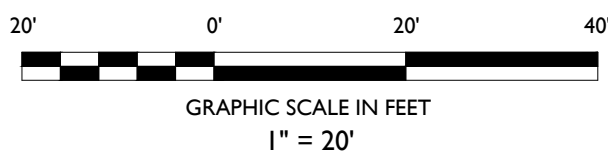
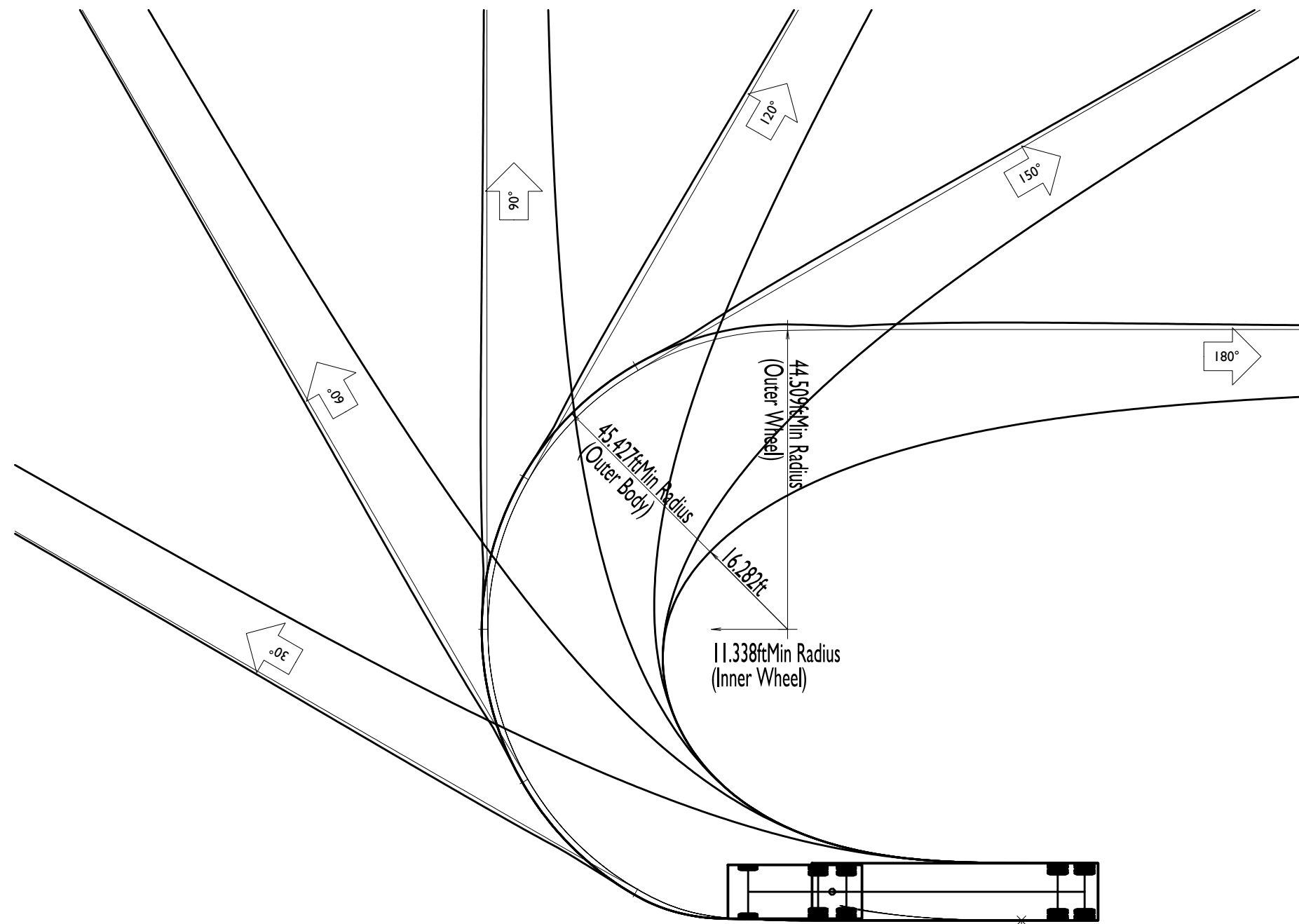
- SEED MIXTURE SHALL BE FRESH, CLEAN, NEW CROP SEED. SOD SHALL BE STRONGLY ROOTED, UNIFORM IN THICKNESS, AND FREE OF WEEDS, DISEASE, AND PESTS.
- SEED OR SOD SHALL BE PURCHASED FROM A RECOGNIZED DISTRIBUTOR AND SHALL BE COMPOSED OF THE MIX OR BLEND WITHIN THE PROVIDED "SEED SPECIFICATION" OR "SOD SPECIFICATION".
- REFERENCE LANDSCAPE PLAN FOR AREAS TO BE SEEDED OR LAID WITH SOD.
- SEEDING SHALL NOT BE PERFORMED IN WINDY WEATHER. IF THE SEASON OF THE PROJECT COMPLETION PROHIBITS PERMANENT STABILIZATION, TEMPORARY STABILIZATION SHALL BE PROVIDED IN ACCORDANCE WITH THE "TEMPORARY SEEDING SPECIFICATION".
- PROTECT NEW LAWN AREAS AGAINST TRESPASSING WHILE THE SEED IS GERMINATING. FURNISH AND INSTALL FENCES, SIGNS, BARRIERS OR ANY OTHER NECESSARY TEMPORARY PROTECTIVE DEVICES. DAMAGE RESULTING FROM TRESPASS, EROSION, WASHOUT, SETTLEMENT OR OTHER CAUSES SHALL BE REPAIRED BY THE LANDSCAPE CONTRACTOR AT HIS EXPENSE. REMOVE ALL FENCES, SIGNS, BARRIERS OR OTHER TEMPORARY PROTECTIVE DEVICES ONCE LAWN HAS BEEN ESTABLISHED.

K:\2018\K-18112-7-ELEVEN-1481 WILLIAM FLOYD PARKWAY, SHELLEY, NY\CAD\EXHIBITS\2022\24-1-6\VEHICLE MANEUVERING EXHIBIT.DWG



WB-50 - Intermediate Semi-Trailer
Overall Length 55.000ft
Overall Width 8.500ft
Overall Body Height 12.052ft
Min Body Ground Clearance 1.334ft
Max Track Width 8.500ft
Lock-to-lock time 6.00s
Max Steering Angle (Virtual) 17.90°

SYMBOL	DESCRIPTION
	PROPERTY LINE
	SETBACK LINE
	PROPOSED CURB
	PROPOSED FLUSH CURB
	PROPOSED SIGNS / BOLLARDS
	PROPOSED BUILDING
	PROPOSED CONCRETE
	PROPOSED RETAINING WALL
	PROPOSED VINYL FENCE
	PROPOSED TREES
	PROPOSED SHRUBS
	PROPOSED GROUND COVER PLANTS



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BY

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DATE

04/14/2022

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DATE

04/14/2022

NOT APPROVED FOR CONSTRUCTION

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engineering & design

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www.stonefieldeng.com

584 Broadway, Suite 310, New York, NY 10012
Phone 718.606.8305

EXHIBIT

7-ELEVEN, INC
**PROPOSED CONVENIENCE STORE
WITH FUEL SALES**

SECTION 642, BLOCK 3, LOT 41.3, 41.4 & 41.5
1481 WILLIAM FLOYD PARKWAY
HAMLET OF SHIRLEY, TOWN OF BROOKHAVEN
SUFFOLK COUNTY, NEW YORK

ZACHARY E. CHAPLIN, P.E.
NEW YORK LICENSE No. 99748
LICENSED PROFESSIONAL ENGINEER

STONEFIELD
engineering & design

SCALE: 1" = 20'

PROJECT ID: K-18112

TITLE:

**VEHICLE MANEUVERING
EXHIBIT**

DRAWING:

I OF I



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ALTMAN &
DILEO, LLP

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OCT 12 2022

Town of Brookhaven
Town Clerk's Office

538 BROADHOLLOW ROAD | SUITE 301
MELVILLE, NEW YORK 11747

TEL 516-222-0222 FAX 516-222-0322
KSDA@BROWNALTMAN.COM WWW.BROWNALTMAN.COM

October 12, 2022

VIA FEDERAL EXPRESS

Karen Sullivan, Sr. Office Assistant
Office of the Town Clerk
Brookhaven Town Hall
1 Independence Hill
Farmingville, NY 11738

Re: Town of Brookhaven (the "Town") Town Board Application for a Change of Zone, Special Use Permits and Waivers of the Special Permit Criteria (the "Application") by 7-Eleven, Inc. ("7-Eleven") and Franklin Johnson, Inc. ("FJ Inc.") to develop a gasoline fueling station and accessory convenience store at the property located at 1481 William Floyd Parkway, Shirley, New York; SCTM No.: Dist. 0200, Sec. 642.00, Bk. 03.00, Lots 41.3, 41.4 and 41.5 (the "Property")
Our File: 100-552-002
Town Log No.: 2021-018

Dear Karen:

In response to the December 22, 2021 Town Planning Division review letter ("December Planning Letter") regarding the Application, enclosed you will find the following documents:

1. One (1) copy of the December Planning Letter;
2. Two (2) copies of a response letter to the December Planning Letter, prepared by Stonefield Engineering ("Stonefield"), dated October 5, 2022 (the "*Stonefield Response Letter*");
3. Six (6) copies of a revised page 2 to the Land Use Application (Form PL-01);
4. Six (6) copies of an updated Schedule "A" to the Land Use Application, last revised October 7, 2022;
5. Seven (7) copies of a Full Environmental Assessment Form, prepared on October 5, 2022 by Zachary Chaplin, P.E. of Stonefield;
6. Seven (7) copies of a metes and bounds description for the Property;
7. Seven (7) copies of a metes and bounds description for the contiguous vacant parcel (0200-642-3-41.1), which is owned by FJ Inc. (the "*Contiguous Parcel*");
8. Seven (7) copies of a Survey for the Property, prepared by Stonefield, dated May 25, 2021, last revised July 29, 2022 (Sheet 1 of 2);
9. Seven (7) copies of a Survey for the Contiguous Parcel, prepared by Stonefield, dated May 25, 2021, last revised July 29, 2022 (Sheet 2 of 2);
10. Three (3) copies of a Key Map (4x4), prepared by Stonefield, dated September 13, 2022;
11. Two (2) copies of a revised draft Notice of Application;



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12. Two (2) copies of a revised draft Notice of Public Hearing;
13. Seven (7) full size copies of the following drawings, prepared by Stonefield, dated August 30, 2021, last revised September 13, 2022, which includes the following sheets:
 - a. Site Plan (C-4);
 - b. Landscaping Plan (C-10); and
 - c. Landscaping Details (C-11).
14. Seven (7) copies of a Vehicle Maneuvering Exhibit (1 of 1), prepared by Stonefield, dated September 13, 2022;
15. Three (3) copies of an Environmental Planning Assessment for Proposed Redevelopment of 7-11 Convenience Store with Fuel Sales, prepared by Nelson Pope and Voorhis ("NPV"), dated October 3, 2022 (the "*NPV Planning Report*"); and
16. Three (3) copies of a Traffic Impact Study, prepared by Stonefield, dated September 28, 2022, which is annexed as Appendix "B" to the NPV Planning Report.

Below is the Applicant's response to the December Planning Letter (Town's comments are italicized). All responses in this letter are intended to supplement the Applicant's responses included in the Stonefield Response Letter, the NPV Planning Report and all documentation submitted herewith.

Comment: INVOLVED AGENCIES & COMMUNITY

Response: Comment acknowledged.

TOWN CODE CHANGE OF ZONE AND SPECIAL PERMIT APPLICATION REQUIREMENTS

Comment 1: The application materials provided fail to address the existing split J-2 and A-1 zoning. Revise all application documents accordingly.

Response 1: Comment acknowledged. Included with this submission are the following documents that address the split J-2 and A-1 zoning:

1. Page 2 of the Land Use Application (Form PL-01);
2. Site Plan (C-4), prepared by Stonefield, last revised September 13, 2022;
3. Full Environmental Assessment Form, prepared by Stonefield, dated October 5, 2022;
4. Key Map, prepared by Stonefield, September 13, 2022
5. Notice of Application letter; and
6. Notice of Hearing letter.



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Comment 2: Provide a detailed analysis of compliance or lack thereof with all of the special permit criteria for each special permit use, summarized in the compliance table on the concept plan and detailed in narrative form. Clearly request waivers and submit fees for all areas of non-compliance.

Response 2: The Site Plan (C-4), prepared by Stonefield, last revised September 13, 2022 has been updated to delineate compliance or lack thereof with all special permit criteria.

It should also be noted that the NPV Planning Report included with this submission provides a narrative for each special permit, waiver and variance request for this project.

Comment 3: Town Code § 85-81.A(3) requires a conceptual plan that indicates the proposal's conformance with Town Code development standards.

Response 3: The Site Plan (C-4), prepared by Stonefield, last revised September 13, 2022 has been updated to include the Town Code development standards.

Comment 4: Provide a survey of the subject and contiguous property such that the metes and bounds can be verified.

Response 4: Included with this submission are the following documents:

1. Metes and bounds description for the Property;
2. Survey for the Property, prepared by Stonefield, dated May 25, 2021, last revised July 29, 2022 (Sheet 1 of 2);
3. Metes and bounds description for the Contiguous Parcel; and
4. Survey for the Contiguous Parcel, prepared by Stonefield, dated May 25, 2021, last revised July 29, 2022 (Sheet 2 of 2).

Comment 5: Address all Law Department comments regarding sufficiency of the application.

Response 5: Comment acknowledged. The Applicant has included a revised Notice of Application Letter and revised Notice of Hearing Letter for review by the Town Law Department.



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PLANNING DIVISION COMMENTS

Significant Concept Plan Issues

Comment A: The split-zoning of the project site has not been recognized as noted above.

Response A: Comment acknowledged. All submission materials have been revised accordingly.

Comment B: The project site does not comply with the lot area or rear yard requirements of the zoning district being sought, or with the special permit criteria of the special permit uses being sought.

Response B: Comment acknowledged. Section 4.0 of the NPV Planning Report includes a detailed analysis for each variance and special permit criteria waiver request.

Comment C: The project site falls within the Compatible Growth Area of the Central Pine Barrens and therefore is subject to the provisions and Development Standards of the Town Code. Demonstrate compliance and provide a detailed Pine Barrens Development Standards compliance table with clearing calculations, re-vegetation plan, if necessary, and consult with the Central Pine Barrens Joint Planning & Policy Commission as to necessity for submission of a hardship application.

Response C: Comment acknowledged and addressed in the NPV Planning Report. Section of the NPV Planning Report discusses the Project as it relates to the Town Code Development Standards for development within the Compatible Growth Area. As further provided in § 5.0(E) of the NPV Planning Report, the Property was developed prior to the 1993 enactment of the Pine Barrens Regulations and approximately 1,000 sf (or 0.2 acres) of the Property remains undeveloped. It should be noted that "the proposed project will occupy all of the previously-developed area and will retain the natural vegetation that currently exists on site. . . . No clearing of the 1,000 SF of [existing] natural vegetation will occur." See NPV Planning Report § 5.0(E) (page 5-5).



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Comment D: Based on non-compliance with the dimensional and special permit criteria of the Town Code, and with the Central Pine Barrens Compatible Growth Area Guidelines & Standards, the Planning Division recommends resolving these issues by adding commonly owned tax lot 4.1 to the subject application.

Response D: As provided in the Stonefield Response Letter, the project has been revised to further comply with the special permit and dimensional criteria of the Town Code.

As previously noted, and further discussed in the NPV Planning Report, the Property was cleared prior to the enactment of the Pine Barrens Regulations. The Applicant will not be clearing the +/- 1,000 sf of existing natural vegetation. The NPV Planning Report also provides a detailed analysis for each waiver/variance requested for this Application. We note that granting variance and special permit waivers will not change the character of the neighborhood nor will it create a detriment to any nearby properties.

Comment E: Reducing the scale of the proposal to provide closer compliance with the Town Code requirement is recommended. Additional landscaping in favor of parking or asphalt facing the roadways; Elimination of the striped asphalt area leading to the dumpster enclosure in favor of landscaping along the road frontage (and as a buffer to the residence across the street); Recommend reduction of number of gas pumps/canopy size in favor of additional landscaping along the road frontage.

Response E: The plans have been revised as noted in the Stonefield Response Letter.

Comment F: The Suffolk County Department of Public Works recommends that the access on Moriches Middle Island Road be eliminated. Recommend reach out to the Suffolk County Department of Public Works, Town Highway and Traffic Safety Departments on this important matter of site access. Closure of this curb cut and provision of landscaping would be favorable due to residential zoning and uses across the street.

Response F: As provided in the Stonefield Response Letter, the driveway connection to Moriches-Middle Island Road has been modified and will be signed to restrict left-turn ingress to the site. The same discussed with and agreed by the Suffolk County Department of Public Works.



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Comment G: Provide a parking analysis, based on ITE parking generation data and actual parking demand at the subject store and similarly situated stores. Additional landscaping would be favored over an excess of parking, particularly facing the intersection of William Floyd Parkway and Moriches Middle Island Road. Consider landbanking unnecessary stalls.

Response G: Included with this submission is a Traffic Impact Study, prepared by Stonefield, dated September 28, 2022. The same is annexed as Appendix "B" to the NPV Planning Report.

It should also be noted that the Applicant has converted three (3) additional parking stalls into landbanked stalls, which enhances the landscaping area along the William Floyd Parkway and Moriches Middle Island frontages.

Other Concept Plan Comments

Comment H: Clearly depict, label and quantify all existing and proposed zoning areas for each differently zoned area. This is important not only for analysis purposes, but also for purpose of intensification fee calculation.

Response H: The Site Plan has been revised accordingly.

Comment I: Compliance with the special permit criteria requires compliance with the land development standard buffers (which applies to both special permit requests). Not only is there non-compliance with the required buffers but also with the 40' required rear yard. Please address/correct.

Response I: The southerly property line is now referenced as the rear yard. The Applicant will request waivers from the special permit criteria and a variance from the rear yard setback requirement.

As further provided in the NPV Report, a 25.0' rear yard setback will not change the character of the neighborhood or cause a detriment to the surrounding properties. It should be noted that the Applicant will not be removing any of the existing natural vegetation currently on-site. In addition, the adjacent property (Lot 41.1) is held in common ownership as the Property. As such, the property owner most directly impacted by the 25.0' rear yard setback will not be impacted. Further, the Applicant will be enhancing overall site landscaping in the area surrounding the new convenience store. As depicted in the Landscaping Plan, the Applicant will be installing new trees and shrubs along the rear yard as well as the westerly property line to screen the convenience store to mitigate any potential impact.



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Comment J: Demonstrate adequate path for fuel truck deliveries with truck turning template overlay.

Response J: Comment acknowledged. A Vehicle Maneuvering Exhibit is included with this submission.

Comment K: Demonstrate compliance with the special permit criterion for fencing adjacent to residential.

Response K: The project will replace the existing 4-foot high chain-link fencing along the southern (rear) and the western (Revilo Avenue frontage) property lines with opaque, 6-foot tall vinyl fencing. The new 6-foot tall opaque fence will be installed along the southerly property line and then extend to the north, along the westerly property line for a distance of 100 linear feet.

However, the project will not include opaque fencing along the remainder of its western property line or along its northern property line (Moriches Middle Island Road frontage), in order to provide open and appropriate views of the front of the site and facility for passing drivers. It is not feasible for the Applicant to achieve this benefit without a waiver from this special permit criterion. The Property will be attractively landscaped, and the overall landscaping exceeds the Town Land Development Standards as it relates to the minimum required site landscaping and the minimum front yard landscaping. See Town Code § 85-843(A)(1)-(2).

Comment L: Clearly provide & label the dimensions relative to the curb cut criteria in Town Code § 85-467.G.(8) on the concept plan.

Response L: Comment acknowledged. The Site Plan has been revised accordingly.

Comment M: Provide a legend for all symbols used on the concept plan. What is the curved striped planted area between the building and William Floyd Parkway next to the sidewalk? What is the striped area surrounded by plants north of the building in a landscape island?

Response M: Comment acknowledged. As provided in the Stonefield Letter, the areas in question are landscape features. Symbols have been included in the legend.



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Architectural Plan Comments

Comment N: Provide a floor plan to verify the retail and display area, showing and labeling retail and display area vs. building area, and to verify the special permit criterion requiring a public restroom.

Response N: A floor plan will be provided under separate cover.

Comment O: Please advise if you are planning a basement for the proposed structure, as basements and storage areas are included in the computation of Commercial Gross Floor Area (GFA) (§ 85-1- Definitions).

Response O: No basement is proposed.

Comment P: Architectural elevations of the proposed structures may facilitate consideration of the application.

Response P: Architectural elevations will be provided under separate cover.

Intensification Fee and Pine Barrens Credit Redemption Comments

Response: Comments acknowledged.

If you have any questions concerning the enclosed submission documents, please do not hesitate to contact me at 516-222-0222 or by e-mail to kbrown@brownaltman.com and mingber@brownaltman.com. Thank you.

Very truly yours,

BROWN ALTMAN & DILEO, LLP

By: 

Keith P. Brown, Esq.

Encls.

cc: Stonefield Engineering (attn.: Zachary Chaplin, P.E., Amanda LaRosa, P.E. and Brian Riley w/o encls.)

Nelson Pope & Voorhis (attn.: Kathryn J. Eiseman, AICP w/o encls.)

Gemstar Construction (attn.: Keri Rosalia w/o encls)



Town of Brookhaven Long Island

Edward P. Romaine, Supervisor

RECEIVED

OCT 12 2022

Town of Brookhaven
Town Clerk's Office

December 22, 2021

Ralph Cozza, Vice President
Franklin Johnson, Inc.
3605 Merrick Road
Seaford, NY 11783

RE: Town Board Application: 7-Eleven @ Shirley, Log # 2021-018-CZ
Proposed change of zone from J2 and A1 to J5 and special permits and special permit waivers for proposed demolition of existing 7-Eleven convenience store and proposed new motor vehicle fueling station and accessory convenience store with associated site improvements.
1481 William Floyd Parkway, S/W/C William Floyd Parkway (CR 46) and Moriches Middle Island Road, and E/S Revilo Avenue, Shirley
SCTM: 0200 64200 0300 041003, 41004 & 41005, 1.38 acres

Dear Mr. Cozza:

This letter is to inform you that the Town of Brookhaven Planning Division has received and processed your Town Board application. A review of your submission finds that several items listed below are necessary to complete your application.

Your Town Board application has been given a Town tracking log number: **2021-018-CZ**. Be also advised that **Amy Moody** from the Planning Division's **Blue Team** has been assigned to review your application. Please note that the Planning Division utilizes your parcel's Suffolk County Tax Map number to file your application. Therefore, please include both the Town's log number and your parcel's Suffolk County Tax Map number in any future correspondences or submissions to the Town Clerk.

INVOLVED AGENCIES & COMMUNITY:

- 1) Be advised that in accordance with Town policy, you are requested to reach out and meet with the local civic organizations in your proposed project area. It is recommended that you provide the local civic organizations with a copy of your site plan as well as typical application materials and supporting documentation before the meeting. For your convenience, both the Town Clerk and the local Town Council office maintain a current list of civic organizations and the appropriate contact person(s). Upon completion of this meeting please submit a letter, with a copy to the local civic organization, summarizing the conclusions of the meeting including the issues in which a consensus was reached as well as those issues that still remain. In addition, as a courtesy, please contact the local Town Council representative.
- 2) As a routine procedure, your application has been sent to the following agencies for their review and consideration and we are awaiting their response(s):
 - Division of Traffic Safety.
 - Division of Environmental Protection.
 - Suffolk County Department of Health Services.
 - Central Pine Barrens Joint Planning & Policy Commission.

Planning, Environment and Land Management
Beth Ann Reilly, Esq., Commissioner

One Independence Hill • Farmingville • NY 11738 • Phone (631) 451-6400 • Fax (631) 451-6419
www.brookhaverny.gov

- 3) As a routine procedure, your application has been sent to the following agencies and they have responded – their comments are attached hereto and should be addressed with any re-submission: Division of Fire Prevention; Department of Highway; Suffolk County Department of Public Works.
- 4) Once your application is determined to be complete and contains a “full statement of facts”, your application will be referred pursuant to New York State Town Law and the Suffolk County Charter to the Suffolk County Planning Commission for recommendation and comments.

TOWN CODE CHANGE OF ZONE AND SPECIAL PERMIT APPLICATION REQUIREMENTS:

- 1) The application materials provided fail to address the existing split J-2 & A-1 zoning. Revise all application documents including application form, concept plan, 4 x 4, notice, etc. accordingly.
- 2) Provide a detailed analysis of compliance or lack thereof with all of the special permit criteria for each special permit use, summarized in the compliance table on the concept plan and detailed in narrative form. Clearly request waivers and submit fees for all areas of non-compliance.
- 3) Town Code § 85-81.A.(3) requires a conceptual plan (your site plan) that indicates the proposal's conformance with Town Code development standards. It is therefore requested that a Town Code development standards compliance table be provided for the following sections of Town code:
 - § 85-466. Dimensional criteria for the J-5 Business District. The southerly property line is a REAR yard. Please correct.
 - § 85-467.B & G. Address all of the special permit criteria in the compliance table. Required buffers to residential across the street are not met, and there is additional non-compliance to the buffering provisions as the southerly lot line is a REAR yard.
 - § 85-843: Land development standards.
- 4) Provide a survey of the subject & contiguous property such that the metes and bounds descriptions can be verified. It is noted that there are discrepancies between the metes and bounds on the concept plan and the descriptions provided; please verify the accuracy of the metes and bounds descriptions to the property survey before submitting. Please note that metes and bounds descriptions need to be provided for all areas to be re-zoned, and need to be able to be verified on surveys.
- 5) Address all Law Department comments regarding sufficiency of the application.

PLANNING DIVISION COMMENTS:

Significant Concept Plan Issues:

- A) The split-zoning of the project site has not been recognized as referenced above.
- B) The project site does not comply with the lot area or rear yard requirements of the zoning district being sought, or with the special permit criteria of the special permit uses being sought.
- C) The project site falls within the Compatible Growth Area of the Central Pine Barrens and therefore is subject to the provisions and Development Standards of Town Code. Demonstrate compliance and provide detailed Pine Barrens Development Standards compliance table with clearing calculations, re-vegetation plan if necessary, and consult with the Central Pine Barrens Joint Planning & Policy Commission as to necessity for submission of a hardship application.
- D) Based on non-compliance with the dimensional and special permit criteria of Town Code, and with Central Pine Barrens Compatible Growth Area Guidelines & Standards, the Planning Division recommends resolving these issues by adding commonly owned tax lot 41.1 to the subject application.
- E) Reducing the scale of the proposal to provide closer compliance with Town Code requirements is recommended: Additional landscaping in favor of parking or asphalt facing the roadways; Elimination of the striped asphalt area leading to the dumpster enclosure in favor of landscaping along the road frontage (and as a buffer to the residence across the street); Recommend reduction of number of gas pumps/canopy size in favor of additional landscaping along the road frontages.

- F) The Suffolk County Department of Public Works recommends that the access on Moriches Middle Island Road be eliminated. Recommend reaching out to the Suffolk County Department of Public Works, Town Highway and Traffic Safety Departments on this important matter of site access. Closure of this curb cut and provision of landscaping would be favorable due to residential zoning and uses across the street.
- G) Provide a parking analysis, based on ITE parking generation data and actual parking demand at the subject store and similarly situated stores. Additional landscaping would be favored over an excess of parking, particularly facing the intersection of William Floyd Parkway and Moriches Middle Island Road. Consider landbanking unnecessary stalls.

Other Concept Plan Comments:

- H) Clearly depict, label, and quantify all existing and proposed zoning areas for each differently zoned area. This is important not only for analysis purposes, but also for purpose of intensification fee calculation.
- I) Compliance with the special permit criteria that requires compliance with the land development standard buffers (which applies to both special permits requested) is referenced – which is not the case. Not only is there non-compliance with required buffers but also with the 40' required rear yard. Please address/correct.
- J) Demonstrate adequate path for fuel truck deliveries with truck turning template overlay.
- K) Demonstrate compliance with the special permit criterion for fencing adjacent to residential.
- L) Clearly provide & label the dimensions relative to the curb cut criteria in Town Code §85-467.G.(8) on the concept plan.
- M) Provide a legend for all symbols used on the concept plan. What is the curved striped planted area between the building and William Floyd parkway next to the sidewalk? What is the striped area surrounded by plants north of the building in a landscape island?

Architectural Plan Comments:

- N) Provide a floor plan to verify the retail and display area, showing and labeling retail and display area vs. building area, and to verify special permit criterion requiring a public restroom.
- O) Please advise if you are planning a basement for the proposed structure, as basements and storage areas are included in the computation of Commercial Gross Floor Area (GFA) (§85-1, Definitions).
- P) Architectural elevations of the proposed structures may facilitate consideration of the application.

Intensification Fee & Pine Barrens Credit Redemption Comments:

- Q) In accordance with Town Code §85-82 entitled Land Use Intensification Mitigation Fee, be advised that this proposed change of zone application if approved will require a land use intensification fee based on the acreage being re-zoned in the amount of \$TBD *(additional information is required for purpose of calculating the fee as referenced above)*. This fee will be a condition of final site plan approval and payable to the Town of Brookhaven, Joseph Macchia Environmental Preservation Capital Reserve Fund at that time. In accordance with 85-82.F. a minimum of 50% of the required land use intensification mitigation fee shall be submitted prior to the effective date of the change of zone. The submission of the outstanding land use intensification mitigation fee shall be a condition of final site plan approval.
- R) Be advised that pursuant to Town Code §85-467.B.(3)(a) & (b), Pine Barrens Credit redemption will be required for the proposed building area increase.

Note that if you are not able to comply with any of the above staff comments, your revised submission should provide a brief synopsis of the rationale for the non-compliance.

Please address the above information and submit to the Town of Brookhaven Clerk's office as soon as possible so that your assigned review analyst can review a complete application.

Very truly yours,

Beth Ann Reilly, Commissioner

A handwritten signature in cursive script that reads "Amy Moody".

By: _____
Amy Moody, Planner
Planning Division

BR:am

Encl. (Agency Comments)

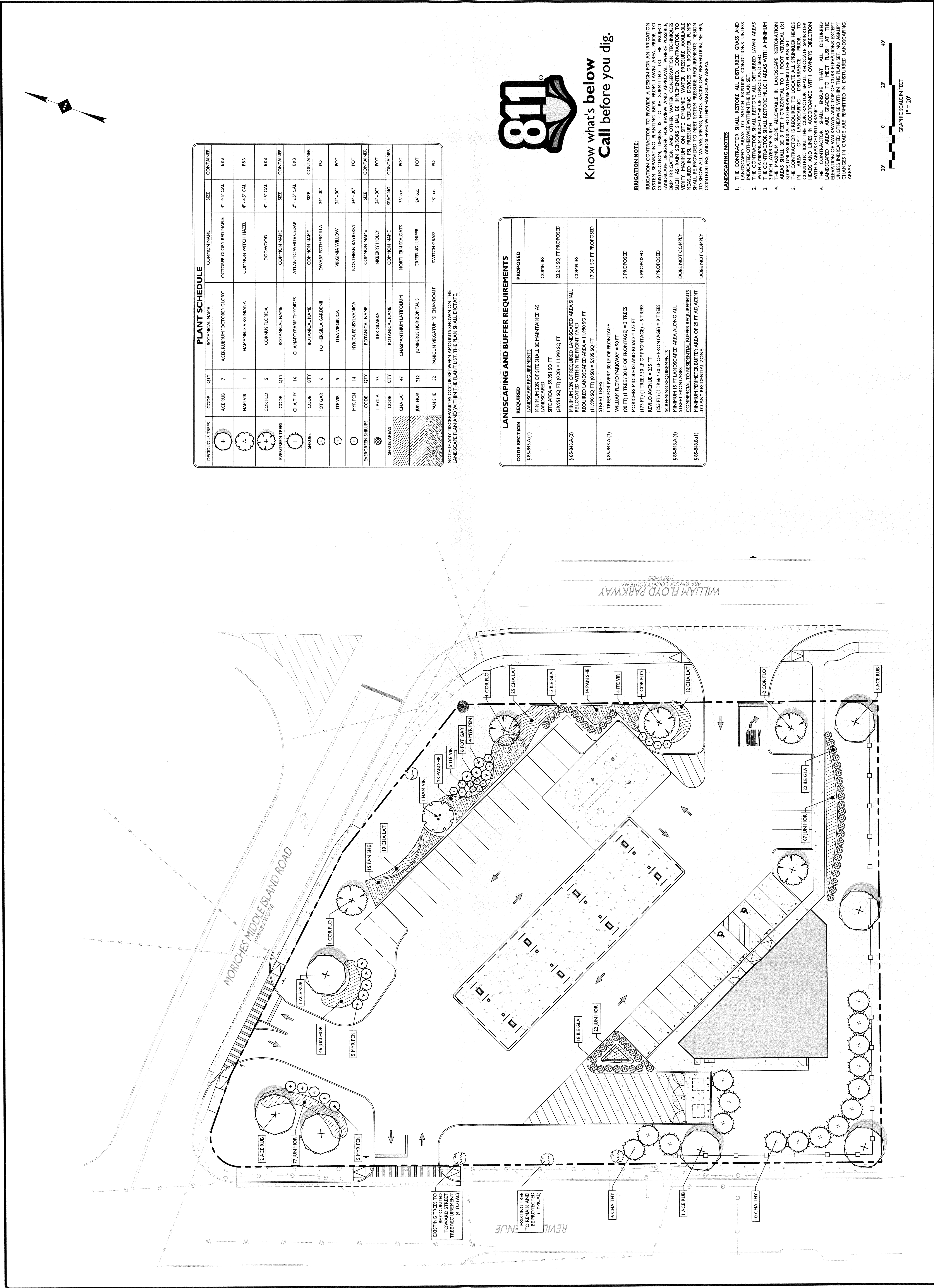
CC: 7-Eleven, Inc.

Keith P. Brown, Esq., Brown & Altman, LLP

Zachary Chaplin, PE, Stonefield Engineering

Donna Lent, Town Clerk

Leigh Rate, Assistant Town Attorney



7-ELEVEN, INC

PROPOSED CONVENIENCE STORE

WITH FUEL SALES

7-ELEVEN

Logo

SECTION 642, BLOCK 3, LOT 41.3, 41.4 & 41.5
HAMLET OF SHIRLEY, TOWN OF BROOKHAVEN
SUFFOLK COUNTY, NEW YORK

STONEFIELD

engineering & design

Rutherford, NJ • New York, NY • Boston, MA
Princeton, NJ • Tampa, FL • Detroit, MI
www.stonefelddesign.com
Phone 718.606.8305

584 Broadway, Suite 310, New York, NY 10012

NOT APPROVED FOR CONSTRUCTION

04

09/13/2022

BR

PLANNING DIVISION RESUBMISSION

03

05/2/2022

BR

UPDATED PER TOWN AND COUNTY COMMENTS

02

11/09/2021

BR

ISSUED FOR REVIEW

01

08/30/2021

BR

DATE

BY

DESCRIPTION

SCALE: 1" = 20'

PROJECT ID: K-18112

LANDSCAPING PLAN

0.1" = 1'-0" (SEE DETAIL)

Town of Brookhaven
Town Clerk's Office

C-10

811

Know what's below you dig.
Call before you dig.

IRRIGATION NOTE:

IRRIGATION CONTRACTOR TO PROVIDE A DESIGN FOR AN IRRIGATION SYSTEM SEPARATING PLANTING BEDS FROM LAWN AREA. PRIOR TO CONSTRUCTION, DESIGN IS TO BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW. DESIGN SHALL INCLUDE MAIN LINES, SUB MAINS, DRIPIRIGATION AND OTHER WATER CONSERVATION TECHNIQUES SUCH AS RAIN SENSORS SHALL BE IMPLEMENTED. CONTRACTOR TO VERIFY MAXIMUM ON-SITE DYNAMIC WATER PRESSURE AVAILABLE PRIOR TO CONSTRUCTION. CONTRACTOR SHALL PROVIDE PRESSURE REQUIREMENTS TO SHOW ALL VALVES, PIPING, HEADS, BACKFLOW PREVENTION, METERS, CONTROLLERS, AND SLEEVES WITHIN HARDSCAPE AREAS.

LANDSCAPING NOTES

1. THE CONTRACTOR SHALL RESTORE ALL DISTURBED GRASS AND LANDSCAPED AREAS TO MATCH EXISTING CONDITIONS UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET.
2. THE CONTRACTOR SHALL RESTORE ALL DISTURBED LAWN AREAS WITH A MINIMUM 1/4 INCH LAYER OF TOPSOIL AND SEED.
3. THE CONTRACTOR SHALL RESTORE MULCH AREAS WITH A MINIMUM 3 INCH LAYER OF MULCH.
4. AREAS SHALL BE 3 FEET HORIZONTAL TO 1 FOOT VERTICAL (3:1) SLOPE UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET.
5. THE CONTRACTOR SHALL RESTORE ALL DISTURBED HEADS OF CONSTRUCTION. THE CONTRACTOR SHALL RELOCATE SPRINKLER HEADS AND LINES IN ACCORDANCE WITH OWNER'S DIRECTION.
6. THE CONTRACTOR SHALL ENSURE THAT ALL DISTURBED LANDSCAPED AREAS ARE GRADED TO MEET FLUSH AT THE EXISTING GRADE. THE CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS TO MATCH EXISTING CONDITIONS UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET. NO ARBITRARY CHANGES IN GRADE ARE PERMITTED IN DISTURBED LANDSCAPING AREAS.

GRAPHIC SCALE IN FEET

1" = 20'

0' 20' 40'

PLANT SCHEDULE					
DECIUOUS TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	ACE RUB	7	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY RED MAPLE	4" - 4.5" CAL
	HAM VIR	1	HAMAMELIS VIRGINIANA	COMMON WITCH HAZEL	4" - 4.5" CAL
	COR FLO	5	CORNUS FLORIDA	DOGWOOD	4" - 4.5" CAL
EVERGREEN TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	CHA THY	16	CHAMAECTIPARIS THYOIDES	ATLANTIC WHITE CEDAR	2" - 2.5" CAL
SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	POT GAR	6	FOTHEBIGILLA GARDNERI	DWARF FOTHEBIGILLA	24" - 30"
	ITE VIR	9	ITEA VIRGINICA	VIRGINIA WILLOW	24" - 30"
	MYR PEN	14	MYRICA PENSYLVANICA	NORTHERN BAYBERRY	24" - 30"
EVERGREEN SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	ILE GLA	53	ILEX GLABRA	INKBERY HOLLY	24" - 30"
SHRUB AREAS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SPACING
	CHA LAT	47	CHASANTHILUM LATIFOLIUM	NORTHERN SEA OATS	36" o.c.
	JUN HOR	212	JUNIPERUS HORIZONTALIS	CREeping JUNIER	24" o.c.
	PAN SHE	52	PANICUM VIRGATUM 'SHEANDOAH'	SWITCH GRASS	48" o.c.

NOTE: IF ANY DISCREPANCIES OCCUR BETWEEN AMOUNTS SHOWN ON THE LANDSCAPE PLAN AND WITHIN THE PLANT LIST, THE PLAN SHALL DICTATE.

LANDSCAPING AND BUFFER REQUIREMENTS		
CODE SECTION	REQUIRED	PROPOSED
\$ 85-843 A.(1)	LANDSCAPE REQUIREMENTS	
	MINIMUM 30% OF SITE SHALL BE MAINTAINED AS LANDSCAPED SITE AREA = 99,951 SQ FT (99,951 SQ FT) (0.20) = 11,990 SQ FT	COMPLIES 23,215 SQ FT PROPOSED
\$ 85-843 A.(2)	MINIMUM 5% OF REQUIRED LANDSCAPED AREA SHALL BE LOCATED WITHIN THE FRONT YARD REQUIRED LANDSCAPED AREA = 11,990 SQ FT (11,990 SQ FT) (0.50) = 5,995 SQ FT	COMPLIES 17,361 SQ FT PROPOSED
\$ 85-843 A.(3)	STREET TREES 1 TREE FOR EVERY 30 LF OF FRONTAGE WILLIAM FLOYD PARKWAY = 90 FT (90 FT) (1 TREE / 30 LF OF FRONTAGE) = 3 TREES MORICHES MIDDLE ISLAND ROAD = 173 FT (173 FT) (1 TREE / 30 LF OF FRONTAGE) = 5 TREES REVELA AVENUE = 255 FT (255 FT) (1 TREE / 30 LF OF FRONTAGE) = 9 TREES	3 PROPOSED 5 PROPOSED 9 PROPOSED
\$ 85-843 A.(4)	SCREENING REQUIREMENTS MINIMUM 15 FT LANDSCAPED AREA ALONG ALL COMMERCIAL TO RESIDENTIAL BUFFER REQUIREMENTS MINIMUM PERIMETER BUFFER AREA OF 25 FT ADJACENT TO ANY RESIDENTIAL ZONE	DOES NOT COMPLY DOES NOT COMPLY

RECEIVED

Print Form

SEP 23 2021



Town of

Brookhaven

Long Island, New York

Town of Brookhaven
Town Clerk's Office

Land Use Application

One Independence Hill, Farmingville, NY 11738

Form PL-01 rev E 1/1/2019

Page 1 of 11

Please check the appropriate application request:

1. TOWN BOARD:

- ☐ 1a. AMENDMENT OF RESTRICTIVE COVENANT (TBR)
- ☒ 1b. CHANGE OF ZONE (CZ)
- ☐ 1c. PLANNED DEVELOPMENT DISTRICT (PDD)(CZ)
- ☒ 1d. SPECIAL PERMIT (CZ)

Case Number:

Application Date:

Town Use Only

2. PLANNING BOARD:

- ☐ 2a. AMEND RESTRICTIVE COVENANT (Relief of Covenant) PBR
- ☐ 2b. FINAL SUBDIVISION - FS
- ☐ 2c. LAND DIVISION - LD
- ☐ 2d. PLANNING BOARD VARIANCE
- ☐ 2e. PRELIMINARY FINAL SUBDIVISION - FS
- ☐ 2f. PRELIMINARY SUBDIVISION - PS
- ☐ 2g. ROAD IMPROVEMENT/RESUBDIVISION - RI
- ☐ 2h. SINGLE FAMILY RESIDENCE - SF
- ☐ 2i. SITE PLAN- SP
- ☐ 2j. SPECIAL PERMIT - SP
- ☐ 2k. 278 CLUSTER TREATMENT (Submitted w/PS, FS, PF, LD or RI)
- ☐ 2l. OTHER:

Application is hereby made to the Town of Brookhaven for the application type requested.

By application submittal, the applicant does hereby authorize employees or agents of the Town of Brookhaven to enter and inspect the project site as necessary in conjunction with this application.

3. PLANNING DIVISION:

- ☐ 3a. CHANGE OF USE - CU
(including facade and minor additions up to 500 sf)
- ☐ 3b. FIRE/AMBULANCE, ETC. - OM
- ☐ 3c. TEST HOLE - TH
- ☐ 3d. TREE CLEARING - TC
- ☐ 3e. REVEGETATION PLAN- RV

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Long Island, New York

OCT 12 2022

Town of Brookhaven
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I. GENERAL APPLICATION: A. PROPERTY LOCATION:

Suffolk County Tax Map (SCTM) Property Number: (Use "Scope of Work" section below to list any additional SCTM #'s)

DISTRICT	SECTION	BLOCK	LOT
0200	642	3	41.3, 41.4, 41.5

Name of Application: 7-Eleven Shirley

Located at #: 1481, on the ☐ N ☐ E ☐ S ☒ W side of: William Floyd Parkway

Distance: c/o, ☐ N ☐ E ☒ S ☐ W of: Moriches Middle Island Road

Hamlet: Shirley

Post Office: Shirley

Ambulance District: Not Applicable

School District: Longwood CSD

Fire District: Ridge

*Total Proposed S.F. of Building(s): 2,995

Property Size (Acres): 1.38 OR Square Feet: 60,112.8

Disturbed Property Size (Acres): 1.38 OR Square Feet: 60,112.8

Present Zoning/Use of Site: J Business 2 and A Residential 1

Proposed Zoning/Use of Site: J Business 5

Name of Subdivision: N/A

Subdivision Lot Number: N/A

☐ Yes ☒ No Does the property in question conform to the lot area requirement?

Scope of Proposed Work: (Please list all SCTM #'s associated with application)

For the property located at 1481 William Floyd Parkway, Shirley, NY, designated as SCTM Nos.: 0200-642-3-41.3, 41.4 and 41.5, the applicant seeks to raze the existing 7-Eleven convenience store and develop a motor vehicle fueling station, install underground fuel storage tanks, and develop a new 2,995 square foot 7-Eleven convenience store and associated site improvements, all as depicted on the Site Plan prepared by Stonefield Engineering submitted herewith and incorporated by reference herein.



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B. PROPERTY OWNER/ENTITY CONSENT: (separate sheets may be used for multiple owners)

Be advised that I am the owner of record of the property referenced herein and hereby consent to this application. By this application, the owner does hereby authorize employees or agents of the Town of Brookhaven, in conjunction with this application, to enter and inspect the project site as necessary.

Owner/Entity Name: Franklin Johnson, Inc.

Firm Name: N/A

Address: 3605 Merrick Road

Hamlet: Seaford State: NY Zip: 11783 Tel#: (516) 222-0222

E-mail: kbrown@brownaltman.com Fax#: (516) 222-0322

If corporation, name of responsible officer: Ralph Cozza Title: Vice President

IN WITNESS WHEREOF I have hereto set my hand onto this 2nd day of September, 2021

Franklin Johnson, Inc.

By: Ralph Cozza Vice President

Sign By Owner/Officer

Name: Ralph Cozza Title: Vice President

STATE OF NEW YORK)

COUNTY OF SUFFOLK

On the 2nd day of September in the year 2021 before me, the undersigned, personally appeared Ralph Cozza personally known to me or proved to me on the basis of satisfactory evidence to be the individual(s) whose name(s) is (are) subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their capacity (ies), and that by his/her/their signature(s) on the instrument, the individual(s), or the person upon behalf of which the individual(s) acted, executed the instrument.

Denise A. Henry
Notary Public

DENISE A. HENRY
Notary Public, State of New York
Reg. No. 01HE5022736
Qualified in Nassau County
Commission Expires January 18, 2022



Town of
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Long Island, New York

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C. APPLICANT/CONTRACT VENDEE/LESSEE

(If same as owner, state in name field below)

Name: Firm Name:
Street Address:
City: State: Zip: Tel#:
E-mail: Fax#:

D. PLAN PREPARER

Name: Firm Name:
Street Address:
City: State: Zip: Tel#:
E-mail: Fax#:

E. ATTORNEY/AGENT (If applicable):

Name: Firm Name:
Street Address:
City: State: Zip: Tel#:
E-Mail: Fax#:

F. Removal of Excess Materials

Engineers Certification:

The site plan or subdivision submitted to the Board depicts an excess of cubic yards, proposed to be removed from the premises.

Name: Phone Number:
E-mail: License Number:

Signature:

Date:



Town of
Brookhaven
Long Island, New York

Land Use Application

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II. BOARD OF ETHICS TRANSACTIONAL DISCLOSURE:

Case Number:

A. APPLICANT

Application Date:

Name:

Address:

City:

State:

Zip:

Tel#:

E-Mail:

Fax#:

☐ Yes ☒ No

Does any officer of the State of New York, officer or employee of the Town of Brookhaven, officer or employee of Suffolk County, officer of a political party in Suffolk County or his or her spouse, brother, sister, parent, child, grandchild, or the spouse of any of them have an interest in this application by virtue of being the actual applicant, or, by virtue of having an interest in the corporation, partnership, or association making such application?

B. If you checked "Yes" above, please complete the following section below:

Interested Party and Nature of Interest:

Name:

Address:

City:

State:

Zip:

Title:

Department:

Relationship to Public Officer/Employee and Title if other than Self:

- ☐ Yes ☐ No 1. Is the owner of greater than five percent (5%) of the corporate stock of the application when the applicant is a corporation whose stock is listed on the New York or American Stock Exchanges,
- ☐ Yes ☐ No 2. The actual applicant,
- ☐ Yes ☐ No 3. An Officer, Director, Partner, or Employee of the applicant, or ,
- ☐ Yes ☐ No 4. Legally or beneficially owns or controls any stock of a non-publicly traded corporate applicant or is a member of a partnership or association of the applicant.

7-Eleven, Inc.

By:

Ian C. Williams

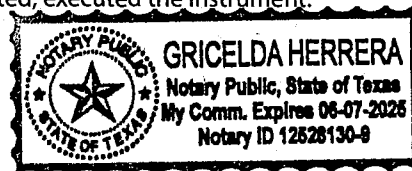
Signature:

Vice President

Print Name:

On the 13th day of September, in the year 2021, before me, the undersigned, a Notary Public in and for said State, appeared Ian C. Williams, personally known to me or proved to me on the basis of satisfactory evidence to be the individual(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their/ capacity(ies), and they by his/her/their signature(s), on the instrument, the individual(s), or the person(s) upon behalf of which the individual(s) acted, executed the instrument.

Notary Public:





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II. BOARD OF ETHICS TRANSACTIONAL DISCLOSURE:

Case Number:

A. OWNER APPLICANT

Application Date:

Name: Address:
City: State: Zip: Tel#:
E-Mail: Fax#:

☐ Yes ☒ No Does any officer of the State of New York, officer or employee of the Town of Brookhaven, officer or employee of Suffolk County, officer of a political party in Suffolk County or his or her spouse, brother, sister, parent, child, grandchild, or the spouse of any of them have an interest in this application by virtue of being the actual applicant, or, by virtue of having an interest in the corporation, partnership, or association making such application?

B. If you checked "Yes" above, please complete the following section below:

Interested Party and Nature of Interest:

Name: Address:
City: State: Zip:
Title: Department:

Relationship to Public Officer/Employee and Title if other than Self:

- ☐ Yes ☐ No 1. Is the owner of greater than five percent (5%) of the corporate stock of the application when the applicant is a corporation whose stock is listed on the New York or American Stock Exchanges,
- ☐ Yes ☐ No 2. The actual applicant,
- ☐ Yes ☐ No 3. An Officer, Director, Partner, or Employee of the applicant, or ,
- ☐ Yes ☐ No 4. Legally or beneficially owns or controls any stock of a non-publicly traded corporate applicant or is a member of a partnership or association of the applicant.

Franklin Johnson, Inc.

Ralph Cozza Title: Vice President

By: Ralph Cozza Vice President

Print Name: Ralph Cozza

Signature: Ralph Cozza

On the 2nd day of September, in the year 2021, before me, the undersigned, a Notary Public in and for said State, appeared Ralph Cozza, personally known to me or proved to me on the basis of satisfactory evidence to be the individual(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their/ capacity(ies), and they by his/her/their signature(s), on the instrument, the individual(s), or the person(s) upon behalf of which the individual(s) acted, executed the instrument.

Notary Public: [Signature]

DENISE A. HENRY
Notary Public, State of New York
Reg. No. 01HE5022736
Qualified in Nassau County
Commission Expires January 18, 2022



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III. A. PROJECT DATA

- ☐ Yes ☒ No 1. Is the property within 500' of the boundary of any village or town? Village/Town
- ☒ Yes ☐ No 2. Within 500' of any existing or proposed County or State Parkway, Thruway, Expressway or highway?
- ☒ Yes ☐ No 3. Within 500' of any existing or proposed boundary of any County, State or Federal owned land?
- ☒ Yes ☐ No 4. Within 500' of any existing or proposed place of public assembly?
- ☐ Yes ☒ No 5. Within 100' of any freshwater or tidal wetland system?
- ☒ Yes ☐ No 6. Within Carmans River Watershed area?
- ☐ Yes ☒ No 7. Within a designated Historic District or Historic District Transition Zone?
- ☐ Yes ☒ No 8. Are there any existing covenants or restrictions affecting the premises for which the approval is sought?
If **Yes**, please attach a copy certified by the Suffolk County Clerk.
- ☐ Yes ☒ No 9. Are there covenants or conditions being offered which would affect the use or development of this property?
- ☒ Yes ☐ No 10. Is the property improved with any structures or signs? If **Yes**, attach a copy of any Certificate of Occupancy(s), Certificates of Existing Use(s), and/or Certificate of Zoning Compliance(s) for all of the existing structures and/or signs.
- ☐ Yes ☒ No 11. Is the property located within the New York State Hydrogeologic Sensitive Zone?
- ☒ Yes ☐ No 12. Is the property located within the New York State designated Central Pine Barrens area?
- ☒ Yes ☐ No 13. Is there any Pine Barrens Credits being purchased?
- ☒ Yes ☐ NO 14. Does the owner/applicant own or have any interest in any contiguous property?
If **Yes**, list the SCTM numbers below:

0200-642.00-03.00-041.001

- ☐ Yes ☒ No 15. Have you applied for Health Department approval for sanitary waste for the proposed use?
- ☒ Yes ☐ No 16. Do any Special Districts or utilities service the site?
If **Yes**, please explain below:

National Grid, Suffolk County Water Authority

- ☐ Yes ☒ No 17. Will there be any use, manufacture, or disposal of any hazardous materials, and/or ground water resources be utilized in any other way other than for normal potable consumption, and/or any air, noise or light emissions occur. If **Yes**, please explain below:

- ☒ Yes ☐ No 18. Is the property located on an improved road?
- ☒ Yes ☐ No 19. Is the road Town maintained?
- ☒ Yes ☐ No 20. Does the proposed property disturb more than one acre of land? If **Yes**, please prepare a Stormwater Pollution Prevention Plan.
- ☐ Yes ☒ No 21. Is the property located within a designated Zoning Overlay District?
- ☐ Yes ☒ No 22. Was the property subject to a public hearing on a change of zone application within the last 12 months?



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III. B. PROJECT DATA: ECONOMIC IMPACTS

Completed for all commercial/industrial projects and residential projects greater than 10 Units
(If not applicable, check here and go to Section IV) ☐ N/A

1. Does project involved Local, State or Federal funding?

☐ Yes ☒ No2. **If single phase project:**

Anticipated period of construction months. (including demolition).

4

3. **If multi-phased:**

a. Total number of phases anticipated.

N/A

b. Expected date of commencement Phase 1. (including demolition)

N/A

c. Approximate completion date of final phase. (month/year)

N/A

d. Is Phase 1 functionally dependent on subsequent phases?

☐ Yes ☐ No

4. Number of jobs generated during construction. (full time equivalent)

20

5. Number of jobs generated after completion. (full time equivalent)

5

6. Number of jobs eliminated by this project.

0

7. What are the current tax revenues generated by the project site?

+/- \$25,000.00

8. What tax revenues will project generate after completion?

+/- \$40,000.00

9. What is the estimated cost of construction?

+/- \$1,000,000.00

10. How many schoolchildren is the project expected to generate? ☒ N/A

11. What is the estimated cost of educating the school-age children generated by the completion of this project?

☒ N/A



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IV. SPECIAL USE PERMITS & VARIANCES:

- A1.** If the proposed project requires a **Special Permit**, please **check** the appropriate **Board** and describe the Special Permit request in the space below:

☒ Town Board ☐ Planning Board ☐ Zoning Board of Appeals

See attached Schedule "A" annexed hereto and made a part hereof.

- B1.** If the proposed project requires **Variances, or Waivers**, please **check** the appropriate **Board**, state the nature of the request and the reason in the space below:

☐ Town Board ☐ Planning Board ☒ Zoning Board of Appeals

See attached Schedule "A" annexed hereto and made a part hereof.

2. VARIANCES/WAIVERS REQUESTED: Please check the type of variance/waiver request and specify the size proposed:

[Note: Upon application review, additional variance/waiver request(s) may be added by Town Application Examiners.]

<input type="checkbox"/> Lot Area	<input type="text" value="See attached Schedule 'A'"/>	<input type="checkbox"/> 1st Story Sq. Ft.	<input type="text"/>
<input type="checkbox"/> Lot Width	<input type="text"/>	<input type="checkbox"/> 2nd Story Sq. Ft.	<input type="text"/>
<input type="checkbox"/> Front Yard Setback	<input type="text"/>	<input type="checkbox"/> Special Permit Criteria	<input type="text"/>
<input type="checkbox"/> Rear Yard Setback	<input type="text"/>	<input type="checkbox"/> Other	<input type="text"/>
<input type="checkbox"/> Side Yard Setback	Minimum <input type="text"/>	Total	<input type="text"/>

3. List the structure(s) requiring variance/waiver(s): Specify whether each structure is **PROPOSED or **EXISTING**:**

<input type="text" value="Convenience Store"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="text" value="Wall and Monument Sign"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="text"/>	<input type="checkbox"/>	<input type="checkbox"/>



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- C1.** If the proposed project requires an **Amendment to a Restrictive Covenant**, please **check** the appropriate **Board** and describe: A) Existing covenant for which relief is sought; B) Description of requested relief; and, C) Reason for requested relief, in the space below:

☐ Town Board

☐ Planning Board

☐ Zoning Board of Appeals

N/A

2. Percentage of current covenant area affected: %. Proposed percentage of covenant area affected: %.

3. Percentage of current buffer area affected: %. Proposed percentage of buffer area affected: %.

V. SUBDIVISION/LAND DIVISION: N/A

A1. Complete for all subdivision/land division/road improvements

Deed(s) recorded in the Suffolk County Clerk's Office:

Number of Lots:

Date:

Liber:

Page:

<input type="text"/>	<input type="text"/>	<input type="text"/>
<input type="text"/>	<input type="text"/>	<input type="text"/>
<input type="text"/>	<input type="text"/>	<input type="text"/>
<input type="text"/>	<input type="text"/>	<input type="text"/>

- ☐ Yes ☐ No 2. Are there encumbrances or liens against this land other than mortgages?
- ☐ Yes ☐ No 3. Will the final plat be filed in sections, or will it cover the entire preliminary layout?
- ☐ Yes ☐ No 4. Are all the public open spaces shown on the layout to be dedicated for public purposes?
5. How many acres are to be dedicated for public park or playground purposes?

Reset Form

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SCHEDULE A

Town of Brookhaven
Town Clerk's Office

IV. Special Use Permits & Variances

A1. Town Board Special Permits:

1. A special permit from the Town Board is requested to operate a motor vehicle fueling station. *See Town Code § 85-460(G).*
2. A special permit from the Town Board is requested to operate a convenience store as an accessory use to a motor vehicle fueling station. *See Town Code § 85-463(A).*

B1. Town Board Waivers:

1. Town Code § 85-467(B)(3): A maximum of 750 square feet of building area shall be devoted to the retail sale and display area, and the total building area shall not exceed 1,500 square feet.
 - Permitted retail and display area: 750 square feet
 - Waiver request: 1,850 square foot retail and display area.
2. Town Code § 85-467(B)(3): A maximum of 750 square feet of building area shall be devoted to the retail sale and display area, and the total building area shall not exceed 1,500 square feet.
 - Permitted building size: 1,500 square feet
 - Waiver request: 2,995 square foot total building area.
3. Town Code § 85-467(B)(7): Buffers and plantings be in accordance with the Town land development standards, except that the rear yard setback shall be 40 feet.
 - Permitted: Minimum 40 foot rear yard buffer
 - Waiver request: 25 foot buffer along rear yard
4. Town Code § 85-467(B)(7): Buffers and plantings be in accordance with the Town land development standards, except that the rear yard setback shall be 40 feet.
 - Permitted: 15 foot landscaped area along all street frontages (*see Town Code § 85-843(A)(4)*).
 - Waiver request: 8.3' feet along William Floyd Parkway
5. Town Code § 85-467(B)(8): A minimum six-foot-high solid (opaque) fence along any property line adjacent to a residential district or use.
 - Waiver request: To not include 6-foot opaque fencing along the northern and western property lines where the same is required along a property line adjacent to residentially zoned land.

6. Town Code § 85-467(G)(9): Screening. Buffers and fencing shall be provided in accordance with the land development standards.
 - Permitted: 15 foot landscaped area along all street frontages (*see* Town Code § 85-843(A)(4)).
 - Waiver request: 8.3' feet along William Floyd Parkway

B1. Planning Board

1. Town Code § 85-843(A)(1)(4): A minimum natural area/landscaping along street frontage. A minimum of 15 feet of landscaped or natural area shall be maintained along all street frontages.
 - Variance request: 8.3' feet along William Floyd Parkway
2. Town Code § 85-555(A): A variance to permit in the front yard along Moriches Middle Island Road and William Floyd Parkway.

B1. Zoning Board of Appeals:

1. Minimum Lot Area: A variance from Town Code § 85-466(A)(3) to permit a lot area of 59,951 square feet where a minimum lot area of 65,000 square feet is required.
2. Rear Yard Setback: A variance from Town Code § 85-466(D)(1) to permit a 25 foot rear yard setback where a 40 foot setback is required.
3. Sign Variance: A variance from Town Code § 57A-6.E to permit the proposed wall sign on the north façade of the convenience store to be 52.5 square feet where a maximum size of 40 square feet is permitted.
4. Sign Variance: A variance to permit the proposed monument sign to have a facial area of 32 square feet per face where Town Code § 57A-6(C) permits a ground sign to have a maximum facial area of 24 square feet per face.

**TOWN OF BROOKHAVEN
TRANSACTIONAL DISCLOSURE FORM
(Conflict of Interest Form)**

A Transactional Disclosure form is required when someone submits certain applications to Brookhaven Town. The purpose of the disclosure is to alert the Town if a party of influence has an interest in this application or if someone within the Town who will participate in the decision has an interest.

***Note:** It is required that a copy of this form be sent to the Director of the Board of Ethics.

Name 7-Eleven, Inc. Address 3200 Hackberry Road

City Irving State TX Zip 75063

Telephone (516) 222-0222 Email kbrown@brownaltman.com Fax (516) 222-0322

This form is for:

- ☐ An individual ☐ A partnership
☒ A corporation/LLC ☐ An association

Nature of Application:

- | | |
|---|--|
| <input type="checkbox"/> Property Assessment Grievance for non-residential parcel | <input type="checkbox"/> Variance |
| <input type="checkbox"/> Amendment | <input checked="" type="checkbox"/> Change of Zone |
| <input type="checkbox"/> Approval of Plat / Site Plan Approval | <input type="checkbox"/> Exemption from Plat or Official Map |
| <input type="checkbox"/> License or Permit affecting real property | <input type="checkbox"/> Bidding on contract(s) |

Affected parcel (address) 1481 William Floyd Parkway, Shirley

SCTM Nos. 0200-642-3-41.3, 41.4 & 41.5

Does any officer or employee of the Town of Brookhaven, member of an executive committee of a political party, or his/her spouse, brother, sister, parent, child, grandchild or spouse of any of them, have an interest in this application by virtue of being the actual applicant, being the owner of the actual property or having an interest in the corporation, partnership or association making such application? Yes _____ No X

If Yes, complete the appropriate section below.

If No, sign and date at end of form.

Please complete the following relevant section below:

For individual:

Interested Party:

Name _____ Address _____

City _____ State _____ Zip _____

Effective 5/15/2018

For corporation:

Interested Party:

Name _____ Address _____

City _____ State _____ Zip _____

Title _____ Department _____

Relationship to Public Officer/Employee and Title, if other than Self: _____

Yes ___ No ___ Is the owner of greater than five percent (5%) of the corporate stock of the application when the applicant is a corporation whose stock is publicly traded.

Yes ___ No ___ The actual applicant,

Yes ___ No ___ An Officer, Director, Partner, or Employee of the applicant, or

Yes ___ No ___ Legally or beneficially owns or controls any stock of a non-publicly traded corporate applicant or is a member of a partnership or association of the applicant.

For partnership or association:

Interested Party:

Name _____ Address _____

City _____ State _____ Zip _____

Title _____ Department _____

Relationship to Public Officer/Employee and Title, if other than Self: _____

Yes ___ No ___ Does the owner hold greater than five percent (5%) interest of publicly traded shares?

Yes ___ No ___ The actual applicant,

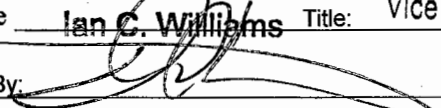
Yes ___ No ___ An Officer, Director, Partner, or Employee of the applicant, or

Yes ___ No ___ Legally or beneficially owns or controls any stock of a non-publicly traded corporate applicant or is a member of a partnership or association of the applicant.

ALL APPLICANTS PLEASE FILL OUT BELOW:

7-ELEVEN, INC.

Print Name Ian C. Williams Title: Vice President Date September 13, 2021

Signature By: 

Effective 05/15/2018

A Transactional Disclosure form is required when someone submits certain applications to Brookhaven Town. The purpose of the disclosure is to alert the Town if a party of influence has an interest in this application or if someone within the Town who will participate in the decision has an interest.

Name Franklin Johnson, Inc. Address 3605 Merrick Rd.

City Seaford State NY Zip 11783

Telephone (516) 826-3700 Email rcozza@cornelldesigncorp.com Fax (516) 222-0322

☐ An individual ☐ A partnership
☒ A corporation/LLC ☐ An association

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SEP 23 2000

☐ Property Assessment Grievance for non-residential parcel
☐ Amendment
☐ Approval of Plat / Site Plan Approval
☐ License or Permit affecting real property

☐ Variance
☒ Change of Zone
☐ Exemption from Plat or Official Map
☐ Bidding on contract(s)

Town of Brookhaven
Town Clerk's Office

Affected parcel (address) 1481 William Floyd Parkway, Shirley
SCTM Nos. 0200-642-3-41.3, 41.4 & 41.5

Does any officer or employee of the Town of Brookhaven, member of an executive committee of a political party, or his/her spouse, brother, sister, parent, child, grandchild or spouse of any of them, have an interest in this application by virtue of being the actual applicant, being the owner of the actual property or having an interest in the corporation, partnership or association making such application? Yes No X

If Yes, complete the appropriate section below.

If No, sign and date at end of form.

For individual:

Interested Party:

[illegible]

City _____ State _____ Zip _____

Effective 5/15/2018

For corporation:

Interested Party:

Name _____ Address _____
City _____ State _____ Zip _____
Title _____ Department _____

Relationship to Public Officer/Employee and Title, if other than Self: _____

Yes ___ No ___ Is the owner of greater than five percent (5%) of the corporate stock of the application when the applicant is a corporation whose stock is publicly traded.

Yes ___ No ___ The actual applicant,

Yes ___ No ___ An Officer, Director, Partner, or Employee of the applicant, or

Yes ___ No ___ Legally or beneficially owns or controls any stock of a non-publicly traded corporate applicant or is a member of a partnership or association of the applicant.

For partnership or association:

Interested Party:

Name _____ Address _____
City _____ State _____ Zip _____
Title _____ Department _____

Relationship to Public Officer/Employee and Title, if other than Self: _____

Yes ___ No ___ Does the owner hold greater than five percent (5%) interest of publicly traded shares?

Yes ___ No ___ The actual applicant,

Yes ___ No ___ An Officer, Director, Partner, or Employee of the applicant, or

Yes ___ No ___ Legally or beneficially owns or controls any stock of a non-publicly traded corporate applicant or is a member of a partnership or association of the applicant.

ALL APPLICANTS PLEASE FILL OUT BELOW:
FRANKLIN JOHNSON, INC.

Print Name Ralph Cozza Title: Vice President Date 9-2-2021

Signature By: Ralph Cozza Vice President

Effective 05/15/2018

**Full Environmental Assessment Form
Part 1 - Project and Setting**

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OCT 12 2022

Instructions for Completing Part 1

Town of Brookhaven
Town Clerk's Office

Part 1 is to be completed by the applicant or project sponsor. Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification.

Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information; indicate whether missing information does not exist, or is not reasonably available to the sponsor; and, when possible, generally describe work or studies which would be necessary to update or fully develop that information.

Applicants/sponsors must complete all items in Sections A & B. In Sections C, D & E, most items contain an initial question that must be answered either "Yes" or "No". If the answer to the initial question is "Yes", complete the sub-questions that follow. If the answer to the initial question is "No", proceed to the next question. Section F allows the project sponsor to identify and attach any additional information. Section G requires the name and signature of the applicant or project sponsor to verify that the information contained in Part 1 is accurate and complete.

A. Project and Applicant/Sponsor Information.

Name of Action or Project: 7-Eleven Shirley		
Project Location (describe, and attach a general location map): 1481 William Floyd Parkway, Shirley, New York		
Brief Description of Proposed Action (include purpose or need): The project proposes a new 2,995 SF 7-Eleven Convenience Store and 16 fueling stations. The site proposes 3 access drives, 32 parking spaces, a double trash enclosure and two underground fuel storage tanks totaling 40,000 gallons of storage. Other site improvements include landscaping, lighting, pedestrian walkways, stormwater improvements and other general site improvements. The existing building will be removed in accordance with all applicable regulations as part of the proposed development.		
Name of Applicant/Sponsor: 7-Eleven, Inc. (c/o Brown & Altman, LLP)		Telephone: (516) 222-0222
		E-Mail: kbrown@brownaltman.com
Address: 538 Broadhollow Road, Suite 301 W		
City/PO: Melville	State: New York	Zip Code: 11747
Project Contact (if not same as sponsor; give name and title/role): Stonefield Engineering & Design, LLC (c/ Zachary Chaplin)		Telephone: 718-606-8305
		E-Mail: zchaplin@stonefieldeng.com
Address: 584 Broadway, Suite 310		
City/PO: New York	State: New York	Zip Code: 10012
Property Owner (if not same as sponsor): Franklin Johnson, Inc.		Telephone:
		E-Mail:
Address: 3605 Merrick Road		
City/PO: Seaford	State: New York	Zip Code: 11783

B. Government Approvals

B. Government Approvals, Funding, or Sponsorship. ("Funding" includes grants, loans, tax relief, and any other forms of financial assistance.)

Government Entity	If Yes: Identify Agency and Approval(s) Required	Application Date (Actual or projected)
a. City Counsel, Town Board, <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No or Village Board of Trustees	Town Board - Change of Zone / Special Permit	TBD
b. City, Town or Village <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Planning Board or Commission	Planning Board - Site Plan Approval	TBD
c. City, Town or <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Village Zoning Board of Appeals	Zoning Board of Appeals - Variance Approval	TBD
d. Other local agencies <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
e. County agencies <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	Suffolk County DPW, Planning Commission, SCDHS, SCWA, Fire Marshal	TBD
f. Regional agencies <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
g. State agencies <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
h. Federal agencies <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
i. Coastal Resources.		
i. Is the project site within a Coastal Area, or the waterfront area of a Designated Inland Waterway?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
ii. Is the project site located in a community with an approved Local Waterfront Revitalization Program?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
iii. Is the project site within a Coastal Erosion Hazard Area?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No

C. Planning and Zoning

C.1. Planning and zoning actions.

Will administrative or legislative adoption, or amendment of a plan, local law, ordinance, rule or regulation be the only approval(s) which must be granted to enable the proposed action to proceed? ☐ Yes ☒ No

- If Yes, complete sections C, F and G.
- If No, proceed to question C.2 and complete all remaining sections and questions in Part 1

C.2. Adopted land use plans.

a. Do any municipally- adopted (city, town, village or county) comprehensive land use plan(s) include the site where the proposed action would be located? ☐ Yes ☒ No

If Yes, does the comprehensive plan include specific recommendations for the site where the proposed action would be located? ☐ Yes ☐ No

b. Is the site of the proposed action within any local or regional special planning district (for example: Greenway; Brownfield Opportunity Area (BOA); designated State or Federal heritage area; watershed management plan; or other?) ☐ Yes ☒ No

If Yes, identify the plan(s):

c. Is the proposed action located wholly or partially within an area listed in an adopted municipal open space plan, or an adopted municipal farmland protection plan? ☐ Yes ☒ No

If Yes, identify the plan(s):

C.3. Zoning	
a. Is the site of the proposed action located in a municipality with an adopted zoning law or ordinance. If Yes, what is the zoning classification(s) including any applicable overlay district?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
J-Business 2 District (J-2)	
b. Is the use permitted or allowed by a special or conditional use permit?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
c. Is a zoning change requested as part of the proposed action?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
If Yes, i. What is the proposed new zoning for the site? J-Business 5 District (J5) High Intensity Business	
C.4. Existing community services.	
a. In what school district is the project site located? Longwood Central School District	
b. What police or other public protection forces serve the project site? Suffolk County Police (Sector 706)	
c. Which fire protection and emergency medical services serve the project site? Ridge Fire	
d. What parks serve the project site? Southaven County Park	

D. Project Details

D.1. Proposed and Potential Development	
a. What is the general nature of the proposed action (e.g., residential, industrial, commercial, recreational; if mixed, include all components)? Commercial	
b. a. Total acreage of the site of the proposed action?	1.38 acres
b. Total acreage to be physically disturbed?	1.38 acres
c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor?	1.38 acres
c. Is the proposed action an expansion of an existing project or use?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
i. If Yes, what is the approximate percentage of the proposed expansion and identify the units (e.g., acres, miles, housing units, square feet)? % _____ Units: _____	
d. Is the proposed action a subdivision, or does it include a subdivision?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes, i. Purpose or type of subdivision? (e.g., residential, industrial, commercial; if mixed, specify types)	
ii. Is a cluster/conservation layout proposed? <input type="checkbox"/> Yes <input type="checkbox"/> No	
iii. Number of lots proposed? _____	
iv. Minimum and maximum proposed lot sizes? Minimum _____ Maximum _____	
e. Will the proposed action be constructed in multiple phases?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
i. If No, anticipated period of construction: _____ 4 months	
ii. If Yes:	
• Total number of phases anticipated _____	
• Anticipated commencement date of phase 1 (including demolition) _____ month _____ year	
• Anticipated completion date of final phase _____ month _____ year	
• Generally describe connections or relationships among phases, including any contingencies where progress of one phase may determine timing or duration of future phases: _____	

f. Does the project include new residential uses? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No				
If Yes, show numbers of units proposed.				
	<u>One Family</u>	<u>Two Family</u>	<u>Three Family</u>	<u>Multiple Family (four or more)</u>
Initial Phase	_____	_____	_____	_____
At completion	_____	_____	_____	_____
of all phases	_____	_____	_____	_____

g. Does the proposed action include new non-residential construction (including expansions)? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
If Yes,	
i. Total number of structures <u>1</u>	
ii. Dimensions (in feet) of largest proposed structure: <u><35'</u> height; <u>63.8'</u> width; and <u>90.2'</u> length	
iii. Approximate extent of building space to be heated or cooled: <u>2995</u> square feet	

h. Does the proposed action include construction or other activities that will result in the impoundment of any liquids, such as creation of a water supply, reservoir, pond, lake, waste lagoon or other storage? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
If Yes,	
i. Purpose of the impoundment: _____	
ii. If a water impoundment, the principal source of the water: <input type="checkbox"/> Ground water <input type="checkbox"/> Surface water streams <input type="checkbox"/> Other specify: _____	
iii. If other than water, identify the type of impounded/contained liquids and their source. _____	
iv. Approximate size of the proposed impoundment. Volume: _____ million gallons; surface area: _____ acres	
v. Dimensions of the proposed dam or impounding structure: _____ height; _____ length	
vi. Construction method/materials for the proposed dam or impounding structure (e.g., earth fill, rock, wood, concrete): _____	

D.2. Project Operations

a. Does the proposed action include any excavation, mining, or dredging, during construction, operations, or both? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
(Not including general site preparation, grading or installation of utilities or foundations where all excavated materials will remain onsite)	
If Yes:	
i. What is the purpose of the excavation or dredging? _____	
ii. How much material (including rock, earth, sediments, etc.) is proposed to be removed from the site?	
<ul style="list-style-type: none"> • Volume (specify tons or cubic yards): _____ • Over what duration of time? _____ 	
iii. Describe nature and characteristics of materials to be excavated or dredged, and plans to use, manage or dispose of them. _____	
iv. Will there be onsite dewatering or processing of excavated materials? <input type="checkbox"/> Yes <input type="checkbox"/> No	
If yes, describe. _____	
v. What is the total area to be dredged or excavated? _____ acres	
vi. What is the maximum area to be worked at any one time? _____ acres	
vii. What would be the maximum depth of excavation or dredging? _____ feet	
viii. Will the excavation require blasting? <input type="checkbox"/> Yes <input type="checkbox"/> No	
ix. Summarize site reclamation goals and plan: _____	

b. Would the proposed action cause or result in alteration of, increase or decrease in size of, or encroachment into any existing wetland, waterbody, shoreline, beach or adjacent area? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
If Yes:	
i. Identify the wetland or waterbody which would be affected (by name, water index number, wetland map number or geographic description): _____	

ii. Describe how the proposed action would affect that waterbody or wetland, e.g. excavation, fill, placement of structures, or alteration of channels, banks and shorelines. Indicate extent of activities, alterations and additions in square feet or acres:

iii. Will the proposed action cause or result in disturbance to bottom sediments? ☐ Yes ☐ No
If Yes, describe: _____

iv. Will the proposed action cause or result in the destruction or removal of aquatic vegetation? ☐ Yes ☐ No
If Yes:

- acres of aquatic vegetation proposed to be removed: _____
- expected acreage of aquatic vegetation remaining after project completion: _____
- purpose of proposed removal (e.g. beach clearing, invasive species control, boat access): _____
- proposed method of plant removal: _____
- if chemical/herbicide treatment will be used, specify product(s): _____

v. Describe any proposed reclamation/mitigation following disturbance: _____

c. Will the proposed action use, or create a new demand for water? ☒ Yes ☐ No
If Yes:

i. Total anticipated water usage/demand per day: _____ 400 gallons/day

ii. Will the proposed action obtain water from an existing public water supply? ☒ Yes ☐ No
If Yes:

- Name of district or service area: Suffolk County Water Authority
- Does the existing public water supply have capacity to serve the proposal? ☒ Yes ☐ No
- Is the project site in the existing district? ☒ Yes ☐ No
- Is expansion of the district needed? ☐ Yes ☒ No
- Do existing lines serve the project site? ☒ Yes ☐ No

iii. Will line extension within an existing district be necessary to supply the project? ☐ Yes ☒ No
If Yes:

- Describe extensions or capacity expansions proposed to serve this project: _____
- Source(s) of supply for the district: _____

iv. Is a new water supply district or service area proposed to be formed to serve the project site? ☐ Yes ☒ No
If Yes:

- Applicant/sponsor for new district: _____
- Date application submitted or anticipated: _____
- Proposed source(s) of supply for new district: _____

v. If a public water supply will not be used, describe plans to provide water supply for the project: _____

vi. If water supply will be from wells (public or private), what is the maximum pumping capacity: _____ gallons/minute.

d. Will the proposed action generate liquid wastes? ☒ Yes ☐ No
If Yes:

i. Total anticipated liquid waste generation per day: _____ 150 gallons/day

ii. Nature of liquid wastes to be generated (e.g., sanitary wastewater, industrial; if combination, describe all components and approximate volumes or proportions of each): _____

Sanitary Wastewater

iii. Will the proposed action use any existing public wastewater treatment facilities? ☐ Yes ☒ No
If Yes:

- Name of wastewater treatment plant to be used: _____
- Name of district: _____
- Does the existing wastewater treatment plant have capacity to serve the project? ☐ Yes ☐ No
- Is the project site in the existing district? ☐ Yes ☐ No
- Is expansion of the district needed? ☐ Yes ☐ No

<ul style="list-style-type: none"> • Do existing sewer lines serve the project site? • Will a line extension within an existing district be necessary to serve the project? <p>If Yes:</p> <ul style="list-style-type: none"> • Describe extensions or capacity expansions proposed to serve this project: _____ 	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Yes <input type="checkbox"/> No
iv. Will a new wastewater (sewage) treatment district be formed to serve the project site?	
If Yes: <ul style="list-style-type: none"> • Applicant/sponsor for new district: _____ • Date application submitted or anticipated: _____ • What is the receiving water for the wastewater discharge? _____ 	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
v. If public facilities will not be used, describe plans to provide wastewater treatment for the project, including specifying proposed receiving water (name and classification if surface discharge or describe subsurface disposal plans):	
The sanitary waste will be treated via on-site septic system in accordance with the Suffolk County requirements and storm water runoff will be maintained on-site in accordance with local and county requirements.	
vi. Describe any plans or designs to capture, recycle or reuse liquid waste: _____	
Not applicable.	
e. Will the proposed action disturb more than one acre and create stormwater runoff, either from new point sources (i.e. ditches, pipes, swales, curbs, gutters or other concentrated flows of stormwater) or non-point source (i.e. sheet flow) during construction or post construction?	
<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
If Yes:	
i. How much impervious surface will the project create in relation to total size of project parcel?	
_____ Square feet or <u>0.87</u> acres (impervious surface)	
_____ Square feet or <u>1.38</u> acres (parcel size)	
ii. Describe types of new point sources. The site grading will direct all on-site stormwater to a series of catch basins and yard inlets.	
iii. Where will the stormwater runoff be directed (i.e. on-site stormwater management facility/structures, adjacent properties, groundwater, on-site surface water or off-site surface waters)?	
The on-site stormwater will be collected and piped to a series of stormwater management structures within the property. The system will be sized in accordance with all applicable standards.	
• If to surface waters, identify receiving water bodies or wetlands: _____	
• Will stormwater runoff flow to adjacent properties?	
<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
iv. Does the proposed plan minimize impervious surfaces, use pervious materials or collect and re-use stormwater?	
<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
f. Does the proposed action include, or will it use on-site, one or more sources of air emissions, including fuel combustion, waste incineration, or other processes or operations?	
<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
If Yes, identify:	
i. Mobile sources during project operations (e.g., heavy equipment, fleet or delivery vehicles)	
ii. Stationary sources during construction (e.g., power generation, structural heating, batch plant, crushers)	
iii. Stationary sources during operations (e.g., process emissions, large boilers, electric generation)	
g. Will any air emission sources named in D.2.f (above), require a NY State Air Registration, Air Facility Permit, or Federal Clean Air Act Title IV or Title V Permit?	
<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
If Yes:	
i. Is the project site located in an Air quality non-attainment area? (Area routinely or periodically fails to meet ambient air quality standards for all or some parts of the year)	
<input type="checkbox"/> Yes <input type="checkbox"/> No	
ii. In addition to emissions as calculated in the application, the project will generate:	
• _____ Tons/year (short tons) of Carbon Dioxide (CO ₂)	
• _____ Tons/year (short tons) of Nitrous Oxide (N ₂ O)	
• _____ Tons/year (short tons) of Perfluorocarbons (PFCs)	
• _____ Tons/year (short tons) of Sulfur Hexafluoride (SF ₆)	
• _____ Tons/year (short tons) of Carbon Dioxide equivalent of Hydrofluorocarbons (HFCs)	
• _____ Tons/year (short tons) of Hazardous Air Pollutants (HAPs)	

<p>h. Will the proposed action generate or emit methane (including, but not limited to, sewage treatment plants, landfills, composting facilities)? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Estimate methane generation in tons/year (metric): _____</p> <p>ii. Describe any methane capture, control or elimination measures included in project design (e.g., combustion to generate heat or electricity, flaring): _____</p>			
<p>i. Will the proposed action result in the release of air pollutants from open-air operations or processes, such as quarry or landfill operations? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes: Describe operations and nature of emissions (e.g., diesel exhaust, rock particulates/dust): _____</p>			
<p>j. Will the proposed action result in a substantial increase in traffic above present levels or generate substantial new demand for transportation facilities or services? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes:</p> <p>i. When is the peak traffic expected (Check all that apply): <input type="checkbox"/> Morning <input type="checkbox"/> Evening <input type="checkbox"/> Weekend <input type="checkbox"/> Randomly between hours of _____ to _____</p> <p>ii. For commercial activities only, projected number of truck trips/day and type (e.g., semi trailers and dump trucks): _____</p> <p>iii. Parking spaces: Existing _____ Proposed _____ Net increase/decrease _____</p> <p>iv. Does the proposed action include any shared use parking? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>v. If the proposed action includes any modification of existing roads, creation of new roads or change in existing access, describe: _____</p> <p>vi. Are public/private transportation service(s) or facilities available within ½ mile of the proposed site? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>vii. Will the proposed action include access to public transportation or accommodations for use of hybrid, electric or other alternative fueled vehicles? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>viii. Will the proposed action include plans for pedestrian or bicycle accommodations for connections to existing pedestrian or bicycle routes? <input type="checkbox"/> Yes <input type="checkbox"/> No</p>			
<p>k. Will the proposed action (for commercial or industrial projects only) generate new or additional demand for energy? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Estimate annual electricity demand during operation of the proposed action: _____</p> <p>TBD _____</p> <p>ii. Anticipated sources/suppliers of electricity for the project (e.g., on-site combustion, on-site renewable, via grid/local utility, or other): _____</p> <p>NATIONAL GRID / PSEG LONG ISLAND</p> <p>iii. Will the proposed action require a new, or an upgrade, to an existing substation? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p>			
<p>l. Hours of operation. Answer all items which apply.</p> <table style="width: 100%; border: none;"> <tr> <td style="width: 50%; vertical-align: top;"> <p>i. During Construction:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ 7am - 5pm • Saturday: _____ 8am - 4pm • Sunday: _____ 8am - 4pm • Holidays: _____ N/A </td> <td style="width: 50%; vertical-align: top;"> <p>ii. During Operations:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ 24 HOUR • Saturday: _____ 24 HOUR • Sunday: _____ 24 HOUR • Holidays: _____ 24 HOUR </td> </tr> </table>		<p>i. During Construction:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ 7am - 5pm • Saturday: _____ 8am - 4pm • Sunday: _____ 8am - 4pm • Holidays: _____ N/A 	<p>ii. During Operations:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ 24 HOUR • Saturday: _____ 24 HOUR • Sunday: _____ 24 HOUR • Holidays: _____ 24 HOUR
<p>i. During Construction:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ 7am - 5pm • Saturday: _____ 8am - 4pm • Sunday: _____ 8am - 4pm • Holidays: _____ N/A 	<p>ii. During Operations:</p> <ul style="list-style-type: none"> • Monday - Friday: _____ 24 HOUR • Saturday: _____ 24 HOUR • Sunday: _____ 24 HOUR • Holidays: _____ 24 HOUR 		

<p>m. Will the proposed action produce noise that will exceed existing ambient noise levels during construction, operation, or both? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If yes:</p> <p>i. Provide details including sources, time of day and duration:</p> <p>_____</p>	
<p>ii. Will the proposed action remove existing natural barriers that could act as a noise barrier or screen? <input type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>Describe: _____</p>	
<p>n. Will the proposed action have outdoor lighting? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If yes:</p> <p>i. Describe source(s), location(s), height of fixture(s), direction/aim, and proximity to nearest occupied structures:</p> <p>Area lighting, building lighting and canopy lighting will be provided in accordance with local requirements.</p>	
<p>ii. Will proposed action remove existing natural barriers that could act as a light barrier or screen? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>Describe: _____</p>	
<p>o. Does the proposed action have the potential to produce odors for more than one hour per day? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes, describe possible sources, potential frequency and duration of odor emissions, and proximity to nearest occupied structures:</p> <p>_____</p>	
<p>p. Will the proposed action include any bulk storage of petroleum (combined capacity of over 1,100 gallons) or chemical products 185 gallons in above ground storage or any amount in underground storage? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Product(s) to be stored in underground fuel tanks _____</p> <p>ii. Volume(s) <u>80,000</u> per unit time <u>month</u> (e.g., month, year)</p> <p>iii. Generally, describe the proposed storage facilities: _____</p> <p>Underground Fuel tanks with a 8" reinforced concrete mat on top of it, directly adjacent to the fuel pumps.</p>	
<p>q. Will the proposed action (commercial, industrial and recreational projects only) use pesticides (i.e., herbicides, insecticides) during construction or operation? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Describe proposed treatment(s):</p> <p>_____</p>	
<p>ii. Will the proposed action use Integrated Pest Management Practices? <input type="checkbox"/> Yes <input type="checkbox"/> No</p>	
<p>r. Will the proposed action (commercial or industrial projects only) involve or require the management or disposal of solid waste (excluding hazardous materials)? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Describe any solid waste(s) to be generated during construction or operation of the facility:</p> <ul style="list-style-type: none"> • Construction: <u>TBD</u> tons per _____ (unit of time) • Operation : <u>10</u> tons per _____ year (unit of time) <p>ii. Describe any proposals for on-site minimization, recycling or reuse of materials to avoid disposal as solid waste:</p> <ul style="list-style-type: none"> • Construction: <u>Construction office recycling and trash containers. Pre-fabricated building materials to minimize construction waste.</u> • Operation: <u>On-site storage provided (trash & recycling enclosure)</u> <p>iii. Proposed disposal methods/facilities for solid waste generated on-site:</p> <ul style="list-style-type: none"> • Construction: <u>On-site storage and standard trash hauling</u> • Operation: <u>On-site storage provided (trash & recycling enclosure)</u> 	

s. Does the proposed action include construction or modification of a solid waste management facility? ☐ Yes ☒ No

If Yes:

i. Type of management or handling of waste proposed for the site (e.g., recycling or transfer station, composting, landfill, or other disposal activities): _____

ii. Anticipated rate of disposal/processing:

- _____ Tons/month, if transfer or other non-combustion/thermal treatment, or
- _____ Tons/hour, if combustion or thermal treatment

iii. If landfill, anticipated site life: _____ years

t. Will the proposed action at the site involve the commercial generation, treatment, storage, or disposal of hazardous waste? ☐ Yes ☒ No

If Yes:

i. Name(s) of all hazardous wastes or constituents to be generated, handled or managed at facility: _____

ii. Generally describe processes or activities involving hazardous wastes or constituents: _____

iii. Specify amount to be handled or generated _____ tons/month

iv. Describe any proposals for on-site minimization, recycling or reuse of hazardous constituents: _____

v. Will any hazardous wastes be disposed at an existing offsite hazardous waste facility? ☐ Yes ☐ No

If Yes: provide name and location of facility: _____

If No: describe proposed management of any hazardous wastes which will not be sent to a hazardous waste facility: _____

E. Site and Setting of Proposed Action

E.1. Land uses on and surrounding the project site

a. Existing land uses.

i. Check all uses that occur on, adjoining and near the project site.

☐ Urban ☐ Industrial ☒ Commercial ☒ Residential (suburban) ☐ Rural (non-farm)

☒ Forest ☐ Agriculture ☐ Aquatic ☐ Other (specify): _____

ii. If mix of uses, generally describe:

Commercial to the east and west, residential to the north and northwest, undeveloped land with tree coverage (forest) to the south

b. Land uses and covertypes on the project site.

Land use or Covertypes	Current Acreage	Acreage After Project Completion	Change (Acres +/-)
• Roads, buildings, and other paved or impervious surfaces	0.79 Acres	0.87 Acres	+0.08 Acres
• Forested			
• Meadows, grasslands or brushlands (non-agricultural, including abandoned agricultural)			
• Agricultural (includes active orchards, field, greenhouse etc.)			
• Surface water features (lakes, ponds, streams, rivers, etc.)			
• Wetlands (freshwater or tidal)			
• Non-vegetated (bare rock, earth or fill)			
• Other Describe: <u>Vegetated (grass/landscape)</u>	0.59 Acres	0.51 Acres	- 0.08 Acres

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v. Is the project site subject to an institutional control limiting property uses? <input type="checkbox"/> Yes <input type="checkbox"/> No													
<ul style="list-style-type: none"> • If yes, DEC site ID number: _____ • Describe the type of institutional control (e.g., deed restriction or easement): _____ • Describe any use limitations: _____ • Describe any engineering controls: _____ • Will the project affect the institutional or engineering controls in place? <input type="checkbox"/> Yes <input type="checkbox"/> No • Explain: _____ 													
E.2. Natural Resources On or Near Project Site													
a. What is the average depth to bedrock on the project site? _____ >6.5 feet													
b. Are there bedrock outcroppings on the project site? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If Yes, what proportion of the site is comprised of bedrock outcroppings? _____ %													
c. Predominant soil type(s) present on project site: <table style="width: 100%; border: none;"> <tr> <td style="border-bottom: 1px solid black; width: 60%;">RdA - Riverhead Sandy Loam</td> <td style="width: 10%; text-align: right;">100 %</td> </tr> <tr> <td style="border-bottom: 1px solid black;"></td> <td style="text-align: right;">%</td> </tr> <tr> <td style="border-bottom: 1px solid black;"></td> <td style="text-align: right;">%</td> </tr> </table>		RdA - Riverhead Sandy Loam	100 %		%		%						
RdA - Riverhead Sandy Loam	100 %												
	%												
	%												
d. What is the average depth to the water table on the project site? Average: _____ >6.5 feet													
e. Drainage status of project site soils: <table style="width: 100%; border: none;"> <tr> <td style="width: 30%;"><input checked="" type="checkbox"/> Well Drained:</td> <td style="width: 30%; text-align: right;">100 % of site</td> </tr> <tr> <td><input type="checkbox"/> Moderately Well Drained:</td> <td style="text-align: right;">% of site</td> </tr> <tr> <td><input type="checkbox"/> Poorly Drained</td> <td style="text-align: right;">% of site</td> </tr> </table>		<input checked="" type="checkbox"/> Well Drained:	100 % of site	<input type="checkbox"/> Moderately Well Drained:	% of site	<input type="checkbox"/> Poorly Drained	% of site						
<input checked="" type="checkbox"/> Well Drained:	100 % of site												
<input type="checkbox"/> Moderately Well Drained:	% of site												
<input type="checkbox"/> Poorly Drained	% of site												
f. Approximate proportion of proposed action site with slopes: <table style="width: 100%; border: none;"> <tr> <td style="width: 30%;"><input checked="" type="checkbox"/> 0-10%:</td> <td style="width: 30%; text-align: right;">100 % of site</td> </tr> <tr> <td><input type="checkbox"/> 10-15%:</td> <td style="text-align: right;">% of site</td> </tr> <tr> <td><input type="checkbox"/> 15% or greater:</td> <td style="text-align: right;">% of site</td> </tr> </table>		<input checked="" type="checkbox"/> 0-10%:	100 % of site	<input type="checkbox"/> 10-15%:	% of site	<input type="checkbox"/> 15% or greater:	% of site						
<input checked="" type="checkbox"/> 0-10%:	100 % of site												
<input type="checkbox"/> 10-15%:	% of site												
<input type="checkbox"/> 15% or greater:	% of site												
g. Are there any unique geologic features on the project site? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If Yes, describe: _____													
h. Surface water features.													
i. Does any portion of the project site contain wetlands or other waterbodies (including streams, rivers, ponds or lakes)? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No													
ii. Do any wetlands or other waterbodies adjoin the project site? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No													
If Yes to either <i>i</i> or <i>ii</i> , continue. If No, skip to E.2.i.													
iii. Are any of the wetlands or waterbodies within or adjoining the project site regulated by any federal, state or local agency? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No													
iv. For each identified regulated wetland and waterbody on the project site, provide the following information: <table style="width: 100%; border: none;"> <tr> <td style="width: 10%;">• Streams:</td> <td style="width: 40%;">Name _____</td> <td style="width: 10%;">Classification _____</td> </tr> <tr> <td>• Lakes or Ponds:</td> <td>Name _____</td> <td>Classification _____</td> </tr> <tr> <td>• Wetlands:</td> <td>Name _____</td> <td>Approximate Size _____</td> </tr> <tr> <td>• Wetland No. (if regulated by DEC)</td> <td colspan="2">_____</td> </tr> </table>		• Streams:	Name _____	Classification _____	• Lakes or Ponds:	Name _____	Classification _____	• Wetlands:	Name _____	Approximate Size _____	• Wetland No. (if regulated by DEC)	_____	
• Streams:	Name _____	Classification _____											
• Lakes or Ponds:	Name _____	Classification _____											
• Wetlands:	Name _____	Approximate Size _____											
• Wetland No. (if regulated by DEC)	_____												
v. Are any of the above water bodies listed in the most recent compilation of NYS water quality-impaired waterbodies? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No If yes, name of impaired water body/bodies and basis for listing as impaired: _____													
i. Is the project site in a designated Floodway? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No													
j. Is the project site in the 100-year Floodplain? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No													
k. Is the project site in the 500-year Floodplain? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No													
l. Is the project site located over, or immediately adjoining, a primary, principal or sole source aquifer? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No If Yes:													
i. Name of aquifer: <u>Nassau-Suffolk SSA</u>													

<p>m. Identify the predominant wildlife species that occupy or use the project site: _____</p> <p>N/A Developed Property _____</p> <p>_____</p>	
<p>n. Does the project site contain a designated significant natural community? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Describe the habitat/community (composition, function, and basis for designation): _____</p> <p>_____</p> <p>ii. Source(s) of description or evaluation: _____</p> <p>iii. Extent of community/habitat:</p> <ul style="list-style-type: none"> • Currently: _____ acres • Following completion of project as proposed: _____ acres • Gain or loss (indicate + or -): _____ acres 	
<p>o. Does project site contain any species of plant or animal that is listed by the federal government or NYS as endangered or threatened, or does it contain any areas identified as habitat for an endangered or threatened species? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Species and listing (endangered or threatened): _____</p> <p>_____</p> <p>_____</p>	
<p>p. Does the project site contain any species of plant or animal that is listed by NYS as rare, or as a species of special concern? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Species and listing: _____</p> <p>_____</p>	
<p>q. Is the project site or adjoining area currently used for hunting, trapping, fishing or shell fishing? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If yes, give a brief description of how the proposed action may affect that use: _____</p> <p>_____</p>	
<p>E.3. Designated Public Resources On or Near Project Site</p>	
<p>a. Is the project site, or any portion of it, located in a designated agricultural district certified pursuant to Agriculture and Markets Law, Article 25-AA, Section 303 and 304? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes, provide county plus district name/number: _____</p>	
<p>b. Are agricultural lands consisting of highly productive soils present? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>i. If Yes: acreage(s) on project site? _____</p> <p>ii. Source(s) of soil rating(s): _____</p>	
<p>c. Does the project site contain all or part of, or is it substantially contiguous to, a registered National Natural Landmark? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes:</p> <p>i. Nature of the natural landmark: <input type="checkbox"/> Biological Community <input type="checkbox"/> Geological Feature</p> <p>ii. Provide brief description of landmark, including values behind designation and approximate size/extent: _____</p> <p>_____</p> <p>_____</p>	
<p>d. Is the project site located in or does it adjoin a state listed Critical Environmental Area? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</p> <p>If Yes:</p> <p>i. CEA name: _____</p> <p>ii. Basis for designation: _____</p> <p>iii. Designating agency and date: _____</p>	

e. Does the project site contain, or is it substantially contiguous to, a building, archaeological site, or district which is listed on the National or State Register of Historic Places, or that has been determined by the Commissioner of the NYS Office of Parks, Recreation and Historic Preservation to be eligible for listing on the State Register of Historic Places? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
If Yes: <ul style="list-style-type: none"> i. Nature of historic/archaeological resource: <input type="checkbox"/> Archaeological Site <input type="checkbox"/> Historic Building or District ii. Name: _____ iii. Brief description of attributes on which listing is based: _____ 	
f. Is the project site, or any portion of it, located in or adjacent to an area designated as sensitive for archaeological sites on the NY State Historic Preservation Office (SHPO) archaeological site inventory? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
g. Have additional archaeological or historic site(s) or resources been identified on the project site? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
If Yes: <ul style="list-style-type: none"> i. Describe possible resource(s): _____ ii. Basis for identification: _____ 	
h. Is the project site within five miles of any officially designated and publicly accessible federal, state, or local scenic or aesthetic resource? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
If Yes: <ul style="list-style-type: none"> i. Identify resource: _____ ii. Nature of, or basis for, designation (e.g., established highway overlook, state or local park, state historic trail or scenic byway, etc.): _____ iii. Distance between project and resource: _____ miles. 	
i. Is the project site located within a designated river corridor under the Wild, Scenic and Recreational Rivers Program 6 NYCRR 666? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
If Yes: <ul style="list-style-type: none"> i. Identify the name of the river and its designation: _____ ii. Is the activity consistent with development restrictions contained in 6NYCRR Part 666? <input type="checkbox"/> Yes <input type="checkbox"/> No 	

F. Additional Information

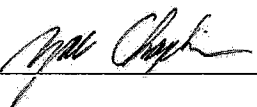
Attach any additional information which may be needed to clarify your project.

If you have identified any adverse impacts which could be associated with your proposal, please describe those impacts plus any measures which you propose to avoid or minimize them.

G. Verification

I certify that the information provided is true to the best of my knowledge.

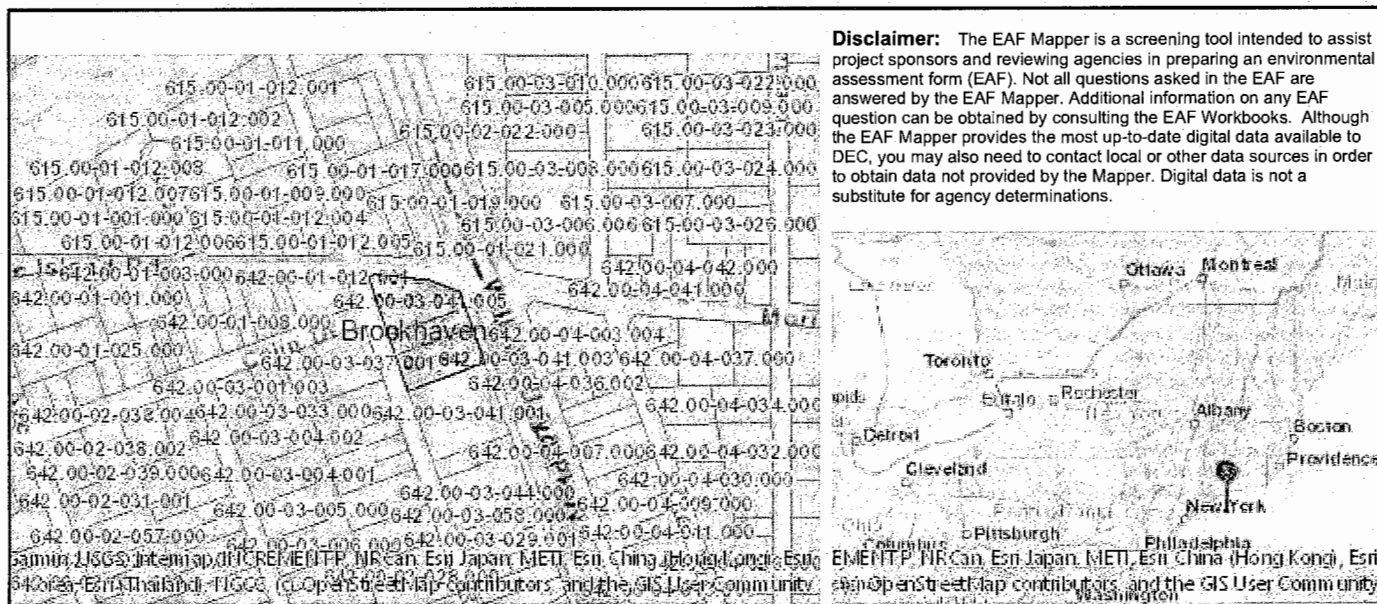
Applicant/Sponsor Name Zachary Chaplin Date 10/05/2022

Signature  Title Project Engineer

PRINT FORM

EAF Mapper Summary Report

Wednesday, August 18, 2021 12:20 PM



B.i.i [Coastal or Waterfront Area]	No
B.i.ii [Local Waterfront Revitalization Area]	No
C.2.b. [Special Planning District]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.1.h [DEC Spills or Remediation Site - Potential Contamination History]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.1.h.i [DEC Spills or Remediation Site - Listed]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.1.h.ii [DEC Spills or Remediation Site - Environmental Site Remediation Database]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.1.h.iii [Within 2,000' of DEC Remediation Site]	No
E.2.g [Unique Geologic Features]	No
E.2.h.i [Surface Water Features]	No
E.2.h.ii [Surface Water Features]	No
E.2.h.iii [Surface Water Features]	No
E.2.h.v [Impaired Water Bodies]	No
E.2.i. [Floodway]	No
E.2.j. [100 Year Floodplain]	No
E.2.k. [500 Year Floodplain]	No
E.2.l. [Aquifers]	Yes
E.2.l. [Aquifer Names]	Sole Source Aquifer Names: Nassau-Suffolk SSA
E.2.n. [Natural Communities]	No
E.2.o. [Endangered or Threatened Species]	No
E.2.p. [Rare Plants or Animals]	No

E.3.a. [Agricultural District]	No
E.3.c. [National Natural Landmark]	No
E.3.d [Critical Environmental Area]	No
E.3.e. [National or State Register of Historic Places or State Eligible Sites]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.3.f. [Archeological Sites]	No
E.3.i. [Designated River Corridor]	No

STONEFIELD

RECEIVED

OCT 12 2022

October 5, 2022

Amy Moody
Town of Brookhaven, Planning Division
One Independence Hill
Farmingville, New York 11738

Town of Brookhaven
Town Clerk's Office

**RE: Proposed Convenience Store with Fuel Sales
Block 3, Lots 41.3, 41.4 & 41.5
1481 William Floyd Parkway
Hamlet of Shirley, Town of Brookhaven, Suffolk County, New York**

Ms. Moody,

Our office is submitting documents on behalf of the Applicant to address the comments contained within the Town Planning Division's review letter, dated December 22, 2021. Please find the following items enclosed:

ITEM DESCRIPTION	DATED	COPIES	PREPARED BY
Site Plan	09-13-2022	9	Stonefield Engineering & Design
Survey	07-29-2022	9	Stonefield Engineering & Design
Metes & Bounds Description (Lot 41.1)	--	9	Stonefield Engineering & Design
Metes & Bounds Description (Lots 41.3 – 41.5)	--	9	Stonefield Engineering & Design
Truck Turning Exhibit	09-13-2022	9	Stonefield Engineering & Design
4"x4" Diagram	09-13-2022	5	Stonefield Engineering & Design
Full EAF	--	9	Stonefield Engineering & Design

The following is an itemized response to the comments contained within the Town of Brookhaven review letter dated December 22, 2021. For the sake of brevity, any comments that are statements of fact or have been previously addressed are not included in the response below:

INVOLVED AGENCIES & COMMUNITY:

1. Be advised that in accordance with Town policy, you are requested to reach out and meet with the local civic organizations in your proposed project area. It is recommended that you provide the local civic organizations with a copy of your site plan as well as typical application materials and supporting documentation before the meeting. For your convenience, both the Town Clerk and the local Town Council office maintain a current list of civic organizations and the appropriate contact person(s). Upon completion of this meeting please submit a letter, with a copy to the local civic organization, summarizing the conclusions of the meeting including the issues in which a consensus was reached as well as those issues that still remain. In addition, as a courtesy, please contact the local Town Council representative.

STONEFIELDENG.COM

584 BROADWAY, SUITE 310, NEW YORK, NY 10012 718.606.8305 T. 201.340.4472 F.

2. As a routine procedure, your application has been sent to the following agencies for their review and consideration and we are awaiting their response(s):
 - Division of Traffic Safety.
 - Division of Environmental Protection.
 - Suffolk County Department of Health Services.
 - Central Pine Barrens Joint Planning & Policy Commission.
3. As a routine procedure, your application has been sent to the following agencies and they have responded – their comments are attached hereto and should be addressed with any re- submission: Division of Fire Prevention; Department of Highway; Suffolk County Department of Public Works.
4. Once your application is determined to be complete and contains a “full statement of facts”, your application will be referred pursuant to New York State Town Law and the Suffolk County Charter to the Suffolk County Planning Commission for recommendation and comments.

All above comments are acknowledged and will be addressed as necessary by the Applicant.

TOWN CODE CHANGE OF ZONE AND SPECIAL PERMIT APPLICATION REQUIREMENTS:

1. The application materials provided fail to address the existing split J-2 & A-1 zoning. Revise all application documents including application form, concept plan, 4 x 4, notice, etc. accordingly.

All submission materials have been revised and updated to accurately reflect the current split zoning of the subject parcel between the J-2 and A-1 districts.

2. Provide a detailed analysis of compliance or lack thereof with all of the special permit criteria for each special permit use, summarized in the compliance table on the concept plan and detailed in narrative form. Clearly request waivers and submit fees for all areas of non- compliance.

A detailed analysis of the special permit criteria is to be provided under separate cover by the project attorney.

3. Town Code § 85-81.A.(3) requires a conceptual plan (your site plan) that indicates the proposal's conformance with Town Code development standards. It is therefore requested that a Town Code development standards compliance table be provided for the following sections of Town code:
 - § 85-466. Dimensional criteria for the J-5 Business District. The southerly property line is a REAR yard. Please correct.
 - § 85-467.B & G. Address all of the special permit criteria in the compliance table. Required buffers to residential across the street are not met, and there is additional non- compliance to the buffering provisions as the southerly lot line is a REAR yard.
 - § 85-843: Land development standards.

The provided tables on the submitted Site Plan have been expanded and updated to reflect the requirements of the above sections of the Town of Brookhaven Zoning Code.

4. Provide a survey of the subject & contiguous property such that the metes and bounds descriptions can be verified. It is noted that there are discrepancies between the metes and bounds on the concept plan and the descriptions provided; please verify the accuracy of the metes and bounds descriptions to the property survey before submitting. Please note that metes and bounds descriptions need to be provided for all areas to be re-zoned, and need to be able to be verified on surveys.

A boundary survey of the contiguous property, Lot 41.1, as well as the corresponding metes and bounds description of the same is included herein.

5. Address all Law Department comments regarding sufficiency of the application.

All Law Department comments have been addressed and previously resubmitted.

PLANNING DIVISION COMMENTS:

Significant Concept Plan Issues:

- A. The split-zoning of the project site has not been recognized as referenced above.

Noted. All submission materials have been updated to reflect the current zoning of the parcel.

- B. The project site does not comply with the lot area or rear yard requirements of the zoning district being sought, or with the special permit criteria of the special permit uses being sought.

Noted. Variances will be requested for areas of non-compliance.

- C. The project site falls within the Compatible Growth Area of the Central Pine Barrens and therefore is subject to the provisions and Development Standards of Town Code. Demonstrate compliance and provide detailed Pine Barrens Development Standards compliance table with clearing calculations, re-vegetation plan if necessary, and consult with the Central Pine Barrens Joint Planning & Policy Commission as to necessity for submission of a hardship application.

Noted, compliance with the Central Pine Barrens requirements will be demonstrated by the project's environmental consultant under separate submission.

- D. Based on non-compliance with the dimensional and special permit criteria of Town Code, and with Central Pine Barrens Compatible Growth Area Guidelines & Standards, the Planning Division recommends resolving these issues by adding commonly owned tax lot 41.1 to the subject application.

The Site Plan has been revised to further comply with the special permit and dimensional criteria of the Town Code. The Applicant does not wish to include the adjoining lot in the present development plan. Lot 41.1 is a fully wooded property providing additional buffering to the south of the subject parcel which is a benefit to the surrounding community.

- E. Reducing the scale of the proposal to provide closer compliance with Town Code requirements is recommended: Additional landscaping in favor of parking or asphalt facing the roadways; Elimination of the striped asphalt area leading to the dumpster enclosure in favor of landscaping along the road frontage (and as a buffer to the residence across the street); Recommend reduction of number of gas pumps/canopy size in favor of additional landscaping along the road frontages.

Modifications to the Site Plan, including those listed in the above comment, have been completed to further comply with the Town Code. The loading zone has been reduced in size, buffer fencing has been increased in length, and parking stalls near each street frontage have been indicated as 'landbanked' to increase the proposed landscape area.

- F. The Suffolk County Department of Public Works recommends that the access on Moriches Middle Island Road be eliminated. Recommend reaching out to the Suffolk County Department of Public Works, Town Highway and Traffic Safety Departments on this important matter of site access. Closure of this curb cut and provision of landscaping would be favorable due to residential zoning and uses across the street.

Per agreement with the Suffolk County DPW, the driveway connection to Moriches – Middle Island Road has been modified and will be signed to restrict left-turn ingress to the site. Modifications required the inclusion of a raised, curb island the channelizes the entrance portion of the curb cut.

- G. Provide a parking analysis, based on ITE parking generation data and actual parking demand at the subject store and similarly situated stores. Additional landscaping would be favored over an excess of parking, particularly facing the intersection of William Floyd Parkway and Moriches Middle Island Road. Consider landbanking unnecessary stalls.

Additional parking (3 stalls) has been converted to 'landbanked' to achieve additional landscaped area along the William Floyd and Moriches Middle Island frontages. The remaining 29 parking stalls are sufficient for the anticipated demand at this location.

Other Concept Plan Comments:

- H. Clearly depict, label, and quantify all existing and proposed zoning areas for each differently zoned area. This is important not only for analysis purposes, but also for purpose of intensification fee calculation.

The zoning boundary that divides the subject parcel has been shown on the plan and labelled accordingly. The area of each zoning designation with the property boundary is indicated on the Site Plan. Note, a change of zone for the rear +/- 50 ft of the property currently zone in the A-1 Residential district will be pursued.

- I. Compliance with the special permit criteria that requires compliance with the land development standard buffers (which applies to both special permits requested) is referenced – which is not the case. Not only is there non-compliance with required buffers but also with the 40' required rear yard. Please address/correct.

The southern property line has been reconsidered as a rear yard per the Town Code. A setback variance will be required.

- J. Demonstrate adequate path for fuel truck deliveries with truck turning template overlay.

A fuel delivery truck exhibit has been prepared and is included in this resubmission.

- K. Demonstrate compliance with the special permit criterion for fencing adjacent to residential.

The site plan provides the required fencing along the southern property line and extending to the north, along the western property line, a distance of 100 linear feet. A variance for the required fencing along the northern property line (Moriches-Middle Island Road frontage) and northwestern corner of the site (Revilo Avenue / Moriches Road intersection) will be requested.

- L. Clearly provide & label the dimensions relative to the curb cut criteria in Town Code §85- 467.G.(8) on the concept plan.

All curb cuts have been labeled and dimensioned to indicate compliance with the Town Code requirements. Specifically, the curb cut along Moriches-Middle Island Road measures 27ft from the nearest intersection. All other curb cuts exceed this measurement.

- M. Provide a legend for all symbols used on the concept plan. What is the curved striped planted area between the building and William Floyd parkway next to the sidewalk? What is the striped area surrounded by plants north of the building in a landscape island?

The areas in question are landscape features, depicted using symbols that represent the various proposed plant species and types of vegetation. A full Landscape Plan will be submitted later along with the fully engineered site plans. These symbols have been included in the legend.

Architectural Plan Comments:

- N. Provide a floor plan to verify the retail and display area, showing and labeling retail and display area vs. building area, and to verify special permit criterion requiring a public restroom.

A detailed floor plan indicating the exact square footage of retail area versus total building area will be provided under separate cover.

- O. Please advise if you are planning a basement for the proposed structure, as basements and storage areas are included in the computation of Commercial Gross Floor Area (GFA) (§85-I, Definitions).

There is no basement planned for the proposed structure.

- P. Architectural elevations of the proposed structures may facilitate consideration of the application.

Noted. Elevations will be prepared for this application.

STONEFIELD

7-Eleven, Inc.
Proposed Convenience Store with Fuel Sales
Shirley, New York
October 5, 2022

Intensification Fee & Pine Barrens Credit Redemption Comments:

- Q. In accordance with Town Code §85-82 entitled Land Use Intensification Mitigation Fee, be advised that this proposed change of zone application if approved will require a land use intensification fee based on the acreage being re-zoned in the amount of \$TBD *(additional information is required for purpose of calculating the fee as referenced above)*. This fee will be a condition of final site plan approval and payable to the Town of Brookhaven, Joseph Macchia Environmental Preservation Capital Reserve Fund at that time. In accordance with 85-82.F. a minimum of 50% of the required land use intensification mitigation fee shall be submitted prior to the effective date of the change of zone. The submission of the outstanding land use intensification mitigation fee shall be a condition of final site plan approval.

Acknowledged. The Applicant is aware of the intensification mitigation fee and payment will be made upon final calculation.

- R. Be advised that pursuant to Town Code §85-467.B.(3)(a) & (b), Pine Barrens Credit redemption will be required for the proposed building area increase.

Acknowledged. Pine Barrens Credits will be purchased as needed.

Should you have any questions regarding the submission items or responses above please do not hesitate to contact our office.

Regards,



Zachary E. Chaplin, PE
Stonefield Engineering and Design, LLC

Via FedEx

K:\2018\K-18112 7-Eleven - 1481 William Floyd Parkway, Shirley, NY\Correspondence\Outgoing\Municipality\2022-10-05_Planning Response Letter (Update).docx